



**CITY OF LOS ALTOS
CITY COUNCIL MEETING
July 28, 2015**

DISCUSSION ITEMS

Agenda Item # 11

SUBJECT: Deny Design Review 14-D-10, Use Permit 14-UP-05 and Subdivision 14-SD-01 applications for 999 Fremont Avenue subject to findings

BACKGROUND

This is a Design Review, Use Permit and Subdivision application for a new mixed-use, multiple-family residential and commercial building at 999 Fremont Avenue. The project would replace an existing 1,000 square-foot commercial building. The proposal includes 1,792 square feet of commercial space, 14 parking spaces and four multiple-family residential condominium units.

Prior to final action by the Planning and Transportation Commission (PTC), the project was reviewed on three occasions by the PTC and once by the Bicycle and Pedestrian Advisory Commission (BPAC). It went to the Commissions on the following dates:

1. PTC study session on January 23, 2014
2. BPAC meeting on August 27, 2014
3. PTC meeting on December 4, 2014
4. PTC meeting on January 15, 2015

The minutes for the PTC meetings of January 23, 2014, December 4, 2014 and January 15, 2015 and the BPAC meeting of August 27, 2014 provide the comments and actions of the Commissions and are included in Attachment 3 of the June 4, 2015 PTC agenda report. The June 4, 2015 PTC agenda report is a comprehensive report that replaces preceding agenda reports. The minutes for the PTC meeting of June 4, 2015 are included in Attachment 4.

Final action of the PTC on June 4, 2015 was a 4-1 vote for denial. Commissioner Bodner recused herself due to owning property within 500 feet. Commissioner Junaid recused herself due to her architectural firm working on the project. Commissioner Moison opposed the motion to recommend denial of the project to the City Council based on concerns that the applicant was close to resolving the PTC's concerns and he wanted to encourage the applicant to consider additional revisions, including addressing scale and perceived lack of parking. The PTC based its recommendation to deny the project relative to the following City goals, policies and requirements:

1. The project was inconsistent with the General Plan and Loyola Corners Specific Plan;
2. The size and massing of the project appeared too large in scale with the surroundings;
3. The project lacked an appropriate attention to providing human scale elements;
4. The project needs to be unified in its design, with greater attention to the immediate neighborhood character, greater setbacks from the street on all floors, and more sensitivity to the gateway site setting tone for the areas future commercial development;
5. The mixed-use is not desirable in accordance with the Specific Plan use permit requirements;
6. The subdivision is not appropriate based on the use permit concerns;
7. The design does not meet the standard of high quality design and is not in keeping with the character of Loyola Corners; and
8. The parking and access to the parking.

The PTC stated these concerns verbally at the meeting, understanding that staff would format them into detailed findings based on the Commission's discussion. These detailed General Plan and Specific Plan policies and design findings are attached. The Commission's concerns about the project's parking and access related to the Specific Plan zoning district allowing a reduced parking ratio of one space for every 300 feet of any building area versus the normal requirement of one parking space for every 200 square feet of retail area, and that the access to the parking was limited.

At the meeting the PTC received considerable comments and correspondence regarding the project. Those who spoke in opposition to the project cited such concerns as its size and height, the need for story-poles, lack of retail emphasis, insufficient building setbacks, and lack of on-street parking. Those in support of the project cited the importance of area revitalization. The correspondence to the PTC is included in Attachment 5.

Along the A Street frontage, the applicant erected two story poles to reflect the top of the second story at 20 feet and the top of the third story at 30 feet. The story poles were not required for the project since the project was an accepted application and under review prior to adoption of the Open Government Policy and story pole requirement; however, the applicant recognized a benefit in constructing the poles on the site for City Council review. Given the existing business and circulation constraints of the site, the story poles reflect only the height of the building and not the shape of the building. The applicant has provided verification of the heights of the story poles, and the new, larger, graphic notice board has been in place since prior to the June 4, 2015 PTC meeting.

EXISTING POLICY

Loyola Corners Neighborhood Commercial Center Specific Plan

PREVIOUS COUNCIL CONSIDERATION

None

DISCUSSION

Design

The PTC found that the project was inconsistent with the General Plan and Loyola Corners Neighborhood Commercial Center (LCNCC) Specific Plan. General Plan Community Design Policy stresses a consideration of a project's bulk, massing and human scale. This is articulated further in the Specific Plan Architectural Design standards (Specific Plan, Page 58) that encourage a design character that is informal, a size and mass of structures and building elements that reflect human scale, and building designs that are compatible in terms of scale, color and material with surrounding structures. The PTC discussed the effectiveness of the applicant's project revisions to meet the architectural design standards in the Specific Plan with a particular attention to changing the mass and bulk of the building to relate better to the surroundings. While the project proposed a conforming building with a height of 30 feet, a second story deemphasized with a sloping roof, a third story deemphasized by a flat roof, and a recessed third story to respond to bulk, mass and scale concerns, the PTC found the project was not consistent with the LCNCC Specific Plan.

The most recent plan iteration increased the commercial area from 1,000 square feet to 1,792 square feet and reduced the number of residential units from 5 units to 4 units. The reduction of dwelling units was not a recommendation made by the PTC.

Mixed-Use Project

Mixed-use buildings in the LCNCC Specific Plan district require a Use Permit. Additionally, the Use Permit requires that commercial expansion build upon the strengths of Loyola Corners and add business which is appropriate in terms of physical size of the business or use and size of site. Since the project only creates a net addition of approximately 800 square feet of ground floor commercial space, the PTC raised a concern that the size of the commercial area for the project will not substantially improve the economic vitality of the Loyola Corners neighborhood and may contribute to parking concerns.

Loyola Corners Specific Plan

The 1990 Loyola Corners Neighborhood Commercial Center Specific Plan included policies for specific parcels that are intended to guide land use decisions (Specific Plan, Page 42). One of the specific parcels is the subject property (SP-3 Photo Drive-Up Site). This policy is tied to an implementation strategy that includes forming a Parking Assessment District (see Specific Plan, Page 68). Although a Parking Assessment District is envisioned in Phase IB of the implementation, it was never formed by the City and the property owners. The area initially formed a Business Improvement District; however, over time, the Business Improvement District waned and disbanded. The Parking Assessment District was to fund acquisition of the subject property for an open plaza. Therefore, without a direct financial means to acquire the property, or the subject property owner's support, SP-3 has remained an infeasible policy.

PUBLIC CONTACT

The project was reviewed by the Bicycle and Pedestrian Advisory Commission at a publicly noticed meeting on August 27, 2014.

A public hearing notice was published in the *Town Crier*, posted on the property and mailed to all properties owners and business owners within 500 feet of the property for the Planning and Transportation Commission hearings that were held on December 4, 2014, January 15, 2015 and June 4, 2015. The mailed notice included 59 property owners and 30 commercial tenants.

A public hearing notice was published in the *Town Crier*, posted on the property and mailed to all properties owners and business owners within 500 feet of the property for the July 28, 2015 meeting of the City Council. The mailed notice included 59 property owners and 30 commercial tenants.

Posting of the meeting agenda serves as notice to the general public.

FISCAL/RESOURCE IMPACT

None

ENVIRONMENTAL REVIEW

Categorically Exempt per CEQA Section 15332 (in-fill project)

Deny Design Review 14-D-10, Use Permit 14-UP-05 and Subdivision 14-SD-01 applications for 999 Fremont Avenue subject to findings

RECOMMENDATION

Deny Design Review 14-D-04, Use Permit 14-UP-05 and Subdivision 14-SD-01 applications for 999 Fremont Avenue subject to findings

ALTERNATIVES

1. Direct staff to prepare the appropriate findings and conditions for adoption
2. Remand the project to the Planning and Transportation Commission to address specific design concerns

Prepared by: Sean K. Gallegos, Assistant Planner

ATTACHMENTS:

1. Project Plans, reviewed by Planning and Transportation Commission on June 4, 2015
2. Applicant cover letter
3. Planning and Transportation Commission Agenda Report, June 4, 2015
4. Planning and Transportation Commission Minutes, June 4, 2015
5. Correspondence

FINDINGS

14-SC-10, 14-UP-05 & 14-SD-01 – 999 Fremont Avenue

1. With regard to commercial design review, the City Council makes the following findings in accordance with Section 14.78.050 of the Municipal Code:
 - a. The proposal does not meet the goals, policies and objectives of the General Plan and Loyola Corners Neighborhood Commercial Center Specific Plan, design guidelines and district design criteria adopted for the area:
 - i. The project does not comply with the General Plan Economic Development Element Policy 6.4 due to the new mixed-use building only creating a net addition of 800 square feet of ground floor commercial space, which will not facilitate the long-term viability of the neighborhood convenience commercial uses or enhance the neighborhood character of Loyola Corners; and
 - ii. The project does not meet the General Plan Community Design Policy 1.6 that requires a focus on mass, scale, character and materials, and LCNCC Specific plan Community Design and Beatification Architectural Design standards for the character of the design: (1) The building character appears bulky in context and fails to consider the gateway site setting for Loyola Corners; and (2) The building does not meet the design standards in the LCNCC Specific plan and Community Design Policy 1.6 due to the three-story massing being out-of-scale and incompatible with the adjacent buildings, the current elevations not providing sufficient setback to reflect the character of the surrounding properties, and a lack of pedestrian and human scale elements.
 - b. The proposal lacks an appropriate architectural integrity and an appropriate relationship with other structures in the immediate area in terms of height, bulk and design:
 - i. The project does not conform with the Design Controls under Section 14.40.150 of the Los Altos Municipal Code that requires the design to complement the scale, proportion of building elements, and human scale due to the three-story massing being bulky and out-of-scale with the character of the surrounding properties, the current elevations not providing sufficient setback to provide bulk relief, and a lack of pedestrian and human-scale elements.
 - c. Building mass is not articulated to relate to the human scale, both horizontally and vertically. Building elevations does not have variation and depth and does not avoid large blank wall surfaces. Residential or mixed-use residential projects incorporate elements that signal habitation, such as identifiable entrances, stairs, porches, bays and balconies.
 - i. The project does not conform with the Design Controls under Section 14.40.150 of the Los Altos Municipal Code due to the proposed building elevations not having

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sufficient setbacks, articulation, variation or depth to relate the building to human scale.

2. With regard to use permit for a mixed-use building with retail and residential uses, the City Council finds in accordance with Section 14.80.060 (A-D and J) of the Municipal Code:
 - a. The proposed conditional use permit does not comply with the regulations prescribed for the district in which it is located and the general provisions of Chapter 14.02.
 - i. The project does not conform with the Design Controls under Section 14.40.150 of the Los Altos Municipal Code that requires the design to complement the scale, proportion of building elements and human scale due to the three-story massing being out-of-scale with the adjacent buildings, the current elevations do not provide sufficient setback to reflect the character of the surrounding properties, and a lack of pedestrian or human-scale elements.
 - b. The proposed construction is not found to meet the specific purposes of the Loyola Corners Specific Plan zoning district pursuant to Section 14.42.020 of the Los Altos Municipal Code:
 - i. The new mixed-use building creates a net addition of 792 square feet of ground floor commercial space, which will not facilitate the long-term viability of the neighborhood convenience commercial uses or enhance the neighborhood character of Loyola Corners; and
 - ii. The three-story massing lacks pedestrian or human scale elements, and it is out-of-scale with the adjacent buildings due to not providing sufficient setback and reduction of bulk and scale.
3. With regard to the subdivision the City Council finds in accordance with Section 66474 of the Subdivision Map Act of the State of California:
 - a. That the proposed subdivision is inconsistent with the General Plan and Specific Plan
 - i. The project does not comply with the General Plan Economic Development Element Policy 6.4 due to the project not facilitating the long-term viability of the neighborhood convenience commercial uses or enhancing the neighborhood character of Loyola Corners; and
 - ii. The project does not conform with the General Plan Community Design Policy 1.6 and LCNCC Specific plan goal due to the proposed building elevations not having sufficient setbacks, articulation, variation or depth to relate the building to human scale.

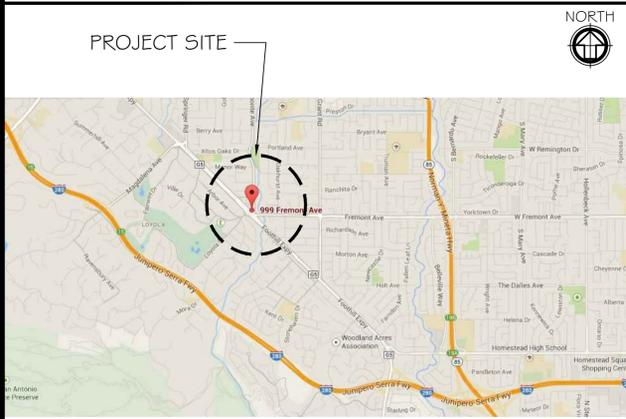
DATE	REVISION
10.29.2014	DESIGN REVIEW
11.12.2014	DESIGN REVIEW
02.17.2015	DESIGN REVIEW
05.20.2015	DESIGN REVIEW



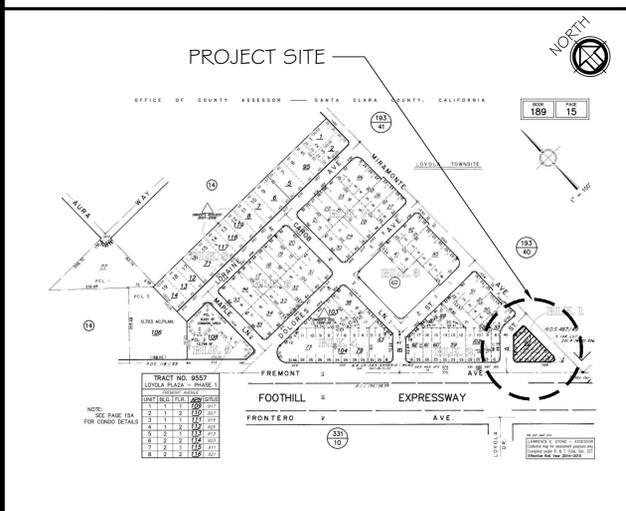
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NEW COMMERCIAL & MULTI-FAMILY LOYOLA CORNERS

VICINITY MAP NOT TO SCALE



PARCEL MAP NOT TO SCALE



CODE SUMMARY

TYPE OF CONSTRUCTION: TYPE V-B - FULLY SPRINKLERED

OCCUPANCY: OCCUPANCY GROUPS: R-3/U
STORIES: 3 STORIES + BASEMENT

CODE COMPLIANCE:

- 2013 CALIFORNIA BUILDING CODE
- 2013 CALIFORNIA GREEN BLDG. STANDARDS CODE
- 2013 CALIFORNIA ELECTRICAL CODE
- 2013 CALIFORNIA MECHANICAL CODE
- 2013 CALIFORNIA PLUMBING CODE
- 2013 CALIFORNIA FIRE CODE
- 2013 CALIFORNIA ENERGY CODE
- 2013 CALIFORNIA RESIDENTIAL CODE
- LOS ALTOS MUNICIPAL CODE

NOTHING ON THE DRAWING SHALL BE CONSTRUED TO PERMIT WORK NOT CONFORMING TO THE LISTED CODES AND REGULATIONS.

GENERAL NOTES

GEOTECHNICAL ASPECTS:
THE GEOTECHNICAL ASPECTS OF THE CONSTRUCTION, INCLUDING SITE GRADING, BASEMENT EXCAVATION, PIER DRILLING, FOOTING EXCAVATIONS, PREPARATION OF SUBGRADE AND PLACEMENT OF NON-EXPANSIVE FILL BENEATH THE BASEMENT SLAB AND SLABS-ON-GRADE, PAVEMENTS, RETAINING-WALL BACKFILL, AND INSTALLATION OF SURFACE AND SUBSURFACE DRAINAGE SHOULD BE PERFORMED IN ACCORDANCE WITH THE RECOMMENDATIONS OF THE GEOTECHNICAL REPORT PREPARED BY MURRAY ENGINEERS INC., DATED JANUARY 10, 2013. MURRAY ENGINEERS, INC. SHOULD BE PROVIDED AT LEAST 48 HOURS ADVANCE NOTIFICATION (650-559-9980) OF ANY EARTH WORK, EARTHWORK OPERATIONS AND SHOULD BE PRESENT TO OBSERVE AND TEST, AS NECESSARY, THE EARTHWORK, FOUNDATION, AND DRAINAGE INSTALLATION PHASES OF THE PROJECT.

SITE EXAMINATION:
THE CONTRACTOR SHALL THOROUGHLY EXAMINE AND SATISFY HIMSELF AS TO THE CONDITIONS UNDER WHICH THE WORK IS TO BE PERFORMED. THE CONTRACTOR SHALL VERIFY AT THE SITE ALL MEASUREMENTS AND CONDITIONS AFFECTING HIS WORK, AND SHALL BE RESPONSIBLE FOR THE CORRECTNESS OF SAME.

SAFETY:
IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONDITIONS OF THE JOB SITE INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.

DAMAGE TO STRUCTURE OR SYSTEMS TO REMAIN:
CONTRACTOR SHALL REIMBURSE OWNER(S) FOR REPAIR AND REPLACEMENT, TOGETHER WITH ANY DESIGN TEAM FEES, FOR ANY DAMAGE CAUSED TO STRUCTURES OR EXISTING SYSTEMS TO REMAIN, AS THE RESULT OF HIS/HER CONSTRUCTION OPERATIONS.

MEASUREMENTS:
THE CONTRACTOR SHALL VERIFY ALL DIMENSION SHOWS ON THE DRAWINGS BY TAKING FIELD MEASUREMENTS IF NECESSARY. PROPER FIT AND ATTACHMENT OF ALL PARTS REQUIRED. BEFORE COMMENCING WORK, CHECK ALL LINE AND LEVELS INDICATED AND SUCH OTHER WORKS TO VERIFY THAT IT HAS BEEN PROPERLY COMPLETED. SHOULD THERE BE ANY DISCREPANCIES, THIS OFFICE IS TO BE NOTIFIED FOR CORRECTIONS AND/OR RESOLUTION PRIOR TO COMMENCEMENT OF ANY RELATED WORK.

EXISTING CONDITIONS:
ALL INFORMATION RELATING TO EXISTING CONSTRUCTION IS GIVEN AS BEING THE BEST INFORMATION AVAILABLE, BUT WITHOUT GUARANTEE OF ACCURACY. THE CONTRACTOR SHALL VERIFY ALL EXISTING CONDITIONS, DIMENSIONS, AND BUILDING DATA AT THE JOB SITE. ANY DISCREPANCIES REQUIRING MODIFICATION TO THE CONSTRUCTION SHALL BE REPORTED TO THE ARCHITECT IMMEDIATELY. NO MODIFICATIONS SHALL BE MADE WITHOUT APPROVAL OF THE OWNER(S).

CONTRACTOR'S EQUIPMENT:
COORDINATE WITH OWNER'S REPRESENTATIVE THE LOCATION OF CONTRACTOR'S BUILDING ENTRANCE AND LOCATION OF CONTRACTOR'S EQUIPMENT AND MATERIAL STORAGE AREA.

UTILITY SHUT-DOWNS AND CONNECTIONS:
BEFORE THE SHUT-DOWN OR TYING INTO ANY UTILITY, PRIOR APPROVAL SHALL BE OBTAINED FROM THE OWNER'S 4CITY REPRESENTATIVES. ALL BUILDING MATERIALS MUST BE ASBESTOS FREE.

CONSTRUCTION SCHEDULING:
CONTRACTOR SHALL COORDINATE HIS/HER CONSTRUCTION OPERATIONS WITH OWNER'S REPRESENTATIVE PRIOR TO SCHEDULING AND START OF THE WORK. CONTRACTOR SHALL PROVIDE PROTECTION TO ALL EXISTING SYSTEMS WHICH ARE IN USE AND ARE ADJOINING THE WORK AND ARE NOT PART OF THE WORK.

PLUMBING & ELECTRICAL:
(SEE ALSO PLUMBING AND ELECTRICAL NOTES) EXAMINATION OF PLUMBING AND ELECTRICAL SERVICES TO SITE BY CONTRACTOR PRIOR TO CONNECTION OR TYING INTO IS REQUIRED. IN ANY CASE WHERE A NEW LINE TIES INTO OR EXTENDS AN EXISTING LINE WITHIN THE LIMITS OF WORK, THE CONTRACTOR SHALL EXAM THE ENTRY LINE, OR ARRANGE FOR THE PROPER AGENCIES TO DO SO NOTIFY OWNER OF ANY DEFECTS PRIOR TO TYING INTO (E) LINES.

ALL WORK SHALL CONFORM AND SHALL BE IN CONFORMANCE TO ALL REQUIREMENTS OF THE STATE OF CALIFORNIA ENERGY COMMISSION (TITLE 24) ENERGY EFFICIENCY STANDARDS FOR NEW RESIDENTIAL BUILDINGS AND SPECIFY REGULATION AND MANDATORY FEATURE AS REFERRED TO IN THE ENERGY CALCULATION OR AS NOTED ON DRAWINGS.

ALL GLASS SHALL CONFORM WITH HUMAN IMPACT AND SAFETY REQUIREMENTS AS PER CRC UNLESS OTHERWISE NOTED. ALL NAILING SHALL BE AS PER CBC.

PROVIDE SECURITY DEVICES AS REQUIRED BY THE CITY OR COUNTY. AS WELL AS SECURITY DEVICES SPECIFICALLY REQUESTED BY OWNER.

CONTRACTOR SHALL INSTALL ALL INSTALLATION AS REQUIRED BY TITLE 24.

FLOOR INSULATION: R-19 BATT
WALL INSULATION: R-13 MIN AT 2x4 STUD WALLS
R-13 MIN AT 2x6 STUD WALLS
R-30 BATT OR OPEN CELL
ATTIC INSULATION: R-30 HIGH PERFORMANCE BATT OR OPEN CELL FOAM
VAULTED CEILING: R-7 WRAPPED HOT WATER ONLY
PIPE INSULATION: R-4.2 MINIMUM BY DESIGN
DUCT INSULATION: R-12 BLANKET (OR AS REQUIRED FOR ON DEMAND WATER HTRS)
HOT WATER HEATER: R-12 BLANKET (OR AS REQUIRED FOR ON DEMAND WATER HTRS)

ALL INSULATION TO BE JOHNS MANVILLE FOMALDEHYDE-FREE FIBER GLASS INSULATION OR EQUAL AND INSTALLED AS PER MANUFACTURER'S SPECIFICATIONS.

CONTRACTOR SHALL COMPLETE INSULATION INSTALLATION CERTIFICATE AND PROVIDE OWNER WITH ALL MANUFACTURER'S MANUALS FOR EQUIPMENT SPECIFICALLY DETAILING EFFICIENT OPERATION AND MAINTENANCE REQUIREMENT

CONDITIONS OF APPROVAL

PROVIDE AUTOMATIC SPRINKLERS AND UNDERGROUND UTILITY SERVICE TO THE BUILDING

PROJECT DESCRIPTION

SUMMARY OF WORK:
NEW COMMERCIAL + MULTI-FAMILY BUILDING:

BASEMENT

- (4) 9' X 18' RESIDENTIAL PARKING STALLS ACCESSED BY LIFT
- STORAGE (RETAIL + RESIDENTIAL)

GROUND FLOOR

- (2) RETAIL STORES
- (9) 9' X 18' PARKING STALLS, (1) ADA PARKING STALL
- TRASH + RECYCLING
- BLDG. MAINTENANCE SUPPORT

SECOND FLOOR

- (1) THREE-BEDROOM / THREE AND A HALF BATH CONDOMINIUM
- (1) THREE-BEDROOM / THREE BATH CONDOMINIUM
- (1) THREE-BEDROOM / TWO AND A HALF BATH CONDOMINIUM

THIRD FLOOR

- (1) FIVE-BEDROOM / FOUR AND A HALF BATH CONDOMINIUM

PROJECT TEAM

OWNERS:
LOYOLA CORNERS ESTATES LLC
1900 CAMDEN AVE.
SAN CARLOS, CA 95124
CONTACT: GREGG BUNKER
PHONE: (408) 558-3600 / (408) 781-1725
email: gregg@greggbunker.com

ARCHITECT:
M DESIGN ARCHITECTS
4546 EL CAMINO REAL, STE 223
LOS ALTOS, CA 94022
CONTACT: ALPHEUS JESSUP
PHONE: (650) 565-9036
email: aly@mdesignsarchitects.com

LANDSCAPE:
DRAKE DESIGN ASSOCIATES
51 UNIVERSITY AVENUE, SUITE 1
LOS GATOS, CA 95030
CONTACT: LEAH DRAKE
PHONE: 408.688.7651
email: LEAH@DRAKEDESIGNASSOC.COM

CIVIL:
SMP ENGINEERS, LLC
1534 CAROB LANE
LOS ALTOS, CA 94024
CONTACT: SAIED RAZAVI
PHONE: (650) 941-8055
FAX: (650) 941-8755
email: srazavi@smpengineers.com

STRUCTURAL:
T.B.D.

CONTRACTOR:
T.B.D.

ENERGY CONSULTANT:

T.B.D.

PROJECT DATA

A.P.N.	189-15-092
ZONING DISTRICT	CN, COMMERCIAL NEIGHBORHOOD
SURVEY	REQUIRED
LOT SIZE BEFORE DEDICATION	7,929 Sq. Ft.
LOT SIZE AFTER SIDEWALK DEDICATION	7,348 Sq. Ft.
MAXIMUM SITE COVERAGE	NONE
MAX. HEIGHT	NO STRUCTURE SHALL EXCEED 30 FT (PER CITY'S SPECIFIC PLAN)
FLOOR AREA RATIO	NONE (SEE CHAPTER 14.40.070 OF MUNICIPAL CODE)

DRAWING INDEX

ARCHITECTURAL:

TO.1	TITLE SHEET
A1.0	PROPOSED ELEVATIONS
A1.1	STREETSCAPE ELEVATIONS
A2.0	PROPOSED SITE PLAN
A2.1	OVERALL BASEMENT PLAN
A2.2	OVERALL GROUND FLOOR PLAN
A2.3	OVERALL SECOND FLOOR PLAN
A2.4	OVERALL THIRD FLOOR PLAN
A2.5	OVERALL ROOF PLAN
A2.6	FLOOR AREA CALCULATIONS, PROJECT DATA
A4.1	SECTIONS A-A
A4.2	SECTIONS A-B
A5.1	DETAILS
A5.2	DETAILS

CIVIL

C-1	COVER SHEET/ NOTES
C-2	GRADING AND DRAINAGE PLAN
STRM-1	STORMWATER CONTROL PLAN
T-1	TOPOGRAPHIC SURVEY MAP
SP-1	EXISTING SITE PLAN
TM	TENTATIVE MAP

LANDSCAPE:

L-1.1	PRELIMINARY LANDSCAPE PLAN- GROUND FLOOR/STREETSCAPE
L-1.2	PLP- PLANT LEGEND AND SECOND LEVEL POTS
L-1.3	PLP- THIRD LEVEL PLANTERS AND STORM WATER FILTRATION PLANTER DETAIL
L-1.4	PLP- CONCEPTUAL ELEVATIONS

PROJECT SUMMARY TABLES

LOT COVERAGE	EXISTING 988 Sq. Ft.	PROPOSED 6,838 Sq. Ft.	REQD.	MAXIMUM ALLOWED NO RESTRICTIONS
FLOOR AREA				
BASEMENT	0 Sq. Ft.	4,457 Sq. Ft.		
GROUND FLOOR	988 Sq. Ft.	6,838 Sq. Ft.		
SECOND FLOOR	0 Sq. Ft.	6,838 Sq. Ft.		
THIRD FLOOR	0 Sq. Ft.	5,632 Sq. Ft.		
TOTAL	988 Sq. Ft.	23,765 Sq. Ft.		NO RESTRICTIONS
SETBACKS				
@ MIRAMONTE AVE	19'-5" ±	0'-0"	0'-0"	4'-6" (SIDEWALK DEDICATION REQD. BY CITY)
@ FREMONT AVE	14'-7" ±	1'-10"	0'-0"	0'-0"
@ A STREET	12'-8" ±	2'-3/11'-8"	0'-0"	0'-0"
HEIGHTS	16'-7" ±	30'-0"		30'-0"
PARKING STALLS CALCULATION				
USE DESCRIPTION	FORMULA	CALCULATION	QUANTITY	
RETAIL PARKING	Sq. Ft. / 300	1792 / 300	6	
RESIDENTIAL (4 UNITS)	# OF UNITS X 2	4 X 2	8	
REQUIRED PARKING STALLS			14	
PROPOSED PARKING STALLS			14	

SEE A2.6 FOR AREA CALCULATIONS

LOYOLA CORNERS
999 FREMONT AVE
LOS ALTOS, CA 94024

DESIGN DEVELOPMENT
VICINITY MAP, GENERAL
NOTES, CODE SUMMARY,
DRAWING INDEX &
PROJECT DATA

03-11-15

T0.1

**NOT FOR CONSTRUCTION
DRAFT REVIEW ONLY**

DATE	REVISION
10.29.2014	DESIGN REVIEW
11.12.2014	DESIGN REVIEW
02.17.2015	DESIGN REVIEW
05.20.2015	DESIGN REVIEW



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- 1) ROOF :
BUILT-UP FLAT ROOF
- 2) SKIRT ROOF:
FLAT CONCRETE TILE AS MANUFACTURED BY BORAL, OR APPROVED EQUAL
- 3) WALL MATERIALS (AS PER ELEVATIONS)
• STUCCO WITH EXPANSION JOINTS - SMOOTH OR LIGHT SAND FINISH
• STONE VENEER
- 4) WINDOWS & DOORS @ UNITS
• CLAD (METAL @ EXTERIOR & WOOD @ INTERIOR) - COLOR BRONZE ANODIZED
- 5) DOORS & GLASS PANELS @ GROUND LEVEL
• ANODIZED ALUMINUM - BRONZE
• DOUBLE PANE TEMPERED CLEAR GLASS
- 6) SERVICE DOORS @ GROUND LEVEL
• WEATHER RESISTANT HOLLOW METAL DOORS - PAINTED BRONZE
- 7) GARAGE DOOR
• MOTORIZED SLIDING DOOR WITH METAL MESH - COLOR BRONZE
- 8) GUARD RAILS
• ANODIZED ALUMINUM HANDRAIL - COLOR BRONZE ANODIZED - WITH TEMPERED GLASS PANELS
- 9) THIRD FLOOR FASCIA
• METAL FASCIA - BROWN COLOR
- 10) THIRD FLOOR CORBELS
• WOOD - PAINTED BROWN
- 11) MECHANICAL EQUIPMENT CLOSURE
• CEMENT BOARD, COLOR & FINISH TO MATCH 3RD FLOOR STUCCO

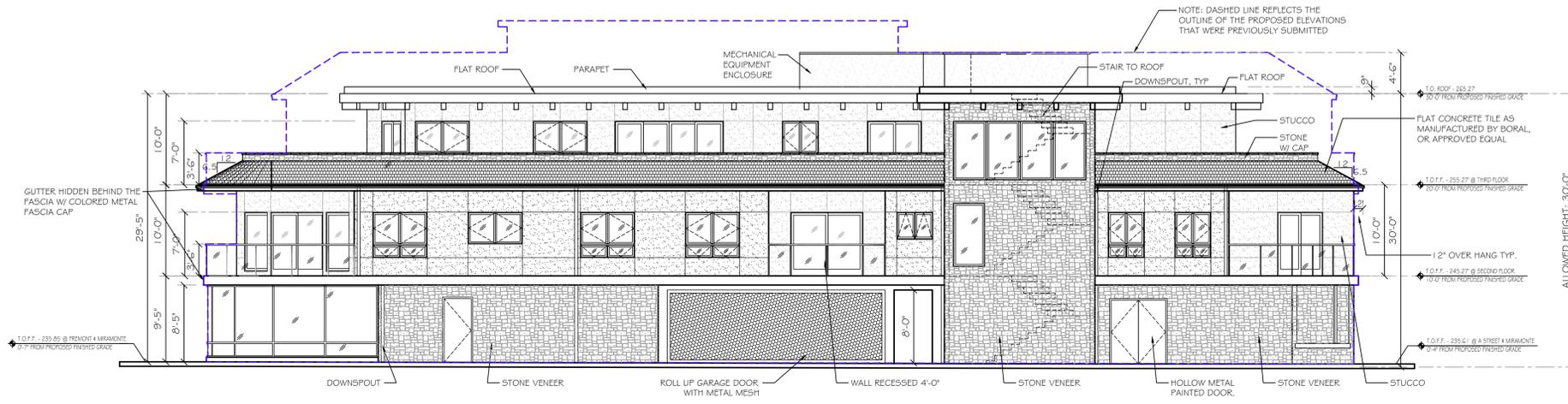
LOYOLA CORNERS
999 FREMONT AVE
LOS ALTOS, CA 94024

DESIGN DEVELOPMENT
PROPOSED ELEVATIONS

03-11-15

A1.0

NOTE:
SEE SHEETS A5.1 & A5.2 FOR DETAIL REFERENCES & DETAILS.



MIRAMONTE AVE

1



FREMONT AVE

SCALE: 1/8"=1'-0"

2



A STREET



SCALE: 1/8"=1'-0"

3

EXTERIOR FINISHES

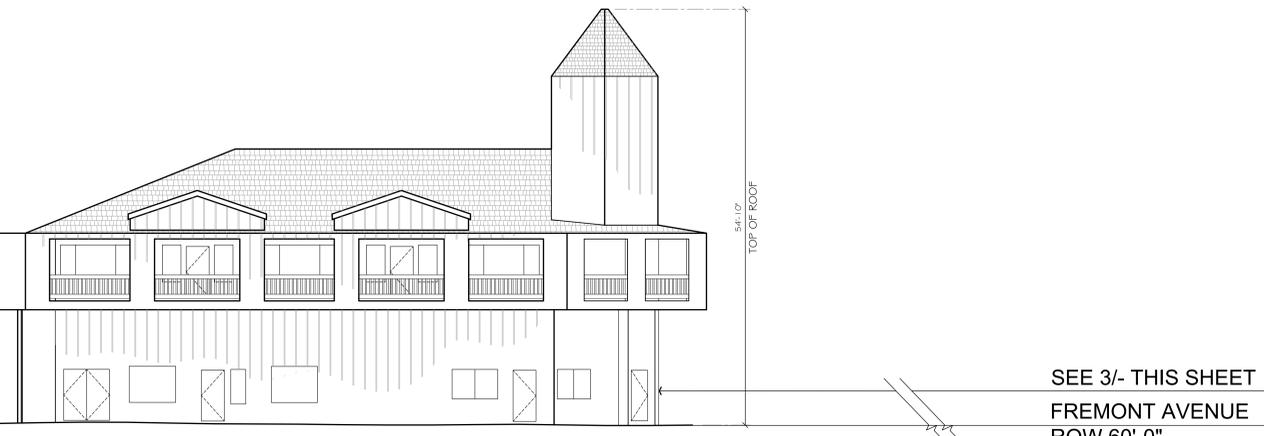


BUILDING F - EXISTING ELEVATION F1

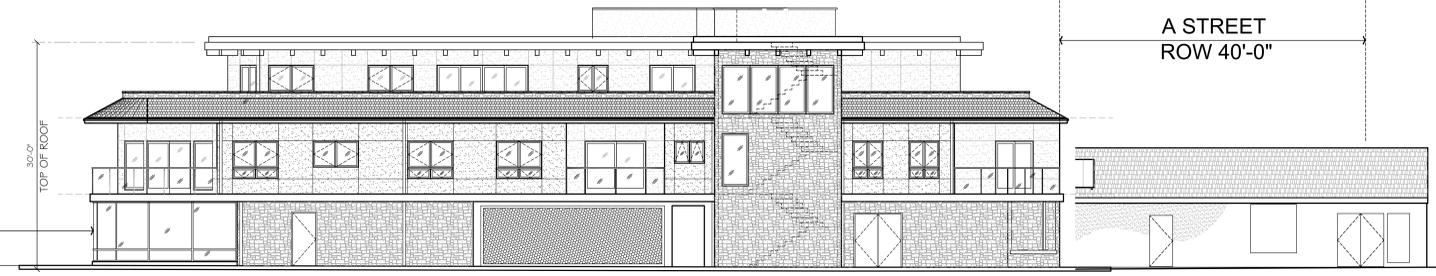
BUILDING A - PROPOSED ELEVATION A1 - FREMONT AVE.

BUILDING C - EXISTING ELEVATION C1

0 2' 4' 8' 16' 32' SCALE: 1/8"=1'-0" 1

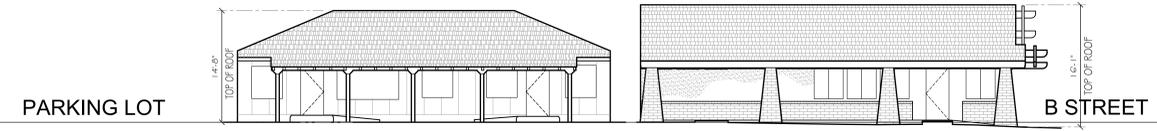


BLDG. B - EXIST. ELEV. B1



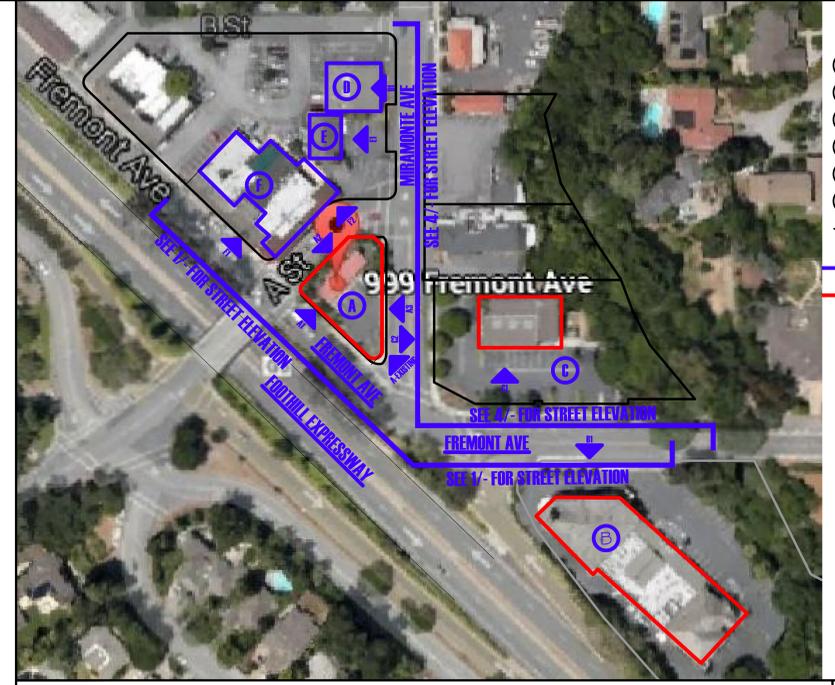
BUILDING A - PROPOSED ELEVATION A3 - MIRAMONTE AVE.

BLDG. F - EXIST. ELEV. F2 (PARTIAL- BEYOND)



BUILD. E - PROP. ELEV. E1 - MIRAMONTE AVE. BLDG. D - PROP. ELEV. D1 - MIRAMONTE AVE.

SCALE: 1/8"=1'-0" 4



KEY PLAN OF NEIGHBORHOOD

SCALE: 1/64"=1'-0" 3

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DESIGN DEVELOPMENT
KEY PLAN OF NEIGHBORHOOD
& STREETSCAPE ELEVATIONS

03-16-15

A1.1

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DATE	REVISION
10.29.2014	DESIGN REVIEW
11.12.2014	DESIGN REVIEW
02.17.2015	DESIGN REVIEW
05.20.2015	DESIGN REVIEW



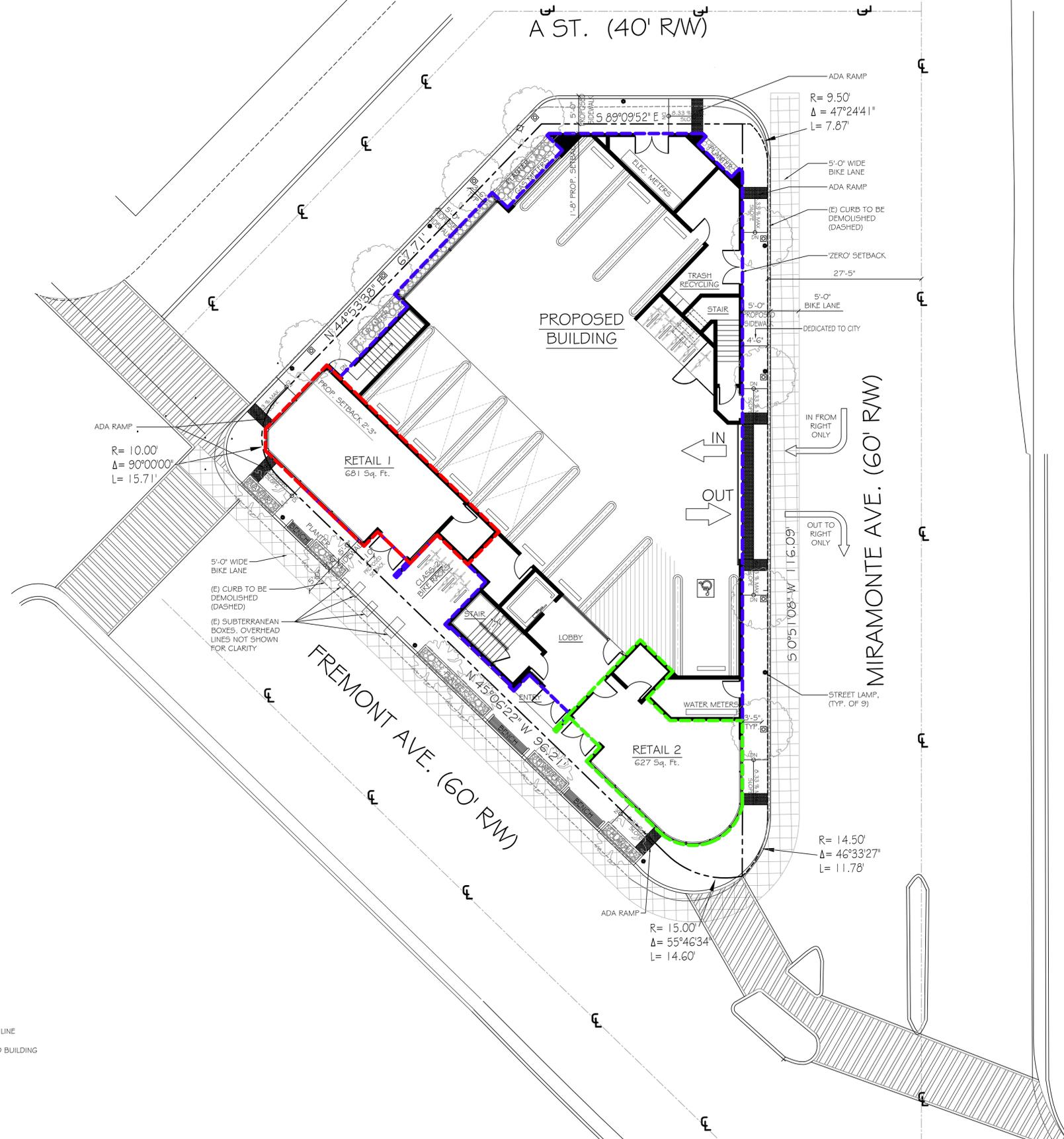
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DESIGN DEVELOPMENT
PROPOSED SITE PLAN

03-11-15

A2.0



- LEGEND
- PROPERTY LINE
 - PROPOSED BUILDING
 - RETAIL 1
 - RETAIL 2

PROPOSED SITE PLAN



SCALE: 1"=10'-0"

1

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BASEMENT:
RETAIL STORAGE: 484 SQ. FT.
BASEMENT: 3200 SQ. FT.
PARKING AREA: 773 SQ. FT.
TOTAL AREA: 4457 SQ. FT.

DATE	REVISION
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05.20.2015	DESIGN REVIEW



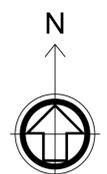
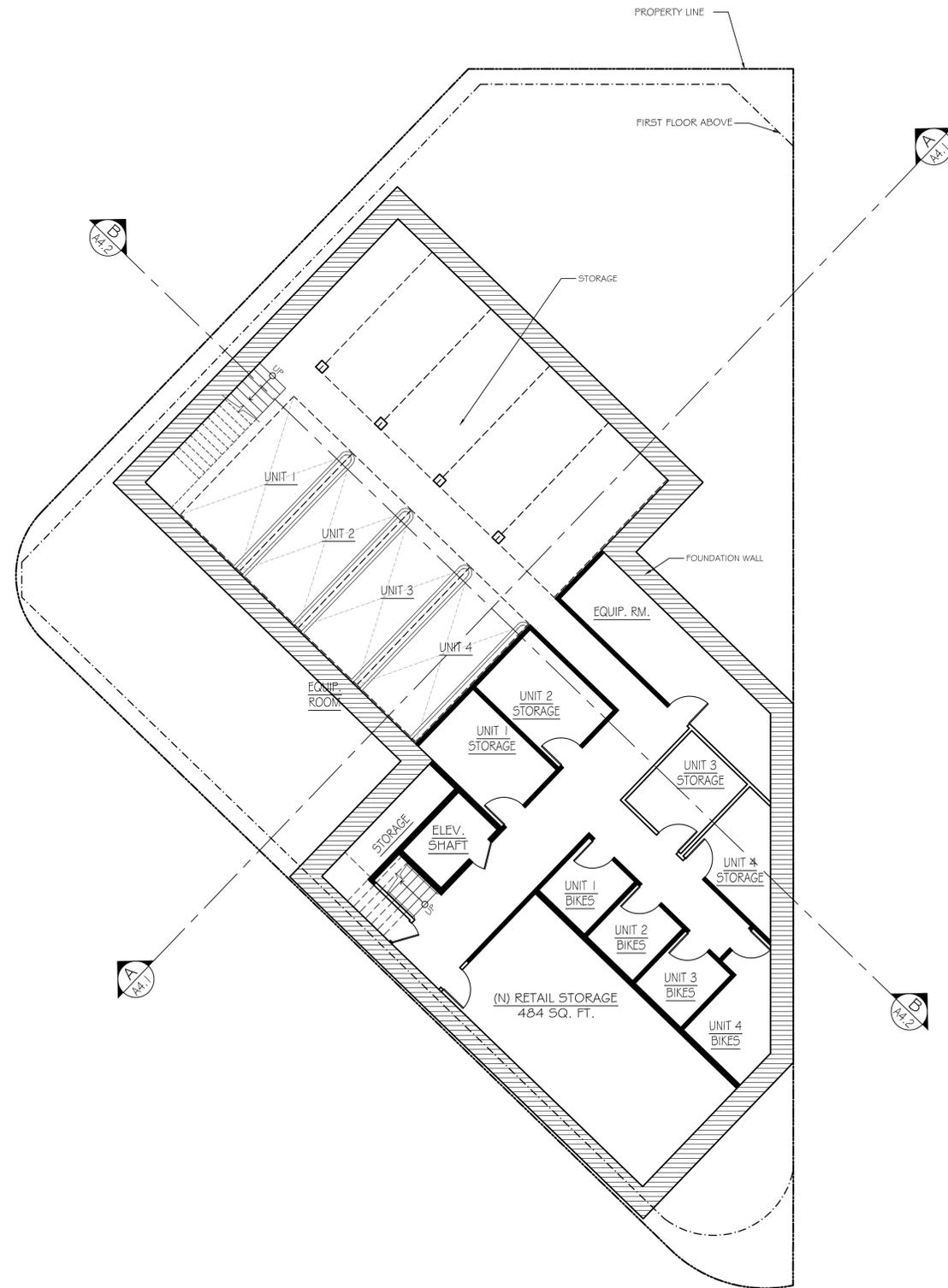
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DESIGN DEVELOPMENT
OVERALL BASEMENT PLAN

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A2.1



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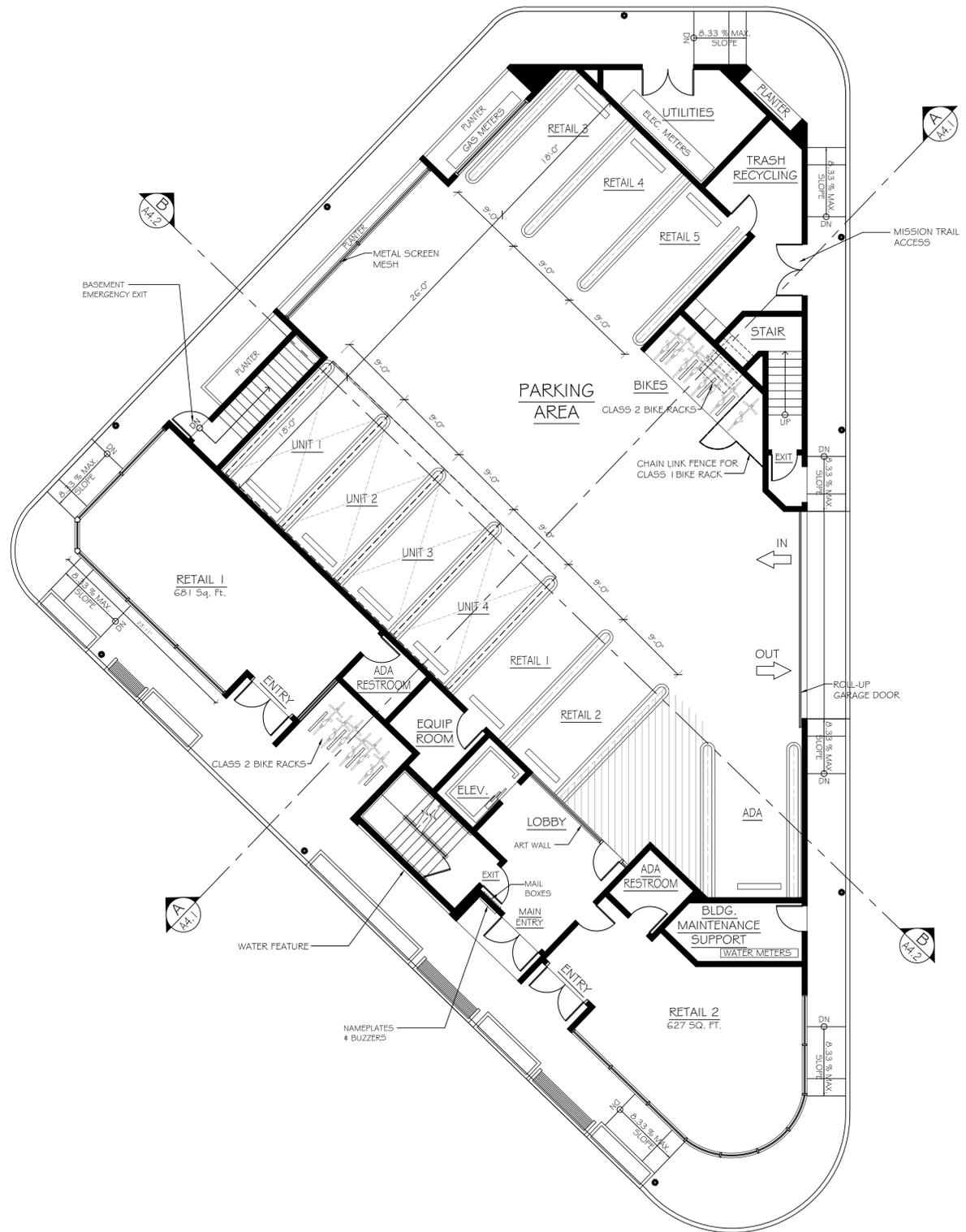
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DESIGN DEVELOPMENT
OVERALL GROUND
FLOOR PLAN

03-11-15

A2.2

1ST FLOOR:
RETAIL 1: 681 SQ. FT.
RETAIL 2: 627 SQ. FT.
PARKING AREA: 3805 SQ. FT.
BIKES & PLANTERS: 382 SQ. FT.
COMMON AREAS: 1343 SQ. FT.
TOTAL AREA: 6838 SQ. FT.



OVERALL GROUND FLOOR PLAN



SCALE: 1/8"=1'-0" 1



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2ND FLOOR:
 UNIT 1: 1982 SQ. FT. (3 BEDROOMS + 3 BATHS)
 UNIT 2: 1811 SQ. FT. (3 BEDROOMS + 3.5 BATHS)
 UNIT 3: 1662 SQ. FT. (3 BEDROOMS + 2.5 BATHS)
 COMMON AREA: 880 SQ. FT.
 BALCONIES: 503 SQ. FT.
 TOTAL AREA: 6838 SQ. FT.

DATE	REVISION
10.29.2014	DESIGN REVIEW
11.12.2014	DESIGN REVIEW
02.17.2015	DESIGN REVIEW
05.20.2015	DESIGN REVIEW



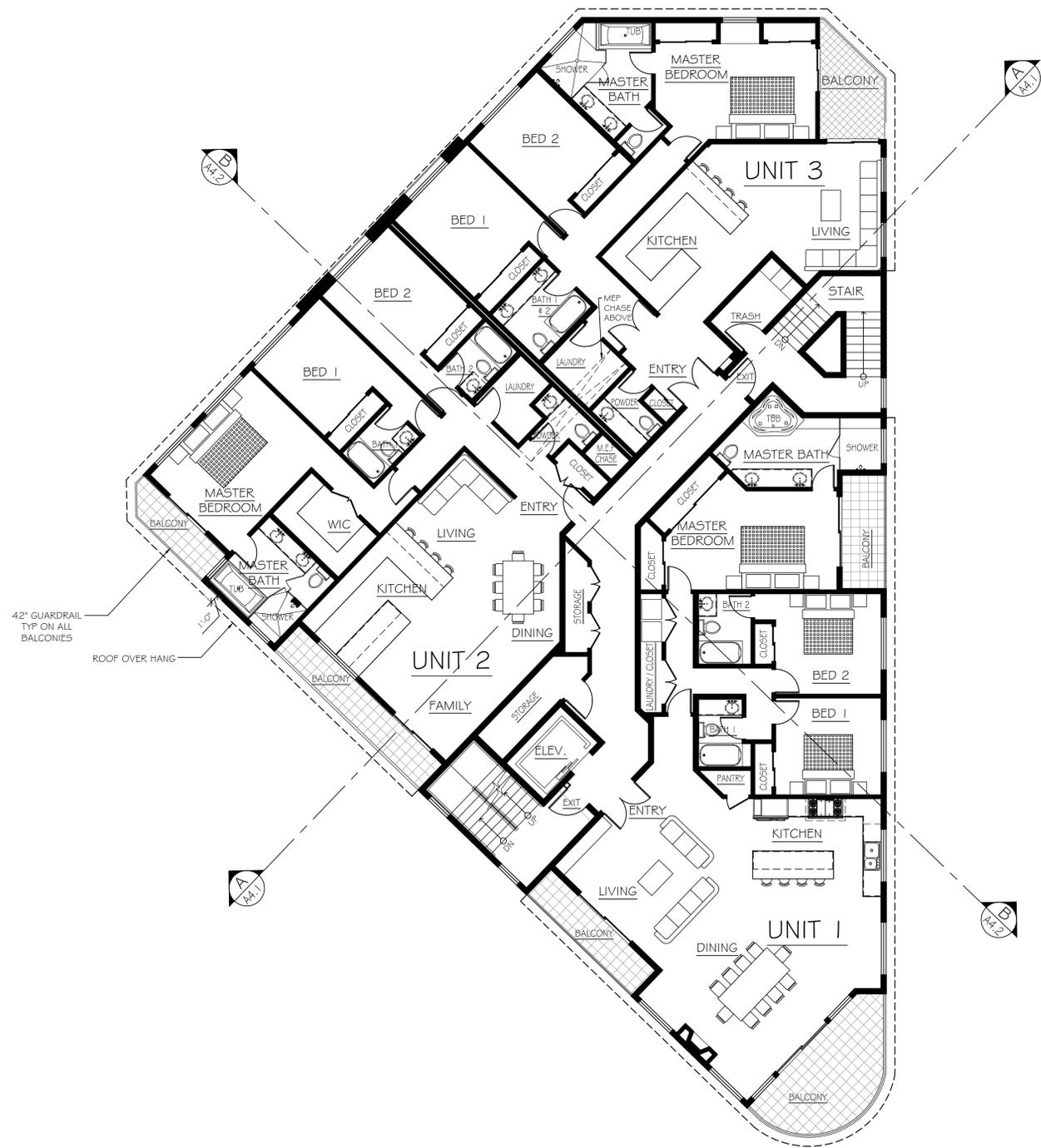
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DESIGN DEVELOPMENT
 PROPOSED
 SECOND FLOOR PLAN

03-11-15

A2.3



PROPOSED SECOND FLOOR PLAN



SCALE: 1/8"=1'-0" 1



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3RD FLOOR:
UNIT 4: 3366 SQ. FT. (5 BEDROOMS # 4.5 BATHS)
COMMON AREA: 454 SQ. FT.
BALCONIES: 1832 SQ. FT.
TOTAL AREA: 5652 SQ. FT.

DATE	REVISION
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02.17.2015	DESIGN REVIEW
05.20.2015	DESIGN REVIEW



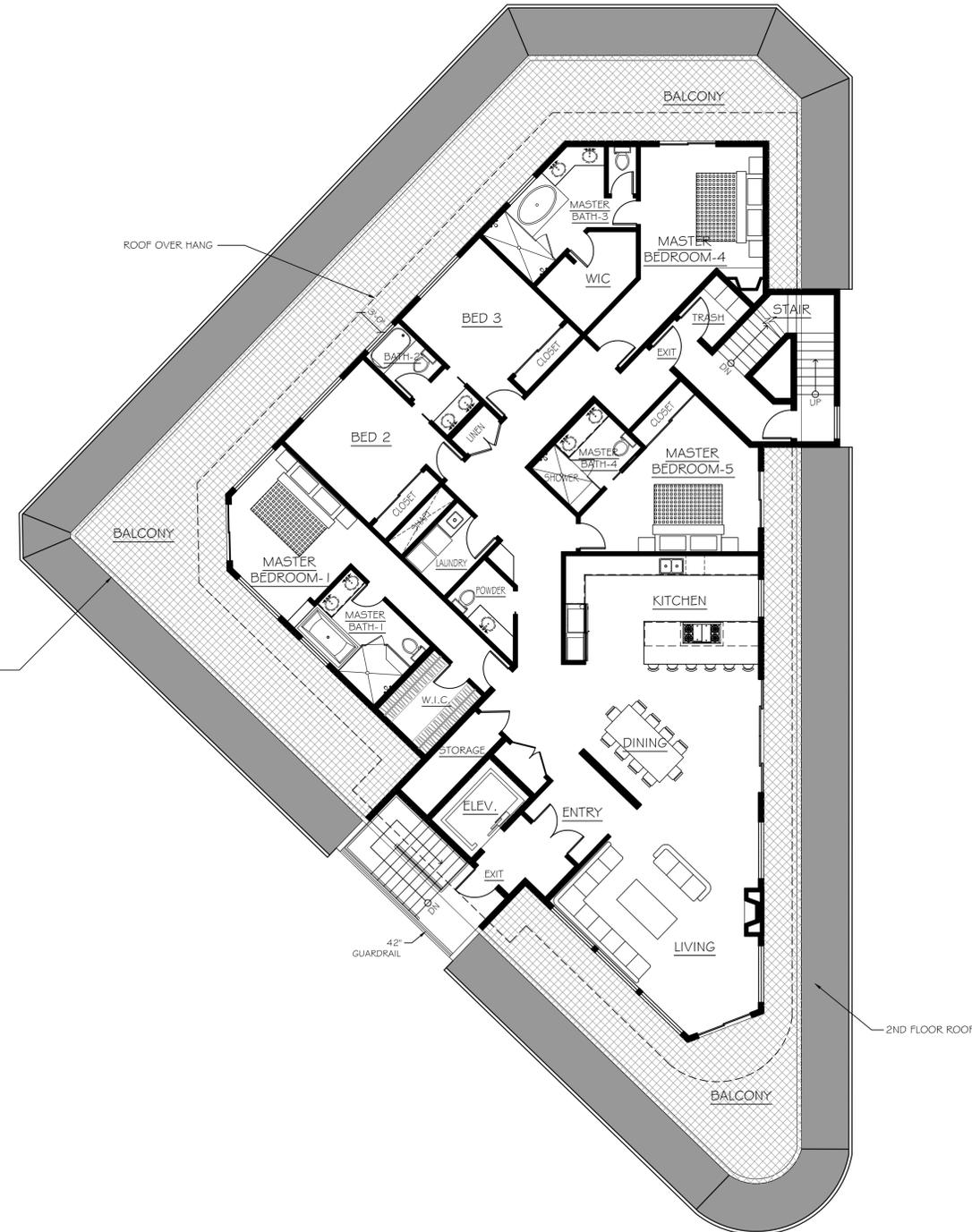
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DESIGN DEVELOPMENT
PROPOSED
THIRD FLOOR PLAN

03-11-15

A2.4



SCALE: 1/8"=1'-0" 1

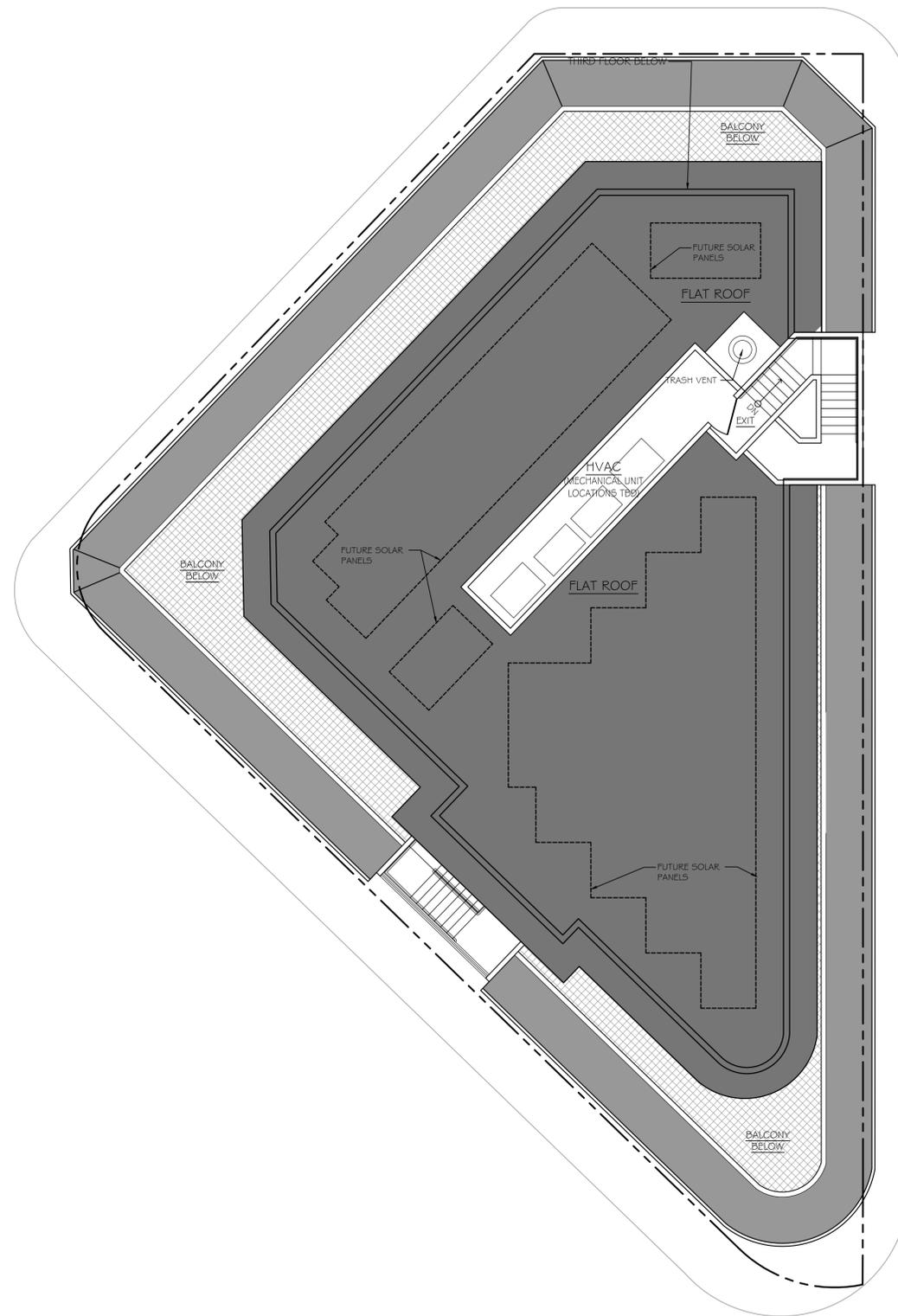
PROPOSED THIRD FLOOR PLAN

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DESIGN DEVELOPMENT
PROPOSED
ROOF PLAN

03-11-15

A2.5

PROPOSED ROOF PLAN



SCALE: 1/8"=1'-0" 1



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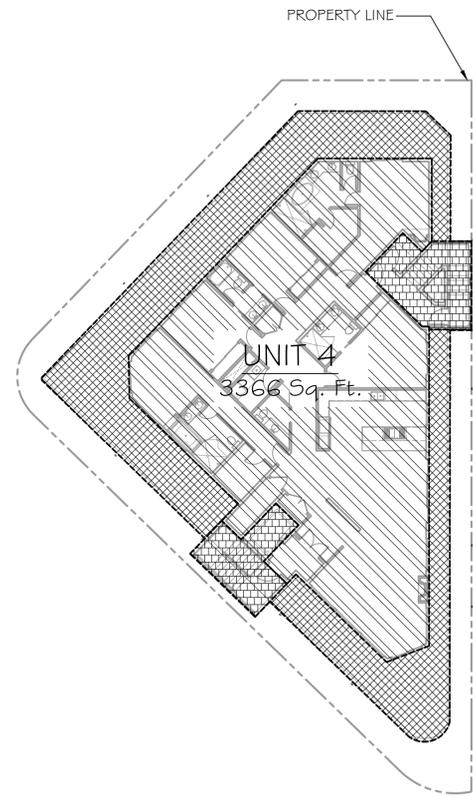
DATE	REVISION
10.29.2014	DESIGN REVIEW
11.12.2014	DESIGN REVIEW
02.17.2015	DESIGN REVIEW
05.20.2015	DESIGN REVIEW

SUMMARY OF LAND DEVELOPMENT

LOT AREA	7929 Sq. Ft.
ALLOWED COVERAGE (100%)	7929 Sq. Ft.
LOT AREA AFTER DEDICATION	7348 Sq. Ft.
PROPOSED COVERAGE	
RETAIL 1	681 Sq. Ft.
RETAIL 2	627 Sq. Ft.
PARKING AREA	3805 Sq. Ft.
PLANTER & BIKES	382 Sq. Ft.
COMMON AREA	1343 Sq. Ft.
TOTAL COVERAGE	6838 Sq. Ft. < 7348 Sq. Ft.
REQUIRED PARKING STALLS	14 (SEE T.O1 FOR STALLS CALCS.)
PROPOSED PARKING STALLS	(10) @ GROUND LEVEL & (4) @ BASEMENT

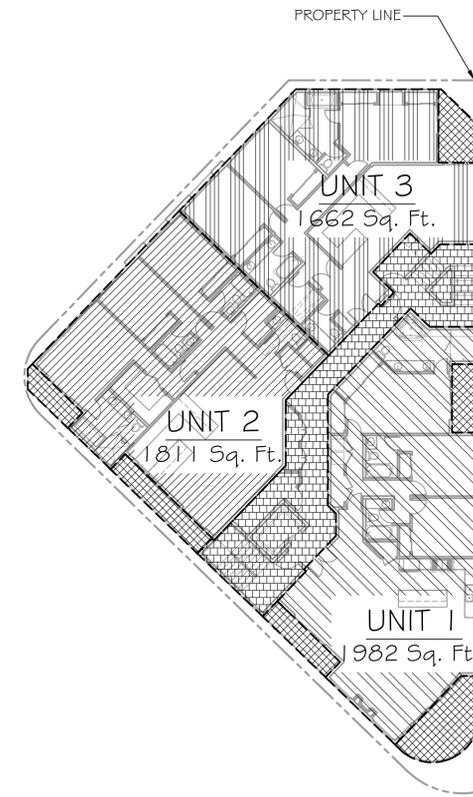


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LEGEND

	UNIT 4	3366 Sq. Ft.
	(5 BEDROOMS & 4.5 BATHS)	
	BALCONIES	1832 Sq. Ft.
	COMMON AREA	454 Sq. Ft.
	TOTAL	5652 Sq. Ft.



LEGEND

	UNIT 1	1982 Sq. Ft.
	(3 BEDROOMS & 3 BATHS)	
	UNIT 2	1811 Sq. Ft.
	(3 BEDROOMS & 3.5 BATHS)	
	UNIT 3	1662 Sq. Ft.
	(3 BEDROOMS & 2.5 BATHS)	
	BALCONIES	503 Sq. Ft.
	COMMON AREA	880 Sq. Ft.
	TOTAL	6838 Sq. Ft.

THIRD FLOOR AREA



SCALE: 1/16"=1'-0"

SECOND FLOOR AREA



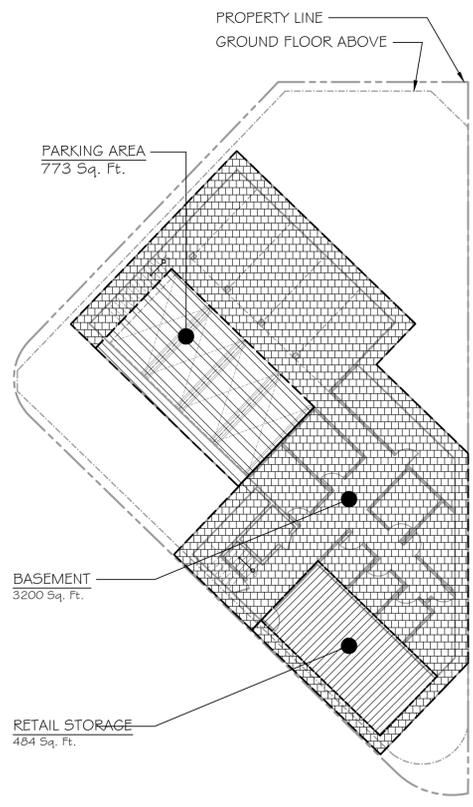
SCALE: 1/16"=1'-0"

8

PROJECT DATA

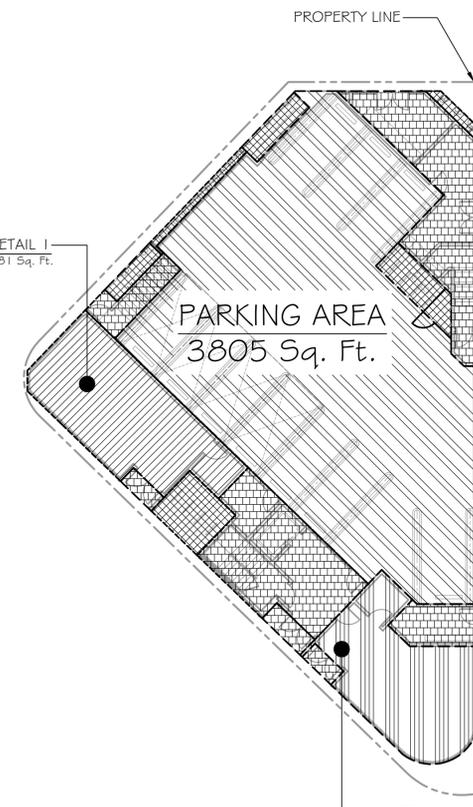
8

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LEGEND

	RETAIL STORAGE	484 Sq. Ft.
	BASEMENT	3200 Sq. Ft.
	PARKING AREA	773 Sq. Ft.
	TOTAL	4457 Sq. Ft.



LEGEND

	RETAIL 1	681 Sq. Ft.
	RETAIL 2	627 Sq. Ft.
	PARKING AREA	3805 Sq. Ft.
	BIKES & PLANTERS	382 Sq. Ft.
	COMMON AREA	1343 Sq. Ft.
	TOTAL	6838 Sq. Ft.

BASEMENT AREA



SCALE: 1/16"=1'-0"

16

GROUND FLOOR AREA



SCALE: 1/16"=1'-0"

8

DESIGN DEVELOPMENT
FLOOR AREA CALCULATIONS,
PROJECT DATA

03-11-15

A2.6

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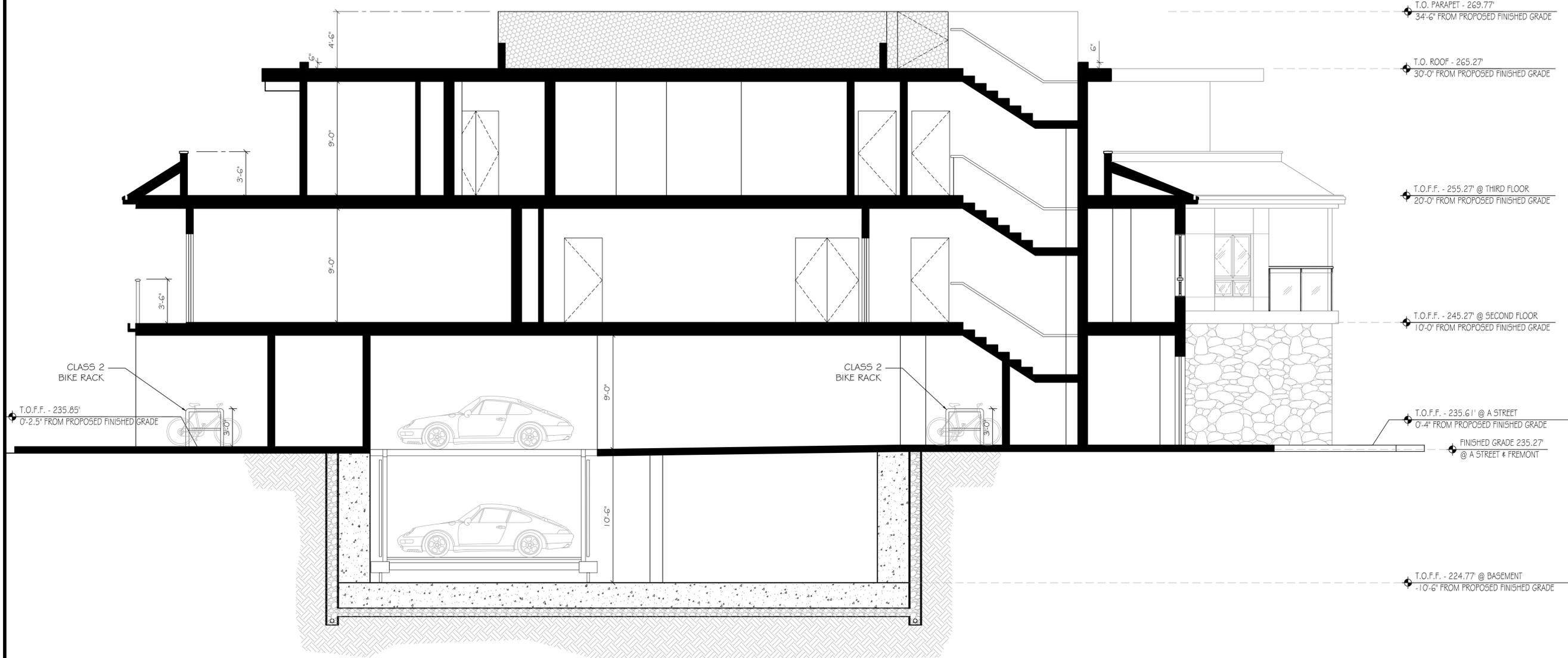
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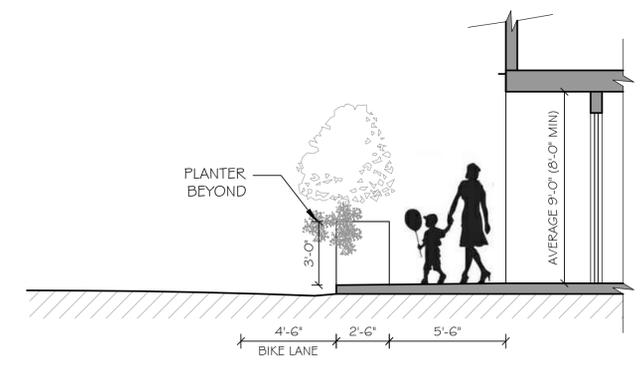
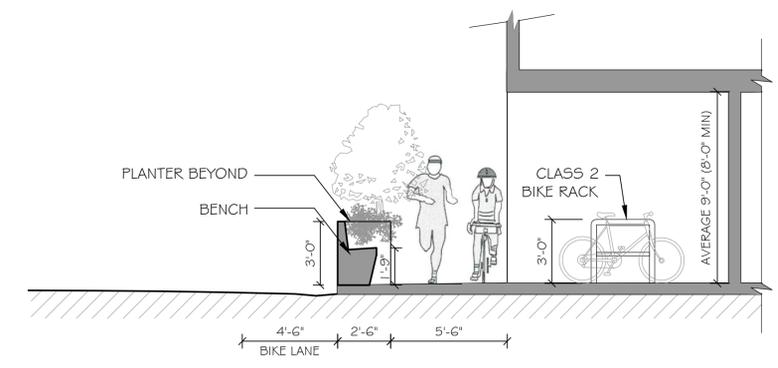
DESIGN DEVELOPMENT
SECTION A-A

03-11-15

A4.1



SECTION A-A



DETAIL SECTIONS



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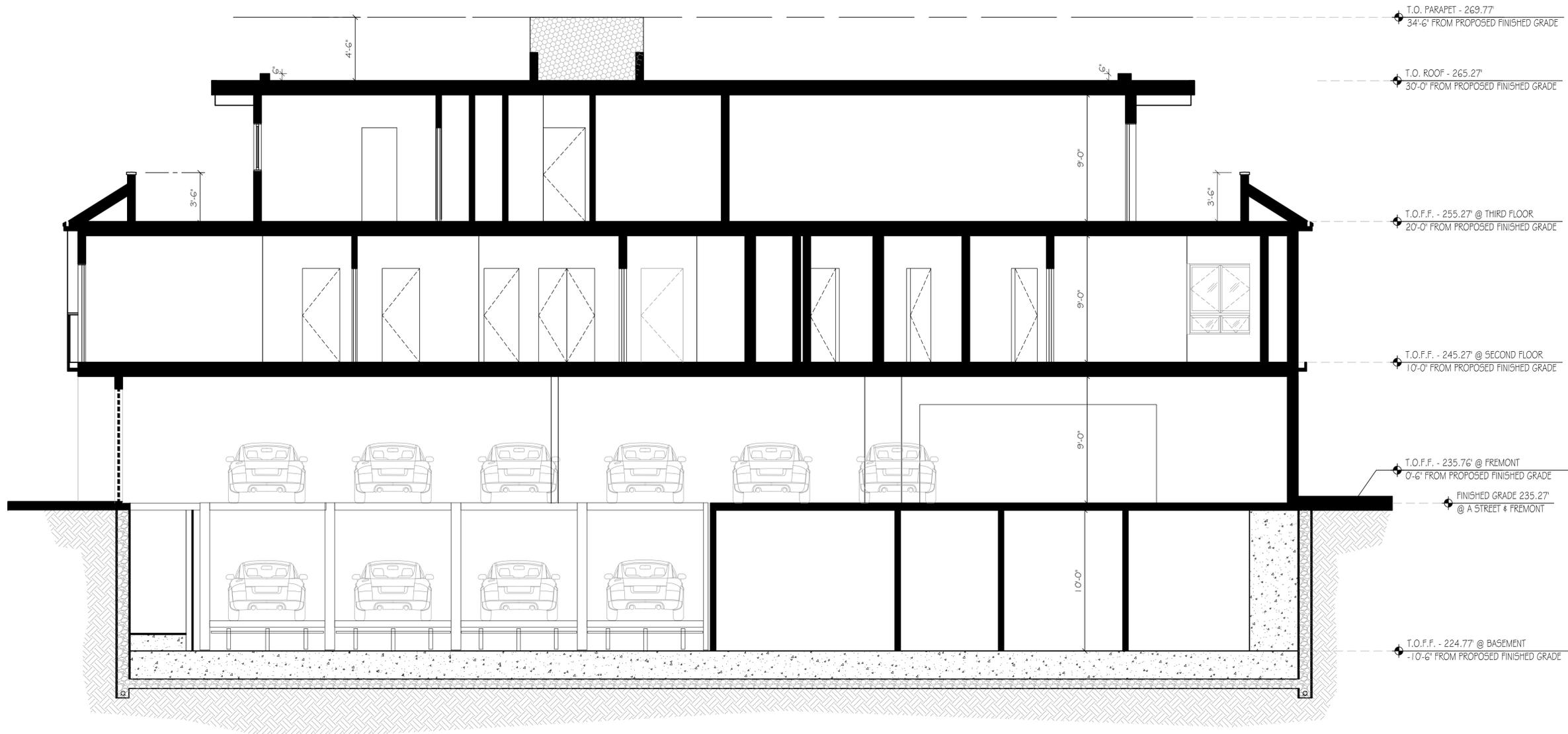
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DESIGN DEVELOPMENT
SECTION B-B

03-11-15

A4.2



SECTION B-B



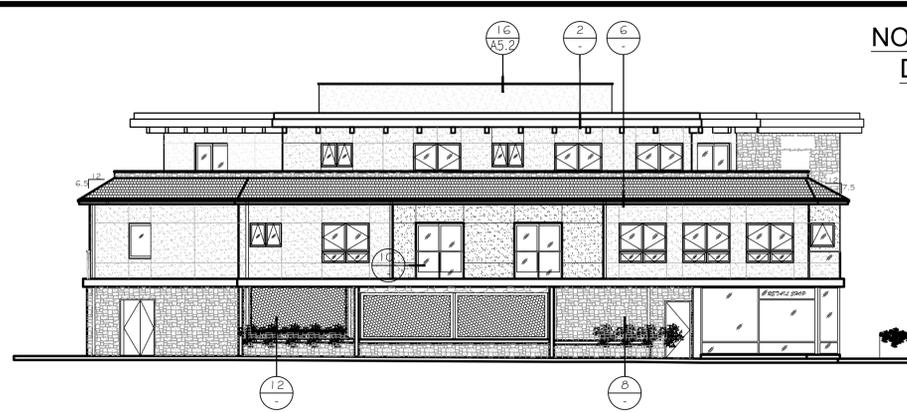
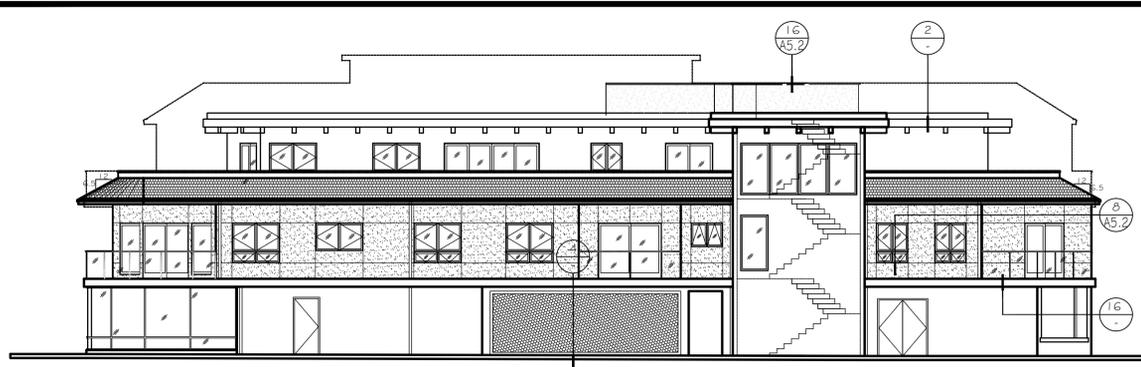
SCALE: 1/4"=1'-0"

1

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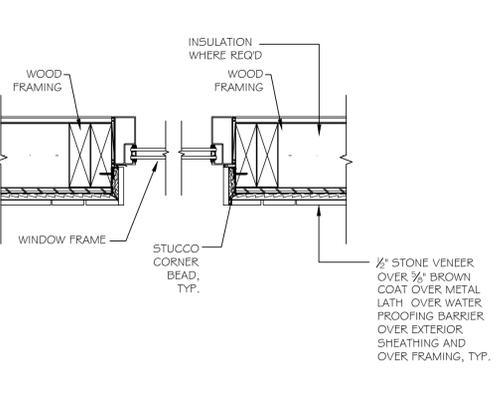
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ELEVATION MIRAMONTE AVENUE

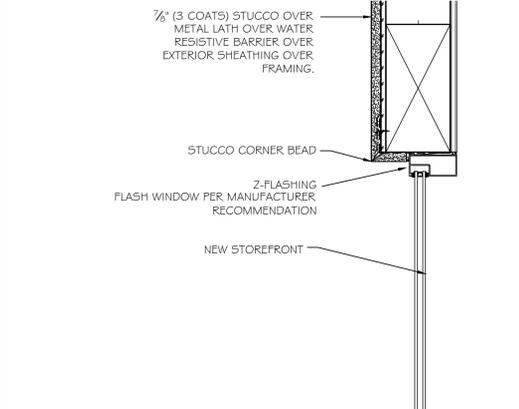
3/32":1'-0" 9

ELEVATION A STREET

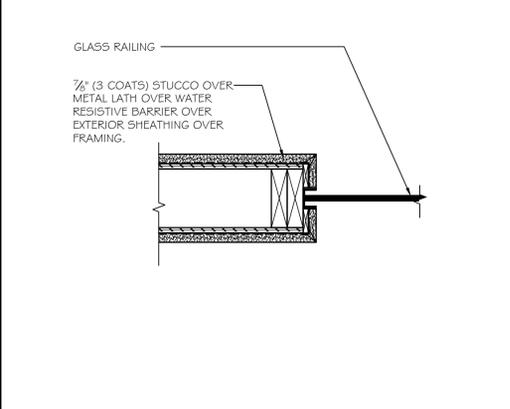
3/32":1'-0" 1



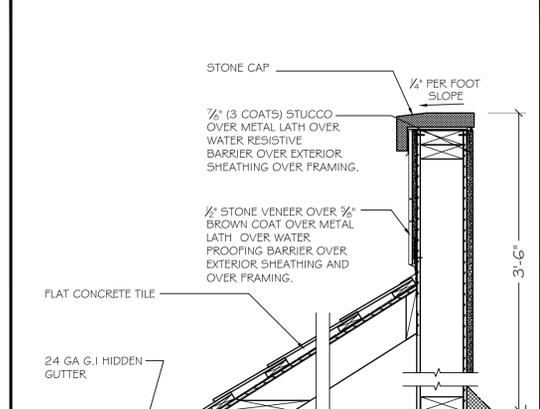
WINDOW @ STONE WALL 1 1/2":1'-0" 17



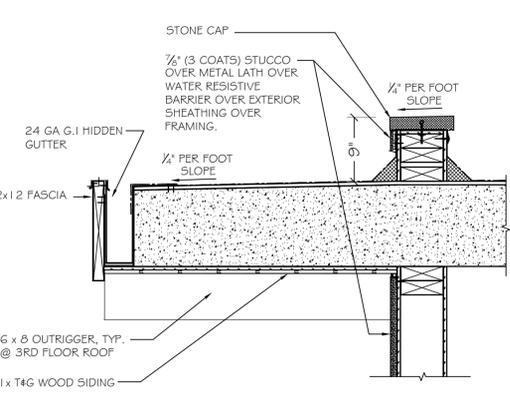
SECTION @ BALCONY 1-1/2":1'-0" 16



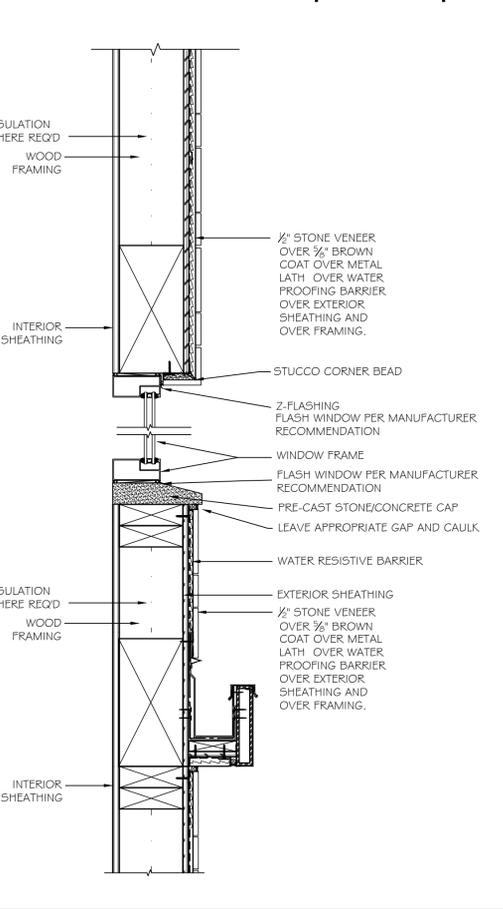
PLAN @ GLASS HANDRAIL 1-1/2":1'-0" 10



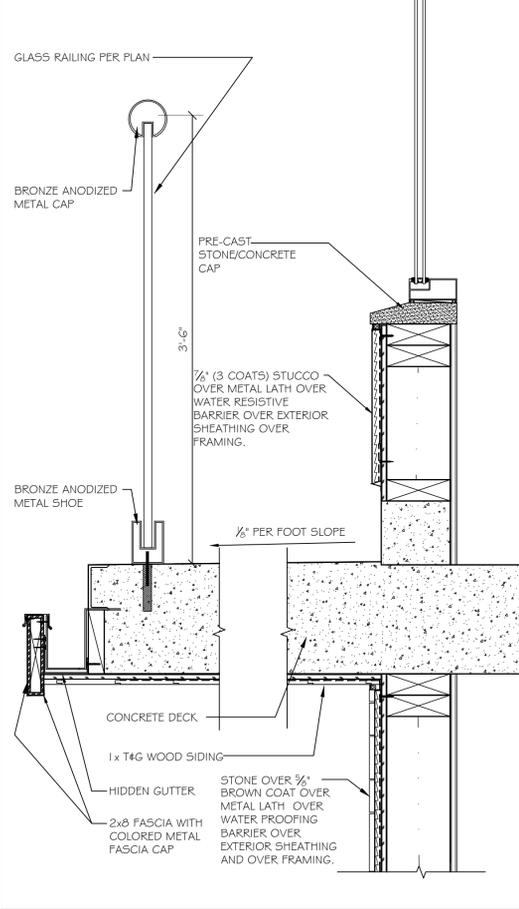
ROOF W/ HIDDEN GUTTER 1":1'-0" 6



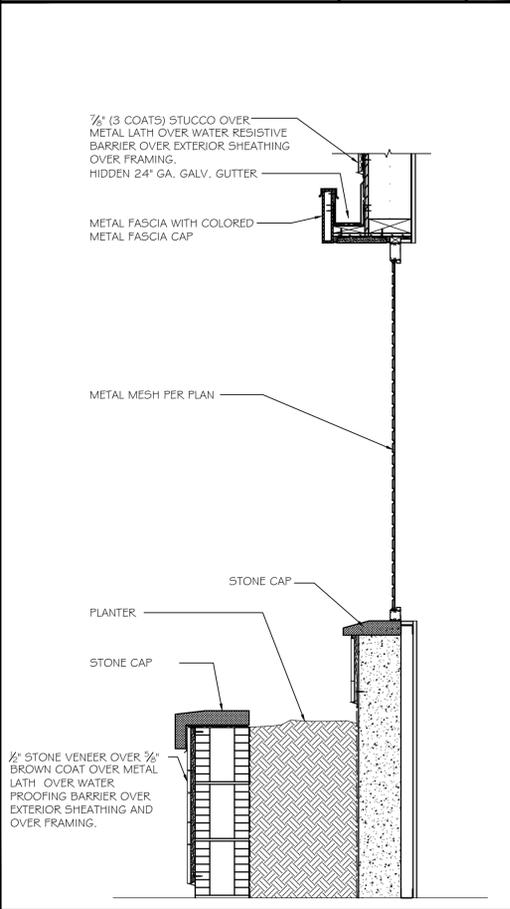
EAVE / PARAPET 1":1'-0" 2



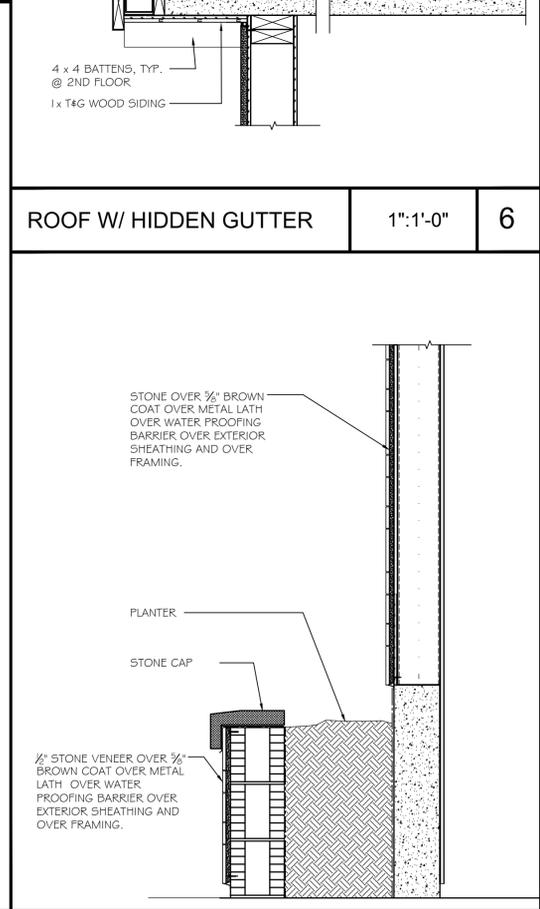
WINDOW @ STONE WALL 1 1/2":1'-0" 18



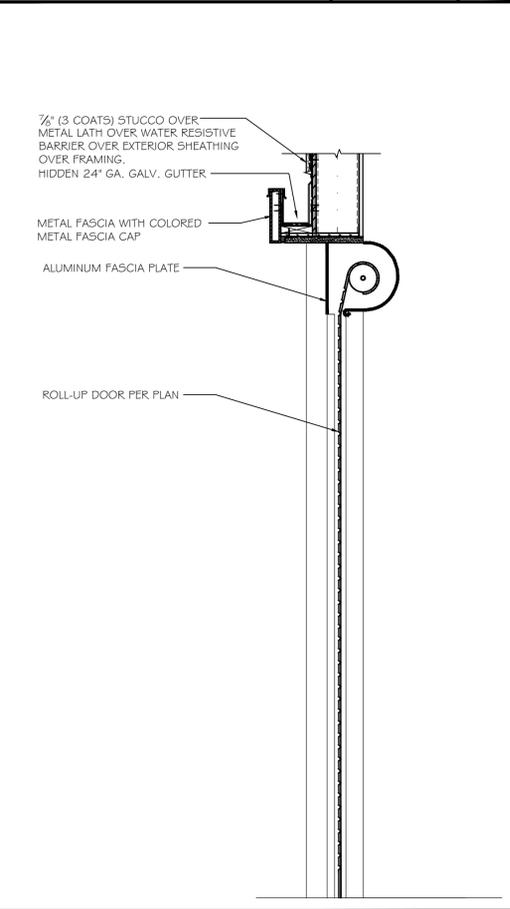
SECTION @ BALCONY 1-1/2":1'-0" 16



SECTION @ PLANTER 1":1'-0" 12



SECTION @ PLANTER 1":1'-0" 8



SECTION @ ROLL-UP DOOR 1":1'-0" 4

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DESIGN DEVELOPMENT
DETAILS

03-11-15

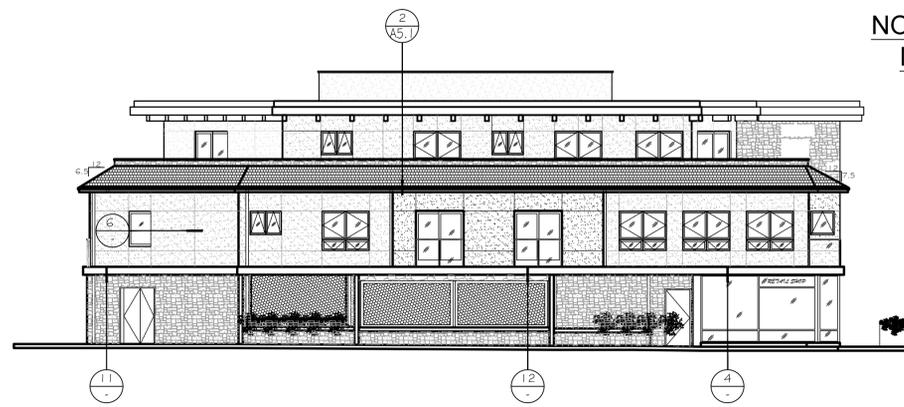
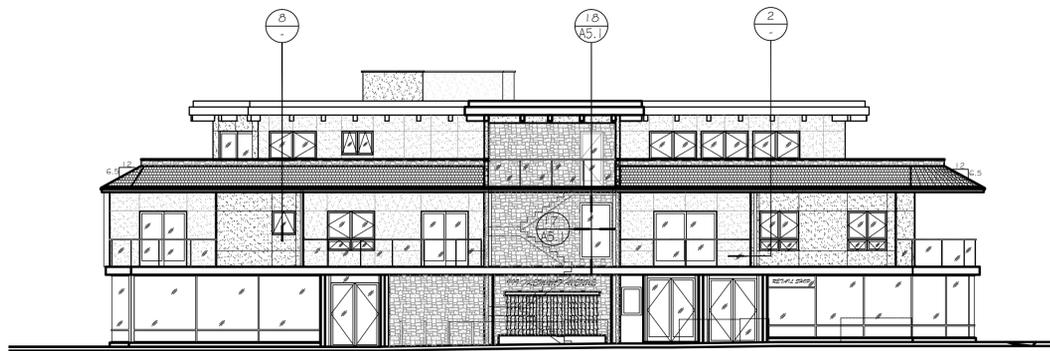
A5.1

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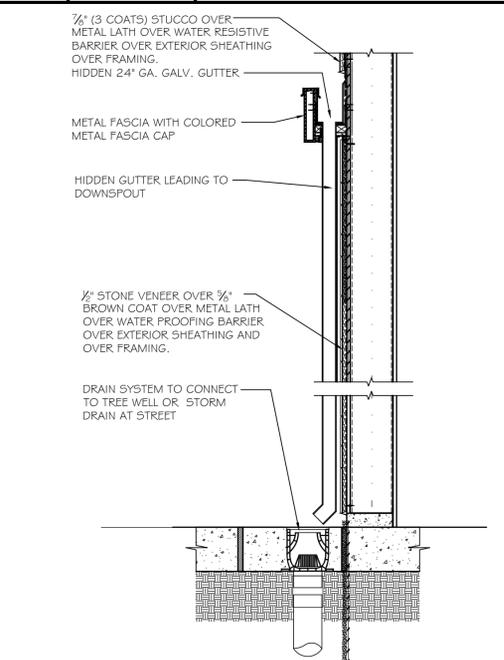
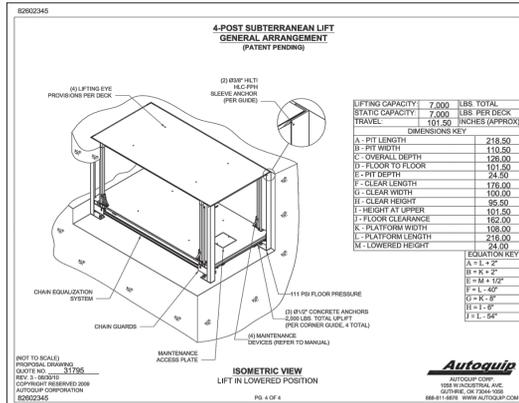
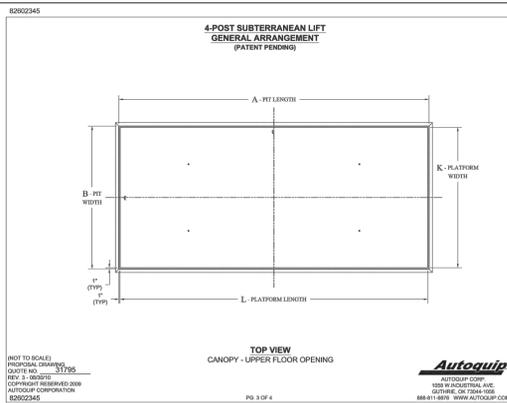
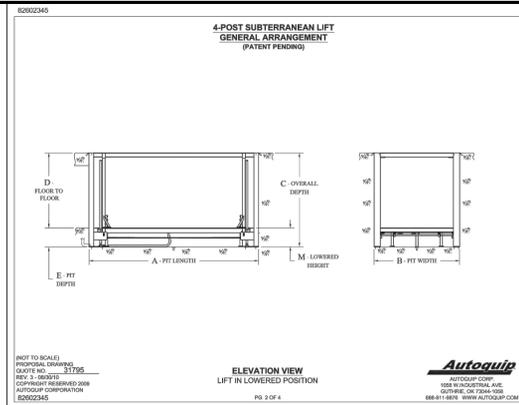
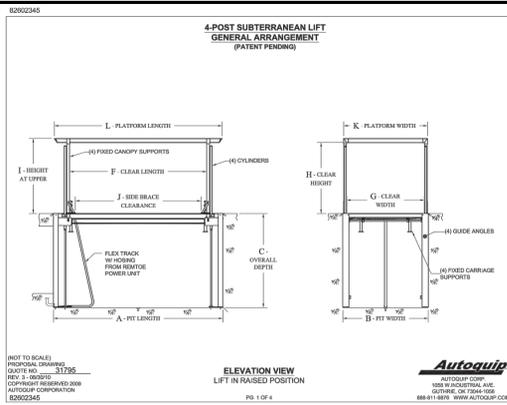


ELEVATION MIRAMONTE AVENUE

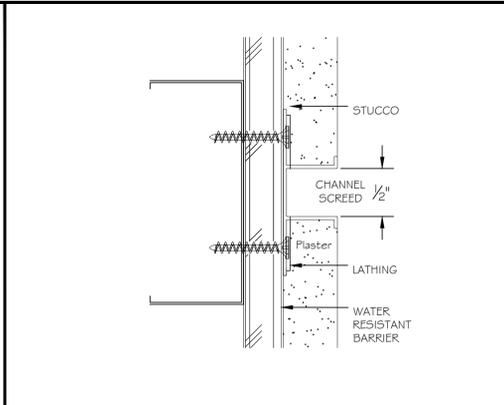
3/32":1'-0" 9

ELEVATION A STREET

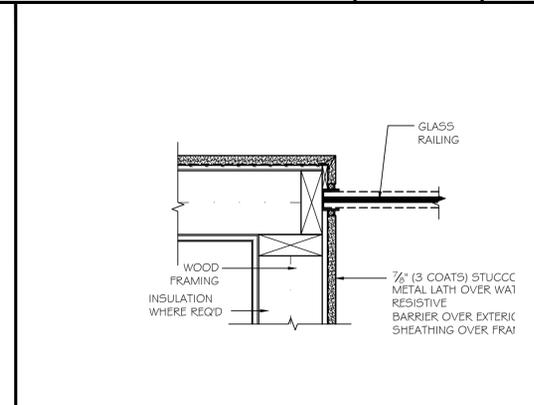
3/32":1'-0" 1



DOWNSPOUT @ STONE WALL 1":1'-0" 11

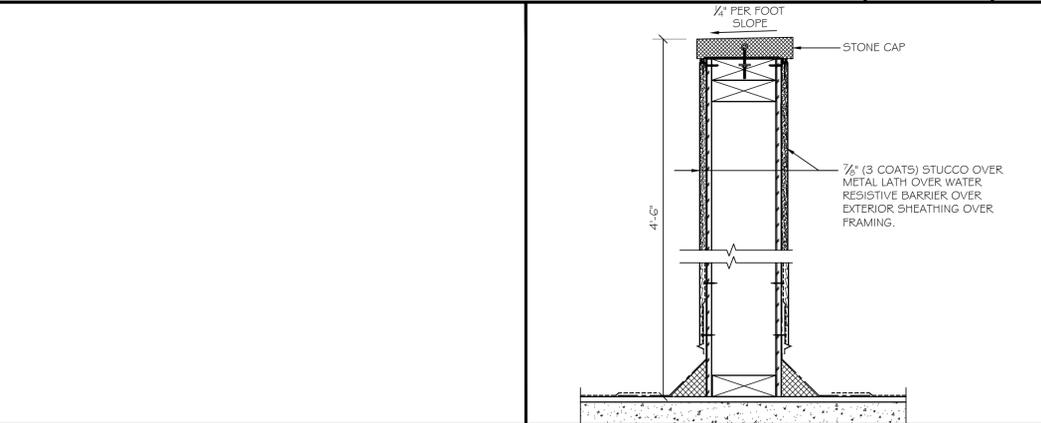


STUCCO REVEAL N.T.S. 6

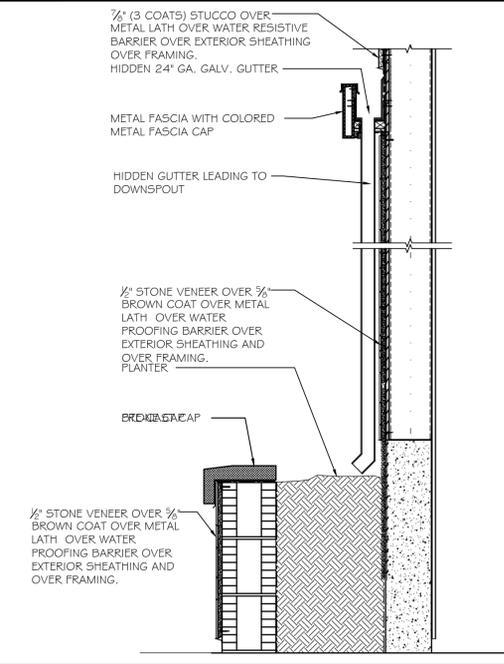


GLASS RAILING JAMB 1/2":1'-0" 2

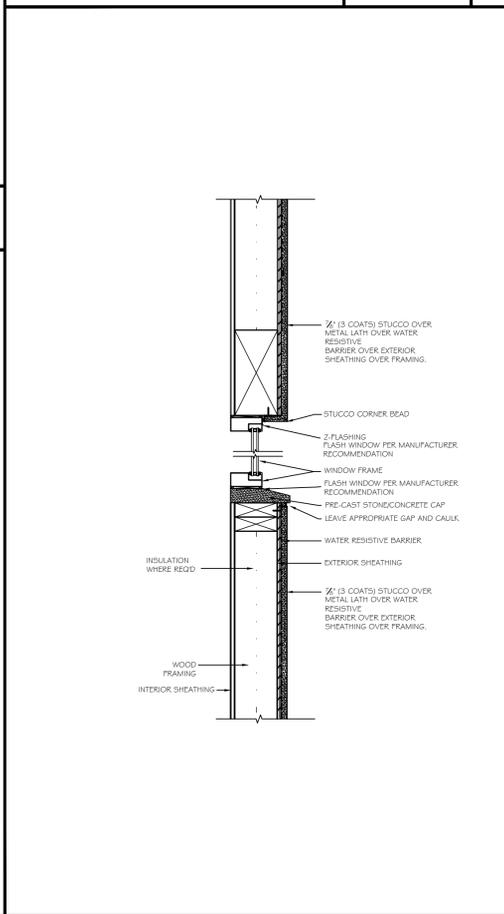
MANUFACTURER'S DETAILS OF VEHICLE LIFT N.T.S. 15



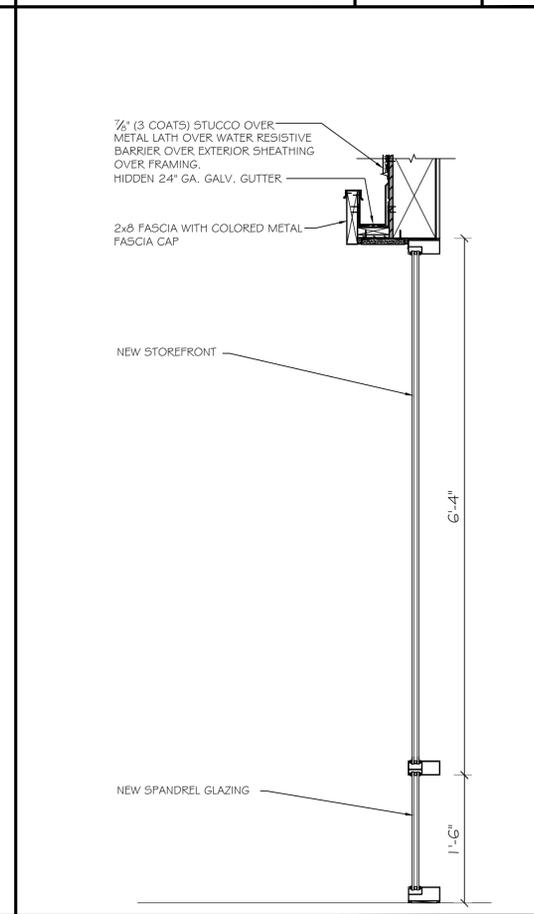
PARAPET @ ROOF 1-1/2":1'-0" 16



DOWNSPOUT @ PLANTER 1":1'-0" 12



STUCCO @ WINDOW 1":1'-0" 8



SECTION @ STOREFRONT 1":1'-0" 4

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DESIGN DEVELOPMENT
DETAILS

03-11-15

A4.2

GRADING AND DRAINAGE PLANS

NEW MIXED USE BUILDING

999 FREMONT AVE. LOS ALTOS

ABBREVIATIONS		
DESCRIPTION	ABBREVIATION	DESCRIPTION
AGGREGATE BASE (CLASS AS NOTED)	LIP	LIP OF GUTTER
ASPHALT CONCRETE	LP	LOW POINT
AREA DRAIN	MON	MONUMENT
BACK OF CURB	OC	ORIGINAL GROUND
BACK FLOW WATER PREVENTOR VALVE	PB	PULL BOX
BOTTOM OF WALL	PGEV	PG&E VAULT
BACK OF WALK	P/L	PROPERTY LINE
C&G CURB AND GUTTER	PP	POWER POLE
GARAGE FINISH FLOOR (BACK)	PERF	PERFORATED PIPE
CENTERLINE CENTERLINE SWALE	PSE	PUBLIC SERVICE EASEMENT
CLEANOUT	PVC	POLYVINYL CHLORIDE
CONTROL POINT	R/W	RIGHT OF WAY
DRIVEWAY	RCP	REINFORCED CONCRETE PIPE
DROP INLET	SD	STORM DRAIN
DETAIL	SDMH	STORM DRAIN MANHOLE
ELECTRIC	STD	STANDARD
EDGE OF PAVEMENT ELEVATION	SS	SANITARY SEWER
EUCALYPTUS TREE	SSMH	SANITARY SEWER MANHOLE
EXISTING	SW	SIDEWALK
FINISHED FLOOR	TC	TOP OF CURB
FINISH GRADE	TOS	TOP OF SLAB
FIRE HYDRANT	TOW	TOP OF WALL
FLOWLINE	TP	TOP OF PAVEMENT
FENCE	(TYP)	TYPICAL
FOG LINE	USS	UNDERGROUND SANITARY SEWER
GRADE BREAK	UST	UNDERGROUND STORM DRAIN
GARAGE FINISHED FLOOR (FRONT)	UT	UNDERGROUND TELEPHONE
GUY WIRE	UW	UNDERGROUND WATER
HIGH POINT	VCP	VITRIFIED CLAY PIPE
IRON PIPE	WL	WHITE LINE STRIPE
INVERT	WLK	WALKWAY
JOINT POLE	WM	WATER METER
JUNCTION BOX (UTILITY)	WV	WATER VALVE

NOTE:

GRADING AND DRAINAGE PLANS SHALL BE REVIEWED AND APPROVED BY THE PROJECT GEOTECHNICAL ENGINEER.

BENCHMARK

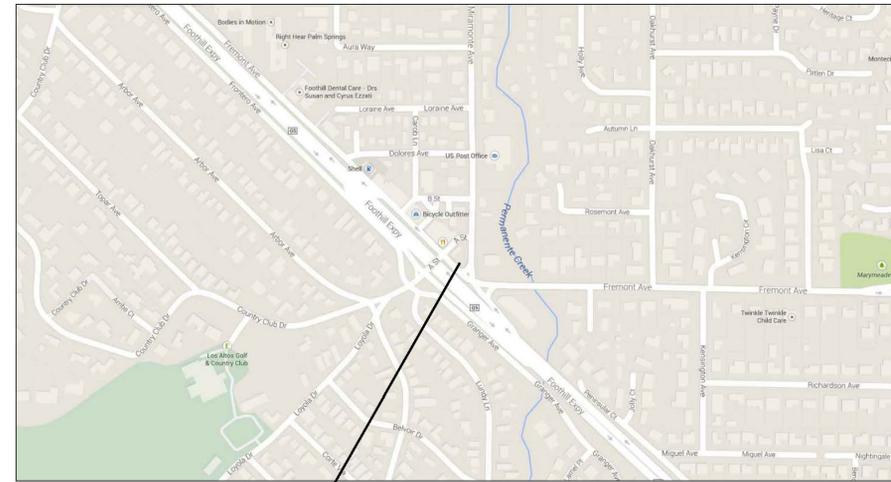
SET MAG NAIL W/ SHINER (PT.#101) ELEV= 31.55 (N.G.V.D. 27)

BASIS OF BEARINGS

THE BEARING ALONG THE CENTERLINE LINE OF S. MAIN ST. N 08°58'30" E, AS SHOWN ON MAP BK 4547, PG 404, SANTA CLARA COUNTY, WAS TAKEN AS THE BASIS OF BEARINGS.

SHEET INDEX:

- C-1 COVER SHEET/ NOTES
- C-2 GRADING AND DRAINAGE PLAN
- STRM-1 STORMWATER CONTROL PLAN
- T-1 TOPOGRAPHIC SURVEY MAP
- SP-1 EXISTING SITE PLAN
- TM TENTATIVE MAP



PROJECT SITE

LOCATION MAP

N.T.S.



ENGINEERS CIVIL ENGINEERS

1534 CAROB LANE
LOS ALTOS, CA 94024
TEL: (650) 941-8055
FAX: (650) 941-8755
E-MAIL: SMPENGINEERS@YAHOO.COM

APPLICANT:

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COVER SHEET
999 FREMONT AVE
LOS ALTOS, CA
IMPROVEMENT PLANS

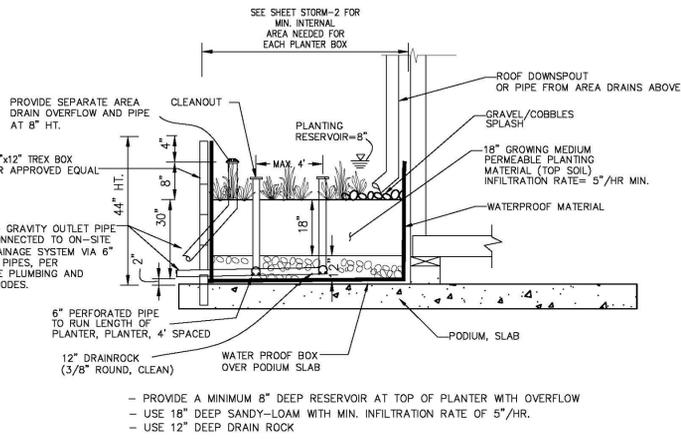
Revisions:



Date: MAY 19, 2015
Scale:
Prepared by: V.G.
Checked by: S.R.
Job #: 214091

Sheet: 1 OF 6
C-1

EXISTING	PROPOSED	DESCRIPTION
---	---	PROPERTY LINE
F	F	FILL AREA LIMIT
C	C	CUT AREA LIMIT
102	102	CONTOUR
W	W	WATER LINE
SD	SD	STORM DRAIN PIPE (SOLID)
SS	SS	SANITARY SEWER PIPE
SUB	SUB	SUBDRAIN PIPE (PERFORATED)
OH e,T,TV	OH e,T,TV	OVERHEAD UTILITIES WITH POLE
G	G	GAS LINE
E	E	ELECTRIC LINE (UNDERGROUND)
JT	JT	JOINT TRENCH
SLV	SLV	STREET LIGHT VAULT
SSCO	SSCO	SANITARY SEWER CLEANOUT
●	●	SANITARY SEWER MANHOLE
⊙	⊙	STORM DRAIN MANHOLE
⊛	⊛	ELECTROLIER
WM	WM	WATER METER
○	○	TREE WITH TRUNK
X	X	6" WOODEN FENCE
102.23	102.23	SPOT ELEVATION
○	○	TREE PROTECTION FENCE 5' TALL CHAIN LINK
---	---	GRASSY SWALE
---	---	SWALE
→	→	DIRECTION OF FLOW IN PIPE
→	→	AREA DRAIN/ INLET
→	→	OVERLAND RELEASE PATH
→	→	GRADE TO DRAIN, 2% MIN. AWAY FROM HOUSE 1% MIN. FROM PROPERTY LINE TO SWALE
⊗	⊗	(E) TREE TO BE REMOVE
↓	↓	DOWN-SPOUT
○	○	POP-UP EMITTER

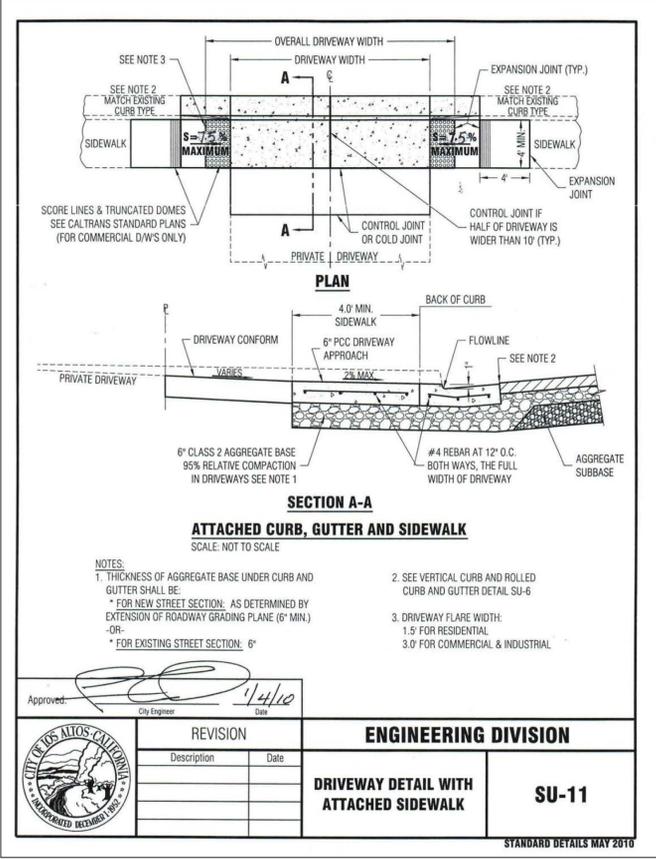
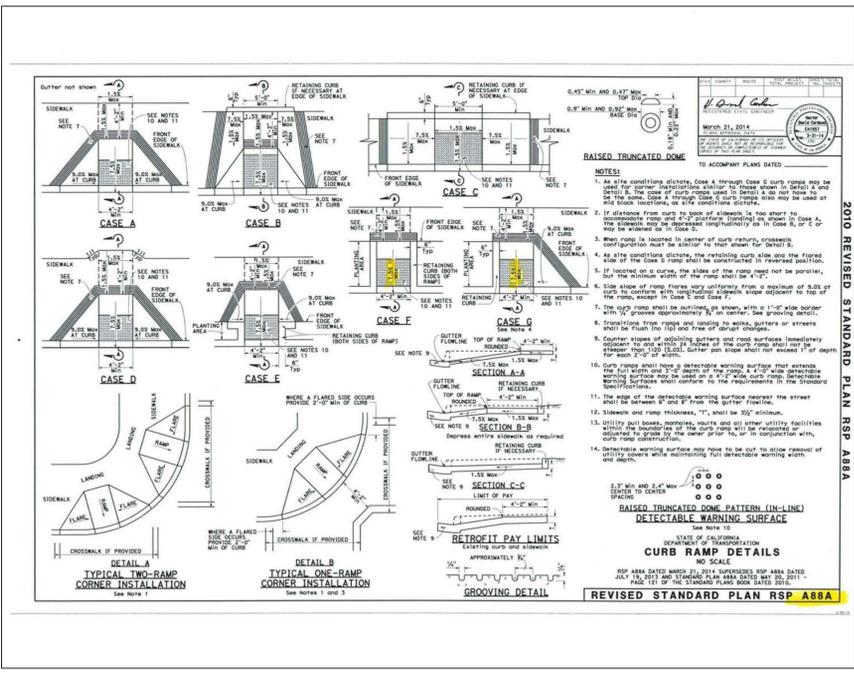


FLOW-TROUGH PLANTER BOX
NTS

FLOW-THROUGH PLANTER MAINTENANCE:

Planter boxes capture runoff from downspouts or sheet flow from plazas and paved areas. The runoff briefly floods the surface of the box and then percolates through an active soil layer to drain rock below. Typically maintenance consists of the following:

- Examine DOWNSPOUTS from the rooftops or sheet flow from paving to insure that flow to the planter is unimpeded. Remove any debris and repair any damaged pipes. Check splash blocks or rocks and repair, replace, or replenish as necessary.
- Examine the OVERFLOW pipe to make sure that it can safely convey excess flow to a storm drain. Repair or replace any damaged or disconnected piping.
- Check the UNDERDRAIN piping to make sure it is intact and unobstructed.
- Observe the STRUCTURE of the box and fix any holes, cracks, rotting, or failure.
- Check that the SOIL is at the appropriate depth to allow a 8" reservoir above the soil surface and is sufficient to effectively filter stormwater. Remove any accumulations of sediment, litter, and debris. Confirm that soil is not clogging and that the planter will drain with 3-4 hours after a storm event.
- Determine whether the VEGETATION is dense and healthy. Replace dead plants. Prune or remove any overgrown plants or shrubs that may interfere with planter operation. Clean up fallen leaves or debris and replenish mulch. Remove any nuisance or invasive vegetation.



NOTICE TO CONTRACTORS
CONTRACTOR TO NOTIFY U.S.A. (UNDERGROUND SERVICE ALERT) AT 800-227-2600 A MINIMUM OF 2 WORKING DAYS BEFORE BEGINNING UNDERGROUND WORK FOR VERIFICATION OF THE LOCATION AND DEPTH OF UNDERGROUND UTILITIES.

1-800-227-2600



APPLICANT:

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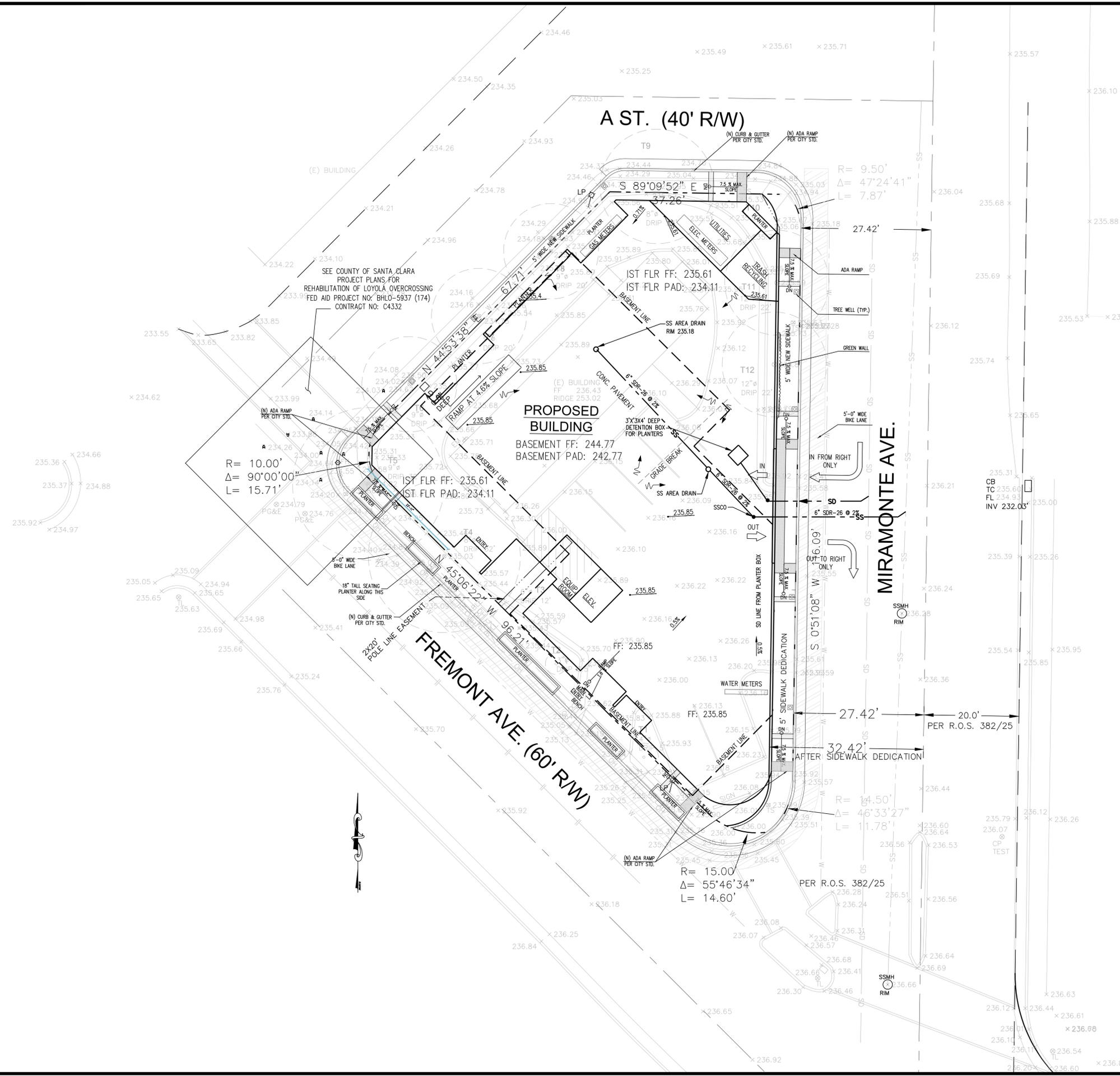
GRADING AND DRAINAGE PLAN
999 FREMONT AVE
LOS ALTOS, CA
IMPROVEMENT PLANS

Revisions:



Date: MAY 19, 2015
Scale: 1"=10'
Prepared by: V.G.
Checked by: S.R.
Job #: 214091

Sheet: 2 OF 6
C-2



STREET IMPROVEMENT NOTE:
REMOVE AND REPLACE ENTIRE CITY SIDEWALK, CURB AND GUTTER ALONG MIRAMONTE AVE., FREMONT AVE. AND A STREET PER CITY STANDARD DETAILS. THE MINIMUM WIDTH OF ALL SIDEWALKS SHALL BE FIVE FEET.
ANY A STREET IMPROVEMENTS TO BE DONE PER ENGINEERING DIVISION. RELOCATE THE CROSSWALK AT ALL CORNERS TO CONFORM TO THE NEW ADA RAMPS. SEE DETAIL ON C-1.
ALL DRIVEWAY APPROACH PER CITY'S COMMERCIAL DRIVEWAY STANDARDS WITH TRUNCATED DOMES. SEE DETAIL ON SHEET C-1.

NOTE:
1. REMOVE AND REPLACE ENTIRE CITY SIDEWALK, CURB AND GUTTER ALONG MIRAMONTE AVE., FREMONT AVE. AND A STREET PER CITY STANDARD DETAILS. THE MINIMUM WIDTH OF ALL SIDEWALKS SHALL BE FIVE FEET.
2. DEVELOPER TO BE RESPONSIBLE FOR A STREET IMPROVEMENTS REQUIRED BY ENGINEERING DIVISION.

LEGEND:

---	PROPERTY LINE
---	NEW PROPERTY LINE
---	EXISTING LOTS
---	CENTERLINE
---	EASEMENT LINE
---	SANITARY SEWER LINE
---	STORM DRAIN LINE
---	PG&E LINE
---	OVERHEAD POWER LINE

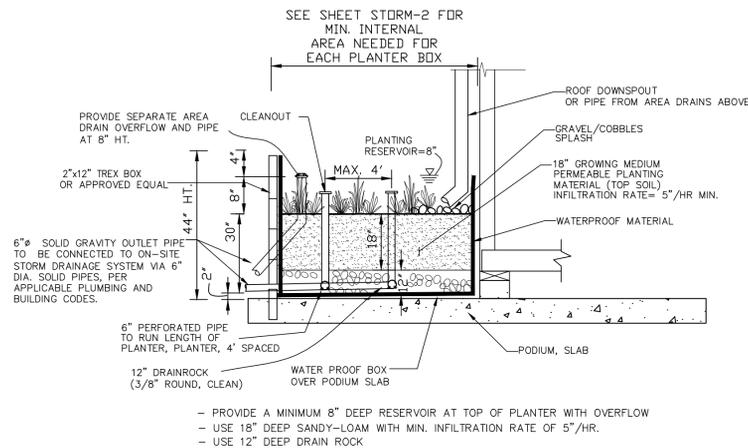
Base Food Elevation (BFE)
Base Food Elevation (BFE) IS 32.50 FT.

STORMWATER CONTROL PLAN

NEW MIXED USE BUILDING

999 FREMONT AVE.

LOS ALTOS, CA



FLOW-THROUGH PLANTER BOX

NTS

FLOW-THROUGH PLANTER MAINTENANCE:

Planter boxes capture runoff from downspouts or sheet flow from plazas and paved areas. The runoff briefly floods the surface of the box and then percolates through an active soil layer to drain rock below. Typically maintenance consists of the following:

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REFERENCED ASSUMED BENCHMARK:

REFERENCED SANTA CLARA VALLEY WATER DISTRICT B.M.:
B.M. 1016 EL: 237.34'(NAVD88)

Compliance with NPDES Permit Provision C.3:

The San Francisco Bay Regional Water Quality Control Board (SFRWQCB) incorporated updated requirements into Santa Clara County's National Pollution Discharge Elimination System (NPDES) Permit in August 06. These updated stormwater quality control requirements are predominantly in the category of new development discharge controls. The Permit requires that permanent, post-construction stormwater quality control measures be implemented as part of development projects.

Updated stormwater quality control measures include:
- Source Control Measures
- Site Design Measures
- Treatment Control Measures

Beginning August 15, 2006, all projects creating or replacing 10,000 sq. ft. or more of impervious surface area must design and install a permanent post-construction stormwater treatment facility on the site. The system must be design and installed according to numeric sizing criteria.

All projects, regardless of size that create or replace impervious surface may be required to install stormwater quality controls to the maximum extent practicable.

Source Control Measures

COVERED GARAGE/ PARKING WITH AREA DRAIN CONNECTED TO SANITARY SEWER

Site Design Measures

EXTENSIVE BENEFICIAL LANDSCAPE AREAS AS GREEN ROOF.

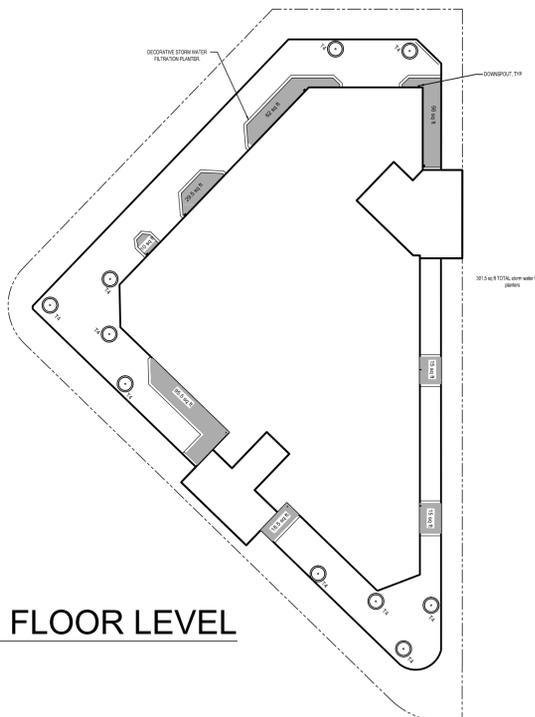
Storm water Treatment Measures:

- FLOW-THROUGH PLANTERS

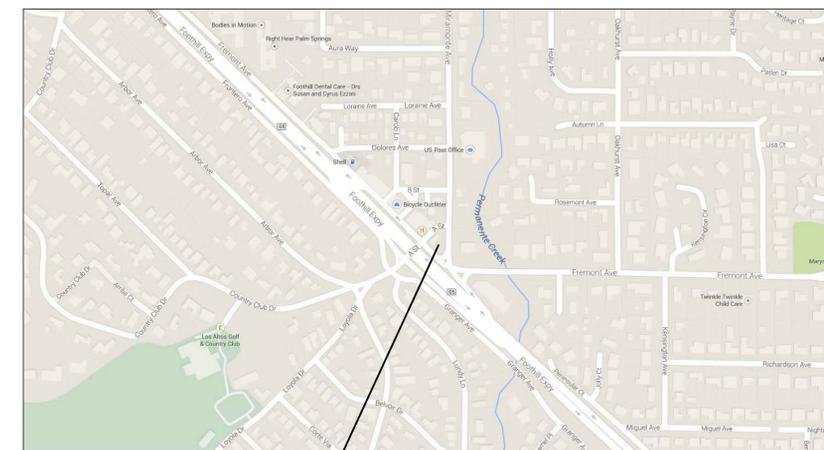
ADDITIONAL INFORMATION:

- NAME AND LOCATION OF RECEIVING WATER BODY: CITY OF LOS ALTOS DRAINAGE SYSTEM, MIRAMONTE AVE., FOLLOWING TO BAY.
- POLLUTANT SOURCES: PARKING LOT, ROOF, TRASH ENCLOSURE.
- NATURAL WATER COURSES NEAR SITE: NONE.
- SOIL TYPE: STIFF SILTY CLAY WITH GRAVEL, BROWN.
- DEPTH TO GROUND WATER: NO GROUND WATER WITHIN 20' DEPTH FOUND.
- FLOOD ZONE: X, (OUTSIDE OF 100 YR. FLOOD ZONE)

PLANTER AREA ON STREET AND 3RD FLOOR)
406.5 SF (5.13 % OF TOTAL IMPERVIOUS AREA)
TOTAL IMPERVIOUS AREA: 7,929 SF

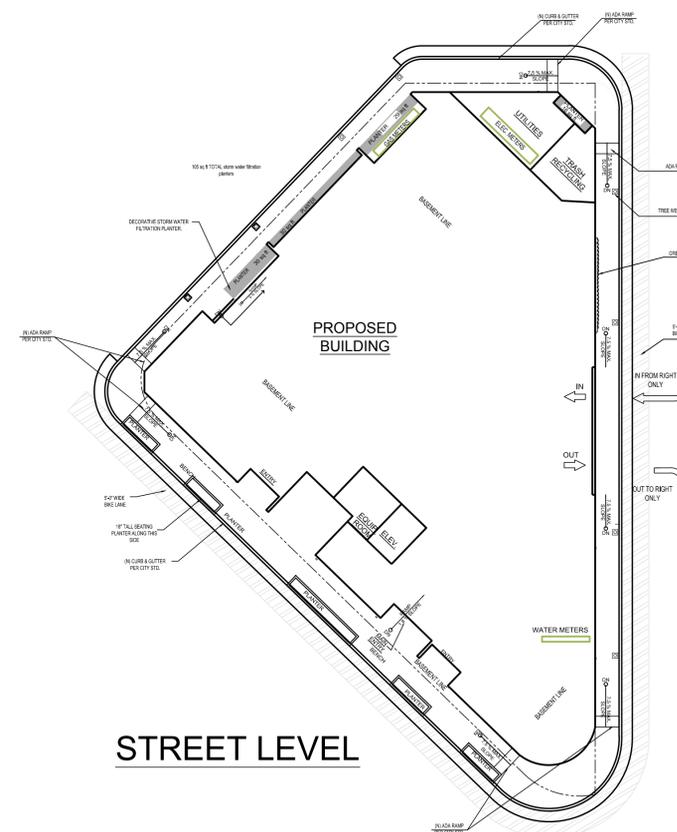


3RD FLOOR LEVEL



PROJECT SITE

LOCATION MAP
N.T.S.



STREET LEVEL

SMP



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CIVIL ENGINEERS

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LOS ALTOS, CA 94024
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FAX: (650) 941-8755
E-MAIL: SMPENGINEERS@YAHOO.COM

APPLICANT:

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STORMWATER CONTROL PLAN
999 FREMONT AVE
LOS ALTOS, CA
IMPROVEMENT PLANS

Revisions:

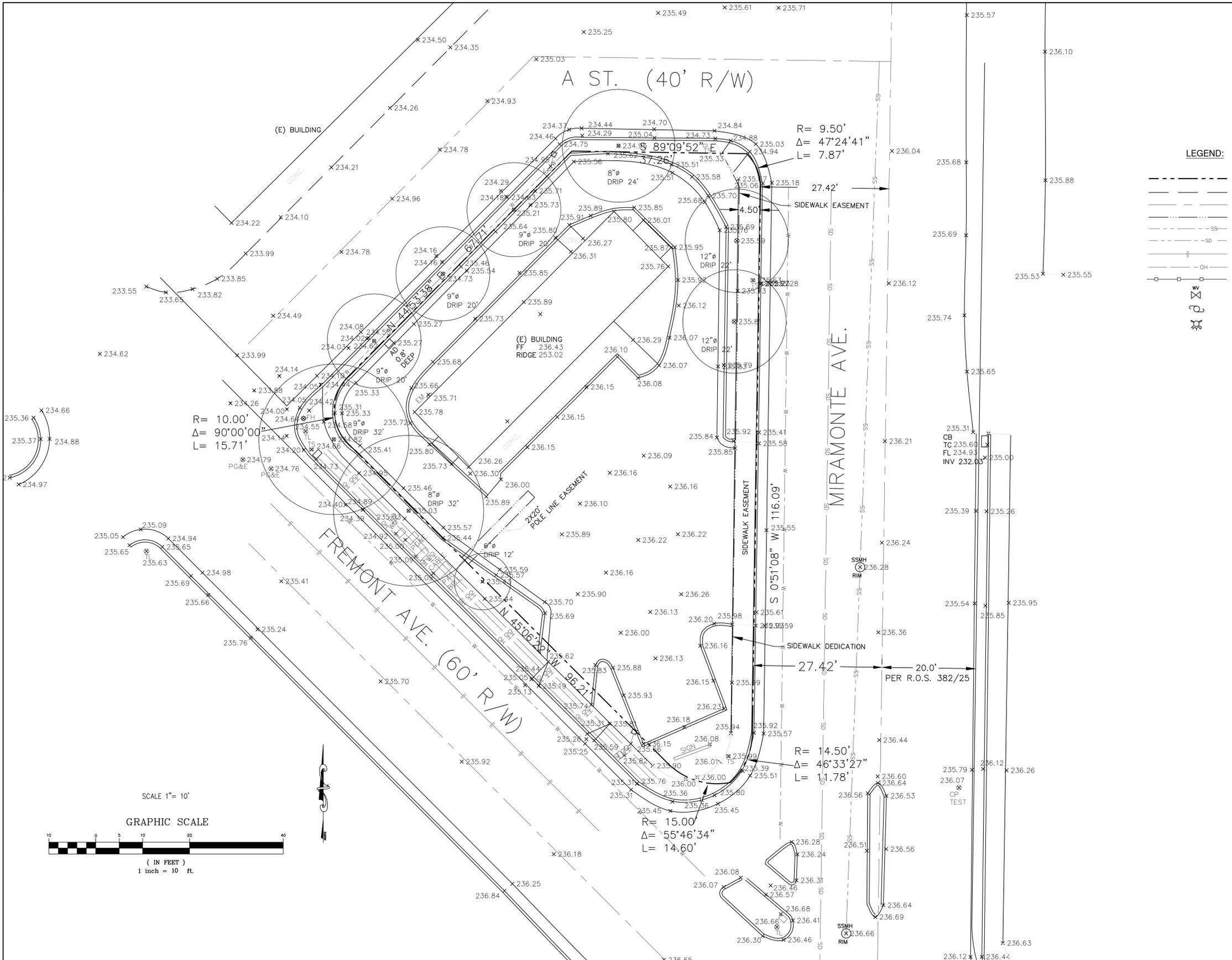


Date: MAY 19, 2015
Scale:

Prepared by: V.G.
Checked by: S.R.
Job #: 214091

Sheet: 3 OF 6

STORM-1



LEGEND:

- PROPERTY LINE
- - - EXISTING LOTS
- CENTERLINE
- EASEMENT LINE
- SANITARY SEWER LINE
- STORM DRAIN LINE
- PG&E LINE
- OVERHEAD POWER LINE
- WOOD FENCE
- WATER VALVE
- POWER POLE
- FIRE HYDRANT

ABBREVIATIONS:

- AC ASPHALT
- AD AREA DRAIN
- ANC ANCHOR
- C&G CURB AND GUTTER
- CB CATCH BASIN
- EM ELECTRIC METER
- DW DRIVEWAY
- EB ELECTRIC BOX
- EP EDGE OF PAVEMENT
- FH FIRE HYDRANT
- GM GAS METER
- GV GAS VALVE
- LP LIGHT POLE
- P.U.E. PUBLIC UTILITY EASEMENT
- PP POWER POLE
- S.D.E. STORM DRAINAGE EASEMENT
- SDMH STORM DRAINAGE MANHOLE
- SSMH SANITARY SEWER MANHOLE
- TL TRAFFIC LIGHT
- TS TRAFFIC SIGN
- CO CLEAN OUT
- VG VALLEY GUTTER
- WM WATER METER

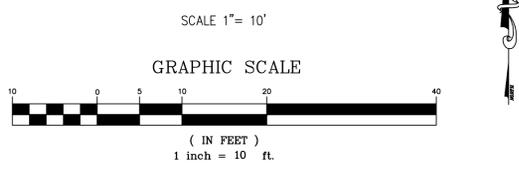
DISCLAIMER:
 SMP ENGINEERS OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF ELECTRONIC COPIES OF THIS PLAN.

NOTE:
 THIS MAP REPRESENTS TOPOGRAPHY OF THE SURFACE FEATURES ONLY. UNLESS SPECIFIED ON THIS MAP, LOCATIONS OF THE UNDERGROUND UTILITIES ARE NEITHER INTENDED NOR IMPLIED. FOR THE LOCATIONS OF UNDERGROUND UTILITIES CALL "USA" (1-800-642-2444). SURFACE FEATURES ARE LOCATED BY MEANS OF A STATION AND OFFSET FROM THE CONTROL LINE.

BASIS OF BEARINGS:
 THE BEARING S 0°51'08" W OF THE CENTERLINE OF MIRAMONTE AVE., AS SHOWN UPON CERTAIN RECORD OF SURVEY MAP, RECORDED IN BOOK 487 OF MAPS, AT PAGE 49, WAS TAKEN AS BASIS OF BEARINGS FOR ALL BEARINGS SHOWN HEREON.

REFERENCED ASSUMED BENCHMARK:
 REFERENCED SANTA CLARA VALLEY WATER DISTRICT B.M.:
 B.M. 1016 EL: 237.34'(NAVDB8)

- NOTES:**
- ALL DIMENSIONS ARE GIVEN IN FEET AND DECIMALS THEREOF.
 - THE GROSS AREA OF LAND OF RECORD IS 7929 SQ. FT. ±.
 - THE SURVEY WAS BASED ON A GRANT DEED DOC.# 13-501043 BY FIDELITY NATIONAL TITLE CO. DATED 8/15/2013, RECORDED IN SANTA CLARA COUNTY.
 - ALL EXISTING BUILDINGS ARE WOOD.
 - FOR PRECISE SPECIES OF TREES A CERTIFIED ARBORIST SHALL BE CONSULTED.
 - THIS DRAWING REPRESENTS A TOPOGRAPHIC SURVEY PREPARED IN CONFORMANCE WITH THE REQUIREMENTS OF THE LAND SURVEYORS ACT. THE PROPERTY LINES SHOWN HEREON ARE COMPILED FROM RECORD DATA AND REPRESENT THE BEST GRAPHICAL FIT BETWEEN RECORD INFORMATION AND THE TOPOGRAPHICAL FEATURES SURVEYED AND SHOULD NOT BE RELIED UPON OR USED FOR ANY OTHER PURPOSES. PURSUANT TO THE CLIENT'S DIRECTION A BOUNDARY SURVEY WAS NOT PERFORMED AT THIS TIME WHICH MAY HAVE DETERMINED THE ACTUAL PROPERTY LINES.



999 FREMONT AVENUE,
 LOS ALTOS, CA
 APN: 189-15-092



SMP ENGINEERS
 CIVIL ENGINEERS—LAND SURVEYORS
 1534 Carob Lane Los Altos, CA 94024
 Tel. (650) 941-8055 Fax (650) 941-8755

Scale: 1" = 10'
 Prepared by: S.P.
 Checked by: S.R.
 Date: 12/23/2013
 Project No: 213176

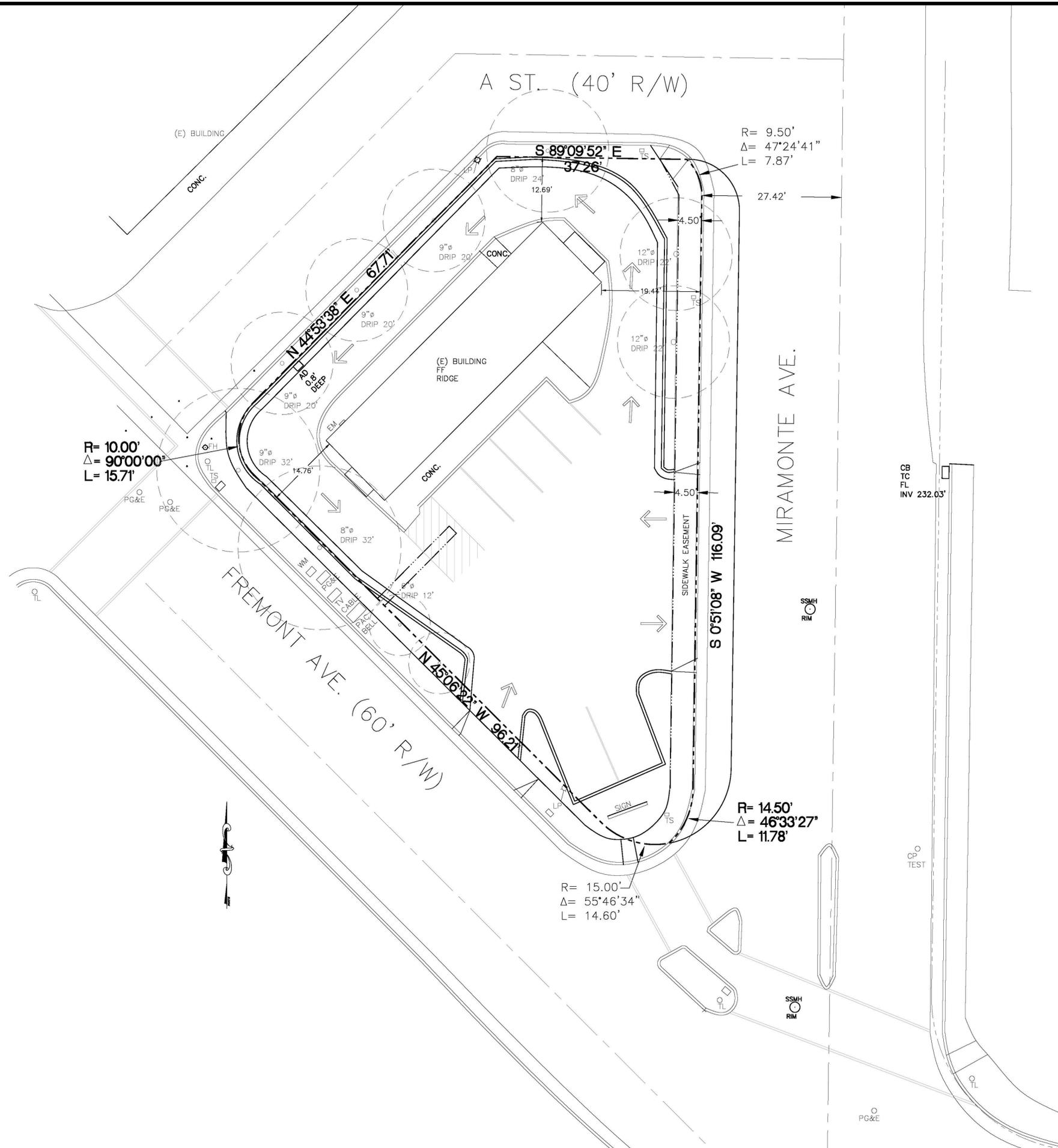
TOPOGRAPHIC SURVEY MAP

Sheet No:

T-1

REVISIONS	DESIGN BY	DESIGN DATE	CITY APPR.	APPR. DATE

CITY OF LOS ALTOS



**ENGINEERS
CIVIL ENGINEERS**

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LOS ALTOS, CA 94024
TEL: (650) 941-8055
FAX: (650) 941-8755
E-MAIL: SMPENGINEERS@YAHOO.COM

APPLICANT:

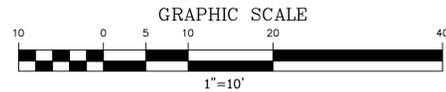
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**EXISTING SITE PLAN
999 FREMONT AVE
LOS ALTOS, CA
IMPROVEMENT PLANS**

Revisions:



Date: MAY 19, 2015
Scale:
Prepared by: V.G.
Checked by: S.R.
Job #: 214091



TENTATIVE MAP

MIXED USE

999 FREMONT AVE,
LOS ALTOS CA 94024-6098

ALL THAT CERTAIN REAL PROPERTY IN THE CITY OF LOS ALTOS, COUNTY OF SANTA CLARA, STATE OF CALIFORNIA, DESCRIBED AS FOLLOWS:

LOTS 1,2,3 AND 4, IN BLOCK 1, AS SHOWN UPON MAP ENTITLED, "LOYOLA TOWNSITE, PART OF LOT NO. 3 SECTION 4, TP. 7 SO. RANCHO 2#M.D.B. & M. SANTA CLARA COUNTY, CALIFORNIA", WHICH SAID MAP WAS FILED FOR RECORD IN THE OFFICE OF THE RECORDER OF THE COUNTY OF SANTA CLARA, STATE OF CALIFORNIA, IN VOL. "V" OF MAPS, PAGE 28.

CITY OF LOS ALTOS
SCALE: 1"=10'

CALIFORNIA
MAY, 2015

**SMP ENGINEERS
CIVIL ENGINEERS**

1534 CAROB LANE
LOS ALTOS, CA 94024

GENERAL NOTES

- OWNERS :
GREGG BUNKER
1900 CAMDEN AVE.
SAN JOSE, CA 95124
PH: (408) 558-3600
- SUBDIVIDERS/DEVELOPERS:
GREGG BUNKER
1900 CAMDEN AVE.
SAN JOSE, CA 95124
PH: (408) 558-3600
- APPLICANT:
SAME AS ABOVE.
- EXISTING/PROPOSED ZONING: COMMERCIAL
- EXISTING APN: 189-15-092
- EXISTING USE: COMMERCIAL
- PROPOSED USE: MIXED USE
- GENERAL PLAN: COMMERCIAL
- EXISTING WELLS: NONE
- FLOOD ZONE: X
- STREETS: THE SIDEWALK AND THE DRIVEWAY ENCROACHMENT SHALL BE REPLACED.
- EXISTING USE OF ADJACENT PROPERTIES: COMMERCIAL
- WATER: CALIFORNIA WATER COMPANY
- FIRE PROTECTION: LOS ALTOS/SANTA CLARA COUNTY FIRE DEPARTMENT
- STORM/ SANITARY SEWER: CITY OF LOS ALTOS
- POWER AND GAS: PACIFIC GAS AND ELECTRIC
- TELEPHONE: AT&T
- STREET TREES: ANY NEW STREET TREES IN PUBLIC RIGHT-OF-WAY TO BE PLANTED IN ACCORDANCE WITH CITY OF LOS ALTOS STANDARDS. THIS SUBDIVISION WILL CONFORM TO THE STREET PLAN OF THE CITY OF LOS ALTOS. NO ORDINANCE SIZE TREES WILL BE REMOVED.
- AREA TO BE SUBDIVIDED: APPROXIMATELY 0.18 ACRES (7,928.81 SQUARE FEET)
- CONTOUR ELEVATION: LOCAL DATUM AND MONUMENTS
- ALL DIMENSIONS ARE EXACT.
- ALL EXISTING STRUCTURES ON THE SITE SHALL BE REMOVED PRIOR TO MAP RECORDATION.
- NO NEW STREET NAME .

EXISTING TREES TO BE REMOVED

SYM	BOTANICAL NAME	COMMON NAME	HEALTH STATUS
T1	PYRUS kawakami	Evergreen Pear	Poor- Stunted growth
T2	SEQUOIA sempervirens	Coast Redwood	Poor- Stunted growth, Die-back
T3	PYRUS kawakami	Evergreen Pear	Sparse leaves
T4	PYRUS kawakami	Evergreen Pear	Good- Some blight die back
T5	PYRUS kawakami	Evergreen Pear	Good
T6	PYRUS kawakami	Evergreen Pear	Good
T7	PYRUS kawakami	Evergreen Pear	Good
T8	PYRUS kawakami	Evergreen Pear	Good
T9	PYRUS kawakami	Evergreen Pear	Good
T10	PYRUS kawakami	Evergreen Pear	Good-sparse
T11	PYRUS kawakami	Evergreen Pear	Good
T12	PYRUS kawakami	Evergreen Pear	Good

Project Description/Improvements

Construction of a new mixed-use residential and commercial building containing:
I- 2 commercial spaces, automobile parking for tenants and homeowners, a residential lobby, bicycle storage, utility meters, trash/recycling room and site improvements at the ground floor;

II- A total of 4 residential units on the 2nd and 3rd floors;

III- Mechanical units, solar panels (Future) at the roof;

STATEMENTS and TABLES

GENERAL PLAN:	COMMERCIAL
EXISTING ZONING:	CN
APN:	189-15-092
SITE AREA:	7928.81 SQUARE FEET (0.18 ACRES)

PROPOSED LOTS	GROSS
TOTAL	7928.81 sf 0.18 acres

EXISTING PARCEL	SQ. FT.	ACRES
PARCEL	7928.81	0.18

UNIT COUNT

TYPE	COUNT
• 3BD + 2.5BA	1
• 3BD + 3.5BA	1
• 3BD + 3BA	1
• 5BD + 4.5BA	1

GROSS AREA

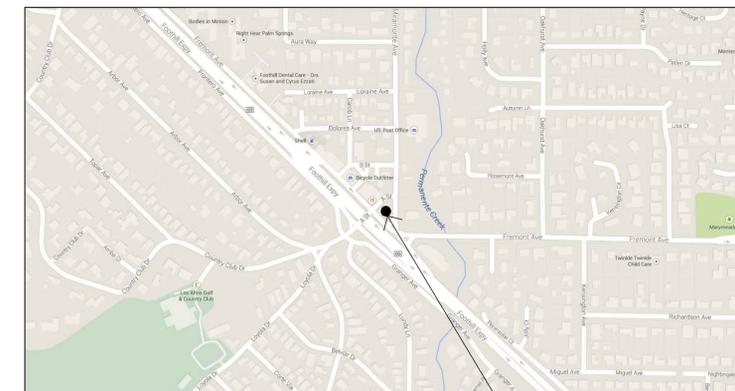
BASEMENT	
• RETAIL STORAGE	484 SQ. FT.
• BASEMENT	3200 SQ. FT.
• PARKING AREA	773 SQ. FT.
TOTAL	4457 SQ. FT.
GROUND FLOOR	
• RETAIL 1	681 SQ. FT.
• RETAIL 2	627 SQ. FT.
• PARKING AREA	3805 SQ. FT.
• BIKE & PLANTERS	382 SQ. FT.
• COMMON AREA	1343 SQ. FT.
TOTAL	6838 SQ. FT.
SECOND FLOOR	
• UNIT 1	1982 SQ. FT.
• UNIT 2	1811 SQ. FT.
• UNIT 3	1662 SQ. FT.
• BALCONIES	503 SQ. FT.
• COMMON AREA	880 SQ. FT.
TOTAL	6838 SQ. FT.
THIRD FLOOR	
• UNIT 4	3366 SQ. FT.
• BALCONIES	1832 SQ. FT.
• COMMON AREA	454 SQ. FT.
TOTAL	5652 SQ. FT.

LEGEND AND ABBREVIATIONS:

	STREET CENTER LINE
	EXTERIOR BOUNDARY LINE
	NEW LOT LINE
	FOUND IRON PIPE
	EASEMENT LINE
	PUBLIC UTILITY EASEMENT

BASIS OF BEARINGS:

THE BEARING S 0°51'08" W OF THE CENTERLINE OF MIRAMONTE AVE., AS SHOWN UPON CERTAIN RECORD OF SURVEY MAP, RECORDED IN BOOK 487 OF MAPS, AT PAGE 49, WAS TAKEN AS BASIS OF BEARINGS FOR ALL BEARINGS SHOWN HEREON.



LOCATION MAP
N.T.S.

PROJECT SITE



1534 CAROB LANE
LOS ALTOS, CA 94024
TEL: (650) 941-8055
FAX: (650) 941-8755
E-MAIL: SMPENGINEERS@YAHOO.COM

OWNER:

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SMP ENGINEERS
CIVIL ENGINEERS

TENTATIVE MAP
999 FREMONT AVE,
LOS ALTOS, CA
APN: 189-15-092



Revisions:

Date:
MAY 19, 2015
Scale:
1" = 10'
Designed by:
S.R.
Drawn by:
S.P.
Job #:
214091

Sheet:

DATE	REVISION
10.29.2014	DESIGN REVIEW
11.12.2014	DESIGN REVIEW
02.17.2015	DESIGN REVIEW
04.3.2015	DESIGN REVIEW

M-DESIGNS ARCHITECTS

M-DESIGNS ARCHITECTS
4546 EL CAMINO REAL,
STE 223, LOS ALTOS, CA
www.mdesignsarchitects.com
info@mdesignsarchitects.com
PH: 650-565-9036 FAX: 949-623-7869

DRAKE DESIGN ASSOCIATES

51 UNIVERSITY AVENUE
SUITE 1
LOS GATOS, CA 95031
Tel: 408.688.7651
WWW.DRAKEDESIGNASSOC.COM

LOYOLA CORNERS
999 FREMONT AVE
LOS ALTOS, CA 94024

DESIGN DEVELOPMENT
LANDSCAPE PLAN
Ground Floor Streetscape

L- 1.1

SHEET INDEX

- L-1.1 GROUND FLOOR STREETScape
- L-1.2 PLANTING LEGEND, DETAILS, & SECOND LEVEL POTS
- L-1.3 THIRD LEVEL PLANTERS & FILTRATION DETAIL
- L-1.4 ELEVATIONS

PRELIMINARY TREE SYMBOLS- STREETScape & 2nd & 3rd LEVELS

SYM	BOTANICAL NAME
T1	PRUNUS serrata 'Kwanzan' (Flowering Cherry Tree)
T3	PYRUS calleryana 'Redspire' (Fruitless Flowering Pear)
T2	LOPHOSTEMON conferta (Brisbane Box) PRUNUS 'Kwanzan' (Flowering Cherry)
T4	AGONIS 'Alter Dark' (Dark Purple Peppermint Willow)
T5	ACER palmatum (Japanese Maple)
V1	SOLANUM JASMINOIDES FOR GREEN WALL EFFECTS ON CABLE WIRE TRELLISES

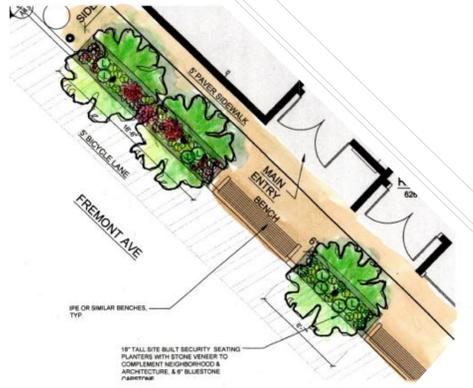
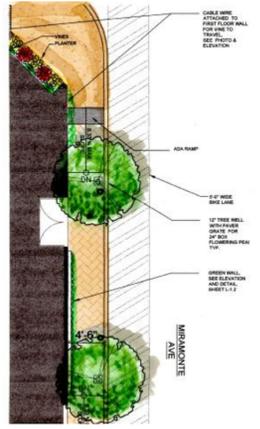
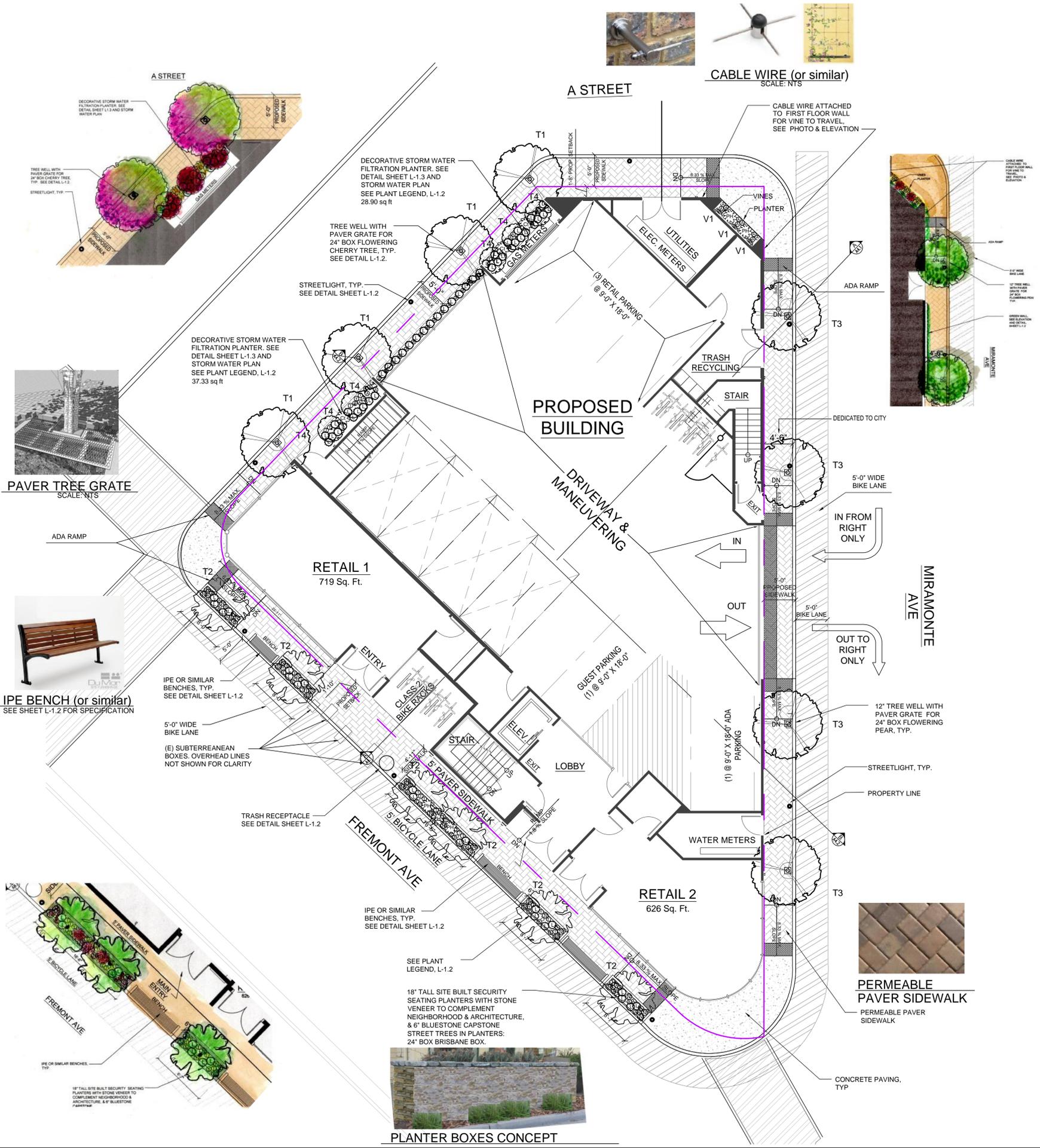
PAVING SYMBOLS

- SIDEWALK- CALSTONE PERMEABLE 4 x 8- BROWN BEIGE CHARCOAL, HERRINGBONE
- CONCRETE WALK- DAVIS BAYOU, BROOM FINISH

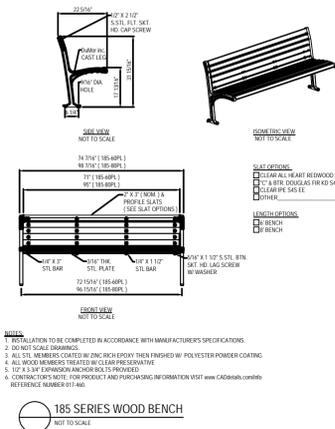
STREET TREES



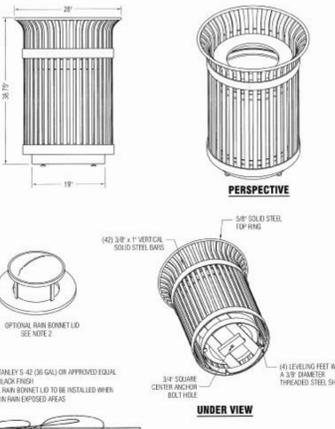
SHRUBS, PERENNIALS, GROUNDCOVERS



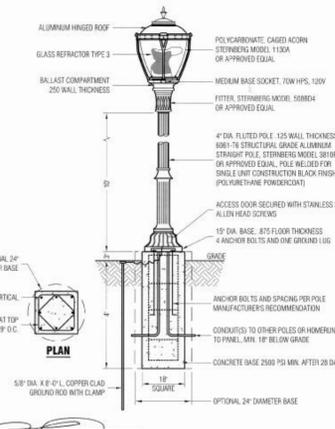
PLANTER BOXES CONCEPT



DET7 IPE BENCH (or similar)
L-2

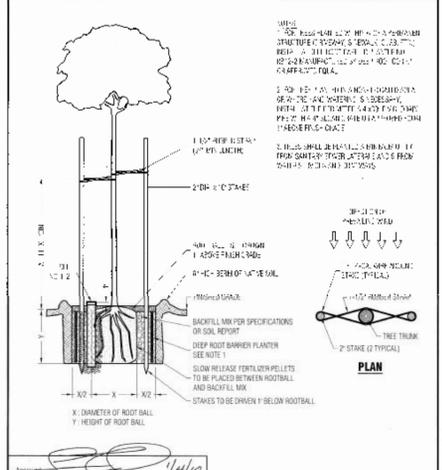


DET6 TRASH RECEPTACLE
L-2

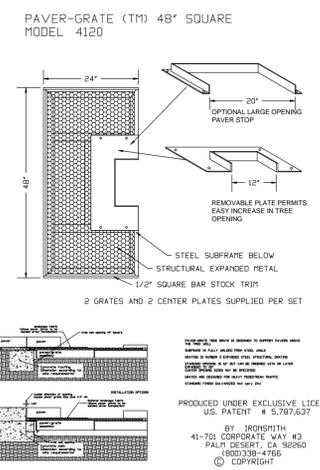


DET5 STREET LIGHT
L-2

REVISION		ENGINEERING DIVISION	
Description	Date		
		LITTER RECEPTACLE	LI-4



DET2 TREE PLANTING
L-2



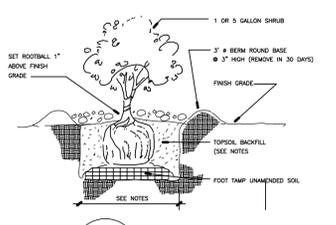
DET3 PAVER TREE GRATE
L-2

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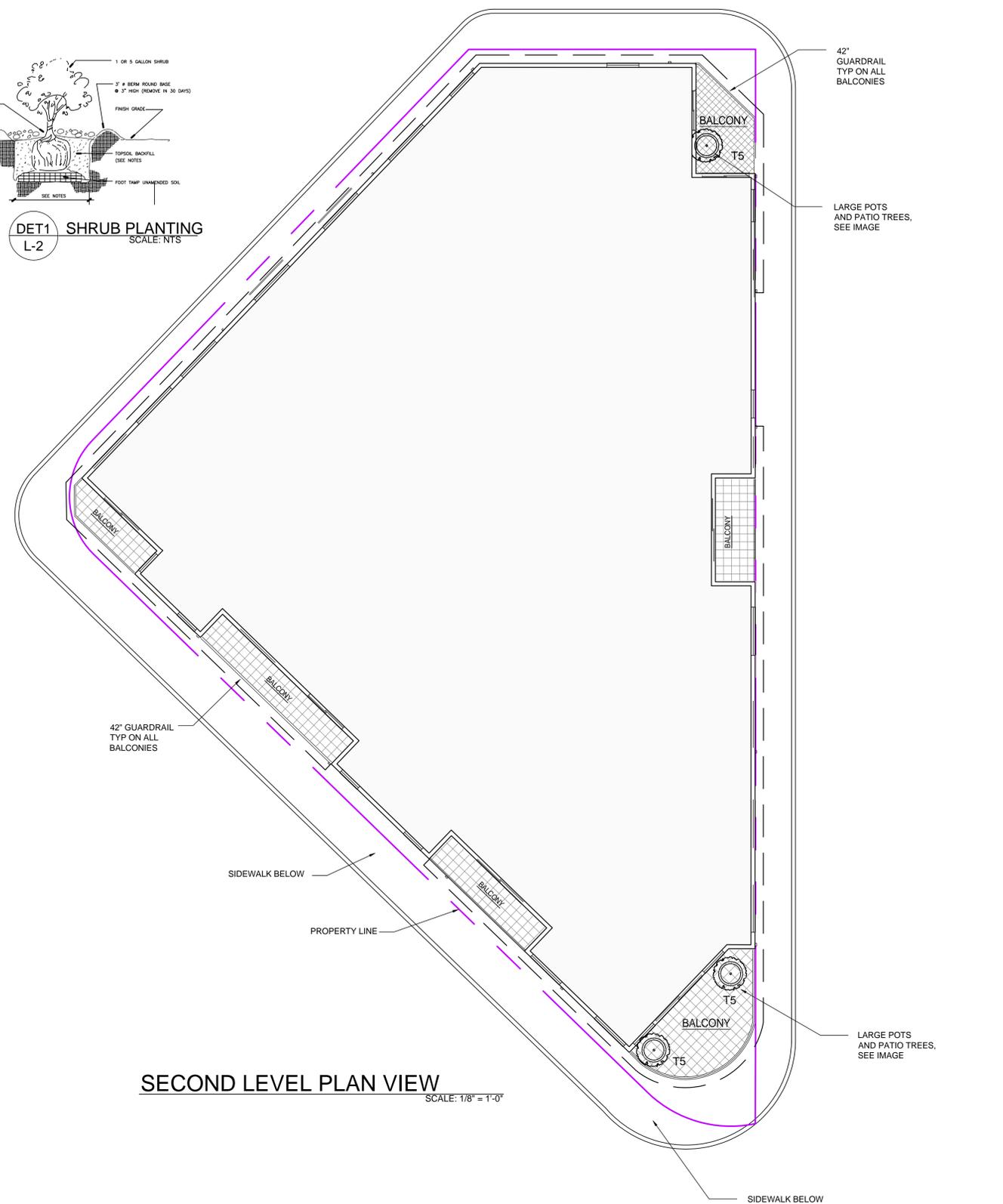
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DET4 SECOND & THIRD LEVEL TREE PLANTERS
L-2



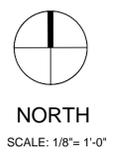
DET1 SHRUB PLANTING
L-2



SECOND LEVEL PLAN VIEW
SCALE: 1/8" = 1'-0"

PLANT LEGEND (1st & 2nd Levels)

SYM	BOTANICAL NAME	COMMON NAME	QTY	SIZE	PLANT TYPE	COMMENTS
TREES						
T1	PRUNUS serrata 'Kwanzan'	Kwanzan Flowering Cherry	4	24" Box	Low Water Use	Deciduous
T2	LOPHOSTEMON conferta	Brisbane Box	6	24" Box	Low Water Use	Evergreen
T3	PYRUS calleryana 'Holmford'	Flowering Pear	4	24" Box	Low Water Use	Deciduous
T4	AGONIS flexuosa 'After Dark'	Dark Purple Leaf Peppermint Willow	6	15 gal	Low Water Use	Natural Form
T5	ACER palmatum 'Bloodgood'	Red Japanese Maple	3	24" Box	Low Water Use	2nd Floor
V1	SOLANUM jasminoides	Potato Vine	3	5 Gal	Low Water Use	Staked
SHRUBS, GRASSES, PERENNIALS, GROUNDCOVERS						
A	AEONIUM Varieties	Assorted Aeonium	47	1 gal	Low Water Use	
Ct	CHONDRPOTALUM tectorum	Cape Rush	4	5 gal	Low Water Use	
L	LOMANDRA 'Breeze'	NCN	67	1 gal	Low Water Use	
N	NEPETA fassenii	Catmint	16	1 gal	Low Water Use	
Ph	PHORMIUM 'Platt's Black' or similar	Platt's Black New Zealand Flax	5	5 gal	Low Water Use	
S	SENECIO serpens	Creeping Chalk Fingers	10	1 gal	Low Water Use	
T	THYMUS citriodorus 'Lemon Variegata'	Variegated Lemon Thyme	40	4" pot	Low Water Use	



DATE	REVISION
10.29.2014	DESIGN REVIEW
11.12.2014	DESIGN REVIEW
02.17.2015	DESIGN REVIEW
04.3.2015	DESIGN REVIEW



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LOYOLA CORNERS
999 FREMONT AVE
LOS ALTOS, CA 94024

DESIGN DEVELOPMENT
LANDSCAPE PLAN
Planting Legend, Details
& Second Level Pots

L- 1.2

DATE	REVISION
10.29.2014	DESIGN REVIEW
11.12.2014	DESIGN REVIEW
02.17.2015	DESIGN REVIEW
04.3.2015	DESIGN REVIEW



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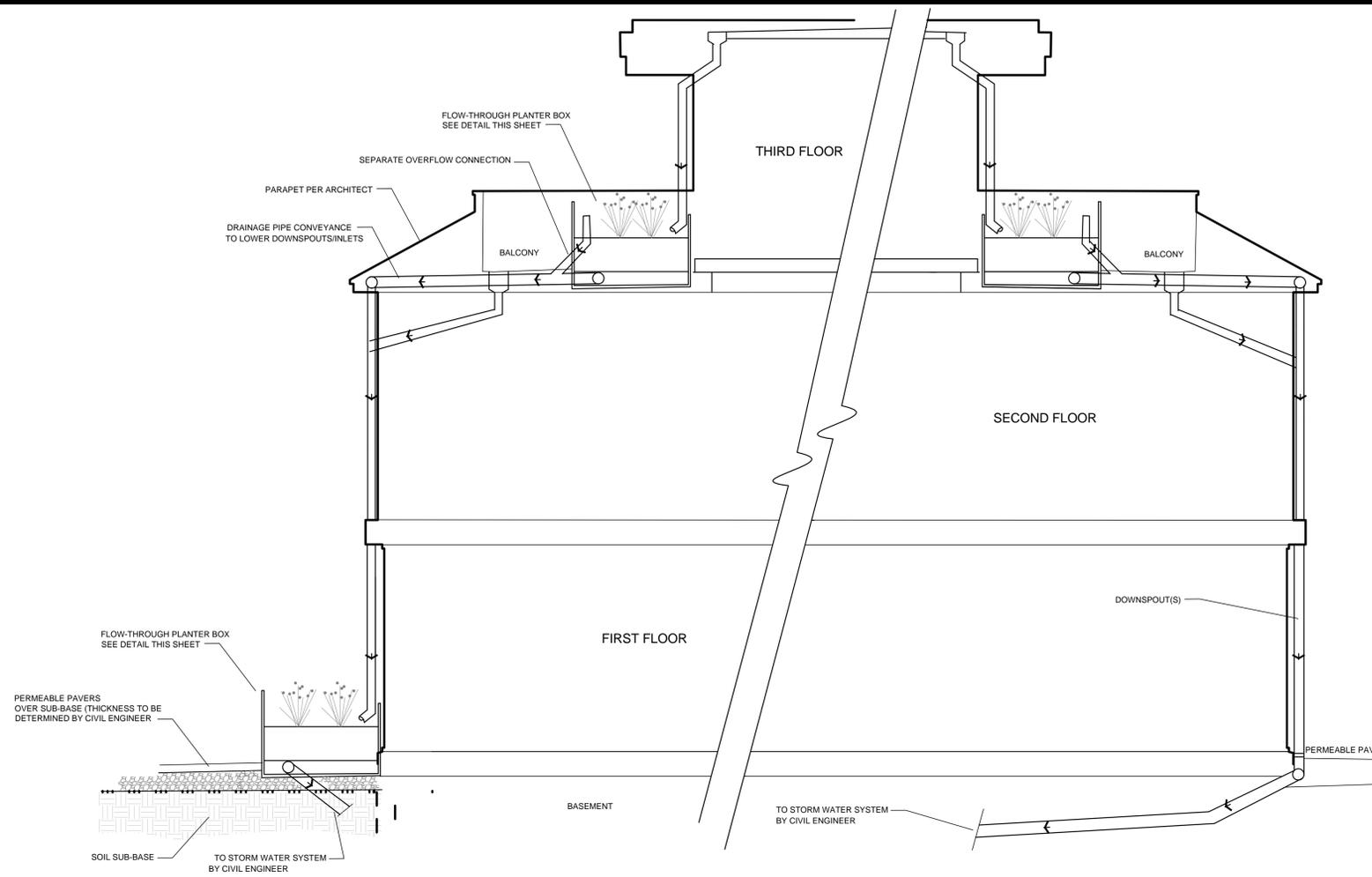
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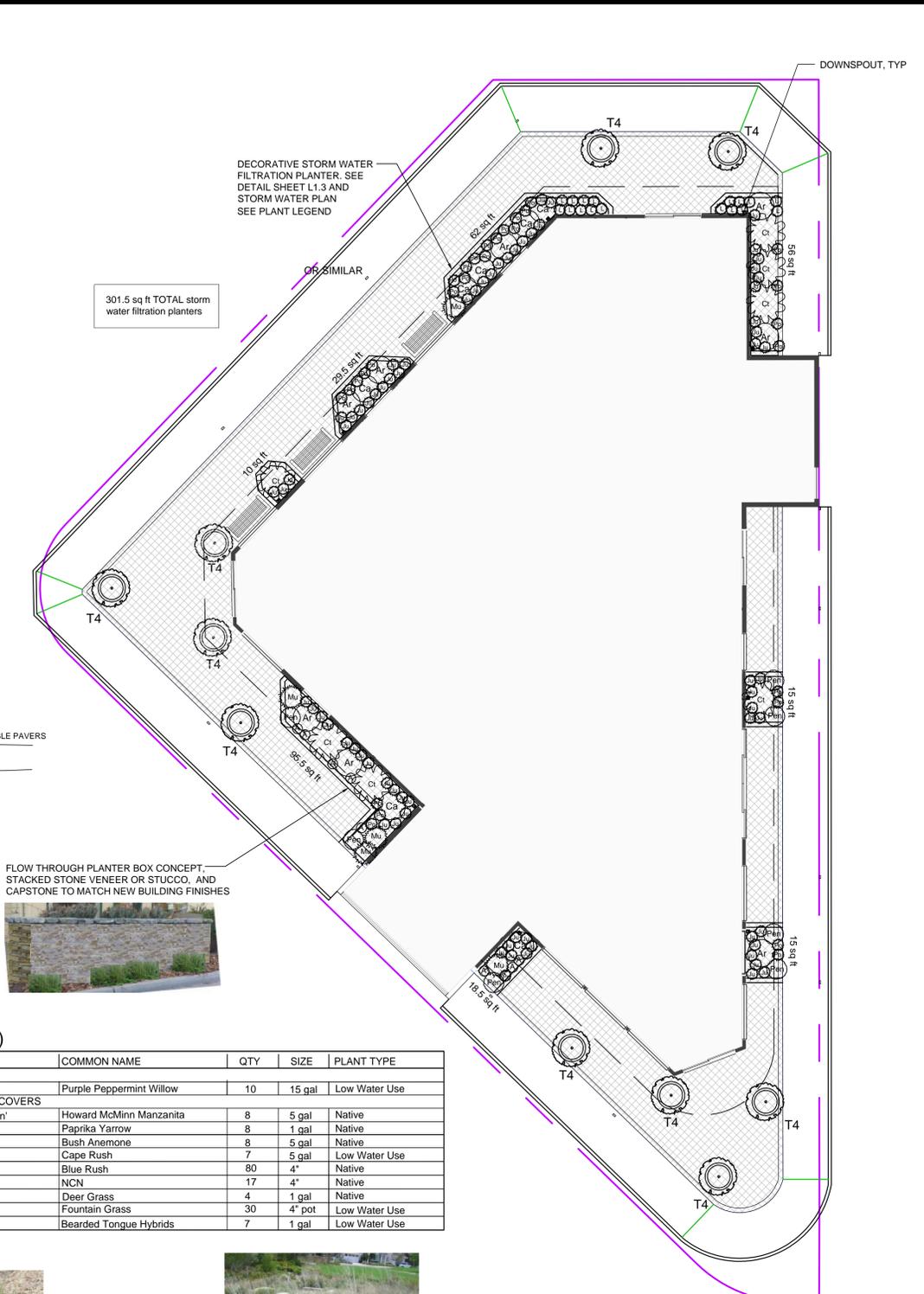
DESIGN DEVELOPMENT

LANDSCAPE PLAN
Third Level Planters &
Filtration Planter Detail

L- 1.3



DET1 CONCEPTUAL STORMWATER CONTROL SCHEMATIC
L-3 SCALE: NTS



THIRD LEVEL PLAN VIEW
SCALE: 1/8" = 1'-0"



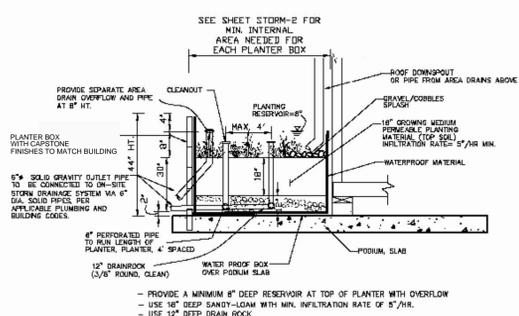
FLOW THROUGH PLANTER BOX CONCEPT, STACKED STONE VENEER OR STUCCO, AND CAPSTONE TO MATCH NEW BUILDING FINISHES

PLANT LEGEND (3rd Level)

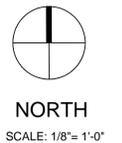
SYM	BOTANICAL NAME	COMMON NAME	QTY	SIZE	PLANT TYPE
TREES					
T4	AGONIS flexuosa 'After Dark'	Purple Peppermint Willow	10	15 gal	Low Water Use
SHRUBS, GRASSES, PERENNIALS, GROUNDCOVERS					
Ar	ARCTOSTAPHYLOS 'Howard McMinn'	Howard McMinn Manzanita	8	5 gal	Native
Am	ACHILLEA millefolium 'Paprika'	Paprika Yarrow	8	1 gal	Native
Ca	CARPENTERIA californica	Bush Anemone	8	5 gal	Native
Ct	CHONDROPETALUM tectorum	Cape Rush	7	5 gal	Low Water Use
Ju	JUNCUS patens	Blue Rush	80	4"	Native
L	LOMANDRA 'Breeze'	NCN	17	4"	Native
Mu	MUHLENBERGIA rigens	Deer Grass	4	1 gal	Native
Po	PENNISETUM orientale	Fountain Grass	30	4" pot	Low Water Use
Pe	PENSTEMON gloxinoides cvs.	Bearded Tongue Hybrids	7	1 gal	Low Water Use



DET2 TREE PLANTER: 35" x 28" KYOTO by Greenform
L-3



DET3 FLOW THROUGH PLANTER BOX
L-3 SCALE: NTS



DATE	REVISION
10.29.2014	DESIGN REVIEW
11.12.2014	DESIGN REVIEW
02.17.2015	DESIGN REVIEW
04.3.2015	DESIGN REVIEW



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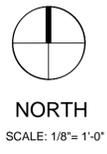
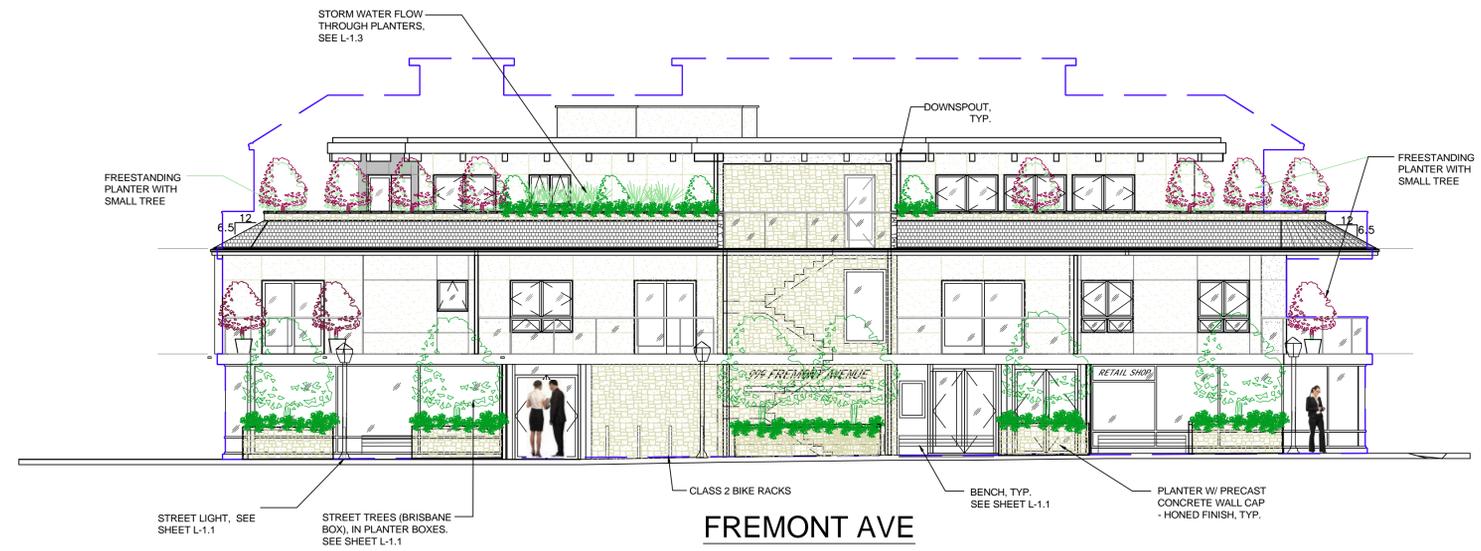


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LOYOLA CORNERS
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LOS ALTOS, CA 94024

DESIGN DEVELOPMENT
LANDSCAPE PLAN
Conceptual Elevations

L- 1.4





Cafe Vitale

Tom's
BREAKFAST LUNCH

RETAIL SHOP

Clock tower



NO RIGHT TURN



North
← NORTH

SOUTH



NO LEFT TURN ON RED

us bank

Handicapped parking sign





A modern, two-story building with a mix of stone and light-colored siding. It features a prominent corner window and a balcony. The building is situated at the corner of the intersection.

A long, single-story yellow building with a brown roof. It has several windows and a sign that reads 'GIVE'. The building is located on the right side of the intersection.

A red SUV driving through the intersection, moving from the bottom towards the top right.

CLEAR

STOP STOP

USA W.V. R-22-14

GIVE



Attachment 2
Applicant Cover Letter



July 3, 2015

Sean Gallegos
City of Los Altos Planning
Los Altos, CA

Subject: Appeal to Council

Re: 999 Fremont Avenue, Los Altos, CA
14-D-04, 14-UP-05 & 14-SD-01



Dear Mr. Gallegos and Members of the City Council:

We are writing this letter on behalf of our client, Gregg Bunker, requesting the Council's support for the captioned project. Mr. Bunker and his design team, including this office, have worked diligently to respond to concerns of the City Staff and Planning & Transportation Committee and the public.

This project has received a great deal of scrutiny as is appropriate for the development of a project like ours. Staff was incredibly diligent in requiring the team to itemize and illustrate the proposed building design. We were delighted that, ultimately, the City Staff were able to make the findings that the project met the objectives and requirements of both the Specific Plan for Loyola Corners and the City's General Plan.

We would say we met both the letter and the intent of Specific Plan as Mr. Elie Alcheck, one of the authors of the Specific Plan, made clear in a letter of support for the project. However, the Planning & Transportation chose to overrule the Professional Staff and recommend denial of the Use Permit and Design Review application. It is our opinion that the PTC was unduly influenced by residents and that the original recommendation by Staff – with Conditions – be approved so that Mr. Bunker may build his project.

This letter briefly addresses the changes and improvements made to the project since the original submission in December of 2014. We did listen to the comments of Staff, the Commission and members of the community. The building is respectful of those comments and concerns and has to be balanced against the costs and benefits of developing the property. It is our opinion that we've done so.

The changes to the building include:

- Reducing its perceived height by 7'
- Increased the commercial component

- Coordinated the streetscape with the development of the County's Loyola bridge project;
- Created as bicycle friendly an area as possible given the existing street configuration;
- Created a pedestrian friendly street level walking area along Fremont and A Streets
- Added four living units that contribute to the housing stock.
- Created parking to support both the commercial and residential development – this parking could be increased by an additional three spaces if permitted

We believe that the modifications and refinements we've made in response to all the comments are significant and positive and that approval of the project will be provide a significant impetus for the continuation of efforts to improve Loyola Corners.

We really do appreciate the time and energy the Staff and Commission have all devoted to working with Mr. Bunker and ourselves on this project. We believe that the project deserves a positive hearing and that Mr. Bunker should be allowed to build out his project. We're hopeful that you will be favorably disposed to respecting the professional expertise of the Staff and approve rather than deny the Project.

Sincerely,

Alpheus W. Jessup, AIA
Architect

CC: Gregg Bunker

CITY CLERK'S OFFICE

LAW OFFICES

2015 JUL 7 AM 10 11

William R. Seligmann

CITY OF LOS ALTOS
CALIFORNIA

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July 6, 2015

Silicon Valley Office:
(408) 356-1950

Of Counsel:
Atchison, Barisone, Condotti & Kovacevich

Honorable Mayor Pepper and
Members of the City Council
Los Altos City Hall
1 North San Antonio Road
Los Altos, CA 94022

Re: 999 Fremont Avenue
14-D-04, 14-UP-05 & 14-SD-01

Dear Mayor Pepper and Council Members:

My office has been retained by Mr. Gregg Bunker to assist with the approval of the above-referenced project in your City. It is my hope that the information that we provide in this letter will be of assistance to you in your consideration of Mr. Bunker's proposed mixed use development.

Nature of the Project

As the Council is no doubt aware, the proposed project seeks to redevelop an aging commercial property into a mixed use project, consisting of approximately 1,800 square feet of ground floor retail space to serve the neighborhood, with four (4) residential units located above. As staff has noted in its reports to the Planning Commission, *the project meets all of the objective standards* required under the applicable zoning provisions, including setbacks, height and parking. Moreover, a traffic study prepared for the project found that the project would have no significant traffic impacts.

Housing Accountability Act (Cal. Gov't. Code § 65589.5)

A neighborhood mixed use development such as the present one is subject to the Housing Accountability Act (sometimes referred to colloquially as the "Anti NIMBY law" - *Honchariw v. County of Stanislaus* (2011) 200 Cal.App.4th 1066, 1068; see Cal. Gov't. Code § 65589.5(h)(2)(B) as to applicability to mixed use projects¹.)

¹ *Honchariw* also held that the Housing Accountability Act is not limited to "affordable" housing developments.

The State Legislature adopted this Act to in recognition that the “lack of housing . . . is a critical problem that threatens the economic, environmental, and social quality of life in California;” and to ensure that local jurisdictions do not apply “excessive standards for housing projects.” (Cal. Gov’t. Code § 65589.5(a)(1)(4).) Under the Act, cities cannot use subjective criteria to deny a neighborhood mixed use project. A city can deny such a project that complies with the “**objective** general plan and zoning standards” only if:

1. The project would have a significant, **quantifiable, direct**, and unavoidable adverse impact upon the public **health or safety, based on objective**, identified **written public health or safety standards**, policies, or conditions as they existed on the date the application was deemed complete; and
2. There is no feasible method to satisfactorily mitigate or avoid the adverse impact other than the disapproval of the project. (Cal. Gov’t. Code § 65589.5(j).)

These provision are intended to “tak[e] away an agency’s ability to use what might be called a ‘subjective’ development ‘policy’ (for example, ‘suitability’)” as grounds for denying a project that is covered by the Act. (*Honchariw v. County of Stanislaus* (2011) 200 Cal.App.4th, *supra*, 1076.) In other word, if a neighborhood mixed use project satisfies a city’s *objective* criteria for development, the city cannot use subjective design standards to deny the project.

In the present case, there is no dispute that the project satisfies the all of the City’s applicable objective development standards; and no evidence has been presented that the project would have a quantifiable, direct impact on the public health or safety. In fact, the traffic study conducted by the City found no significant impact. Consequently, under the Housing Accountability Act, the project should therefore be approved.

The Act further provides that if a city denies a neighborhood mixed use project in violation of the Act, or conditions an approval in such a way as to make the project infeasible, the courts can overturn the city’s decision, and award attorney’s fees and costs to the applicant, as well as imposing fines in some cases. (Cal. Gov’t. Code § 65589.5(k), (l).)²

The Applicant has Gone Above and Beyond the Legal Requirements

Although Mr. Bunker and his architect could simply have rested on the Housing Accountability Act, they have instead gone to well beyond the mandates of the law to try to accommodate the subjective concerns raised in the hearing process. These accommodations include:

1. Reducing the project density by eliminating a residential unit;
2. Reducing the floor area of the third floor by 1,978 square feet;
3. Lowering the apparent building height by seven (7) feet;

² Similar provisions also apply to projects that offer affordable units, as was included with the original design for this project. (Cal. Gov’t. Code § 65589.5(d).)

4. Deemphasizing the third story by using a flat roof and increasing the third story setbacks;
5. Emphasizing the second story with a sloping roof;
6. Increasing the commercial retail area;
7. Simplifying the material palate; and
8. Increasing the amount of landscaped areas.

In short, Mr. Bunker has gone out of his way to ensure that this project will not only meet the requirements of the law, but will be a true asset to the community. It will provide desirable neighborhood commercial space, and contribute to the City's needed housing stock.³As such, the project deserves the Council's endorsement.

I hope that the Council finds the information contained in this letter to be valuable; and if you have any questions, I would be more than happy to address them.

Sincerely,



William R. Seligmann

cc: Marcia Somers, City Manager
Jon Maginot, City Clerk
Jolie Houston, City Attorney
James Walgren, Community Development Director
clients

³ As the Council is aware the recently adopted Housing Element documented that as of August of 2014, the City needed a total of 441 new housing units to satisfy its Regional Housing Needs Assessment ("RHNA"), including 111 moderate and 63 above-moderate units. (City of Los Altos 2015 – 2023 Housing Element, P. 68, Table B-34A.)

Attachment 3

Planning and Transportation Commission Agenda Report,
June 4, 2015



DATE: June 4, 2015
AGENDA ITEM # 2

TO: Planning and Transportation Commission
FROM: Sean K. Gallegos, Assistant Planner
David Kornfield, Planning Services Manager
SUBJECT: 14-D-04, 14-UP-05 & 14-SD-01—999 Fremont Avenue

RECOMMENDATION

Recommend approval of Design Review, Use Permit and Subdivision applications 14-D-04, 14-UP-05 & 14-SD-01 to the City Council subject to the findings and conditions

PROJECT DESCRIPTION

This project is a mixed-use, multiple-family residential and commercial building at 999 Fremont Avenue. The project includes 1,792 square feet of commercial space, 14 parking spaces and four, multi-family residential condominium units. The project replaces the existing building that has approximately 1,000 square feet of commercial area. The following table summarizes the project's technical details:

GENERAL PLAN DESIGNATION: Neighborhood Commercial
ZONING: Commercial Neighborhood (CN)/Loyola Corners
Specific Plan Overlay (LC/SPZ)
PARCEL SIZE: 7,348 square feet
MATERIALS: Stone veneer, stucco, aluminum clad storefront system,
concrete tile roof, and glass railings with aluminum
handrail

	Existing	Proposed	Allowed/Required
Front setback (Fremont Avenue)	15 feet	2 feet	0 feet
Side setback (Miramonte Avenue)	20 feet	0 feet	0 feet
Side setback (A Street)	13 feet	2 feet	0 feet
Parking	6 spaces	14 spaces	14 spaces
Height	16 feet	30 feet	30 feet
DENSITY:	n/a	4 units	n/a

BACKGROUND

Study Session

On January 23, 2014, the Planning and Transportation Commission held a study session to consider a proposal for a mixed-use building with 1,000 square feet of retail area, 15 parking spaces at ground level, and six, multiple-family condominium units at the second and third floors. The Commission provided preliminary feedback, including that: the design was too overwhelming for the site and needed to acknowledge the architectural character of Loyola Corners; the project required some relief from the third floor to fit-in better with the surroundings; the parking plan was marginal and created circulation problem; and, the project should provide adequate sidewalks and pedestrian access. The meeting minutes are included as Attachment C.

Bicycle and Pedestrian Advisory Commission

On August 27, 2014, the Bicycle and Pedestrian Advisory Commission held a public meeting to consider the project. The Commission was supportive of the project, with suggestions to include wider sidewalks, bicycle lanes, improving and realigning ADA ramps, improving line-of-sight issues at corners, and providing Class I and II bicycle facilities. The meeting minutes are included as Attachment D.

Planning and Transportation Commission

On December 4, 2014 the Planning and Transportation Commission held a public hearing to consider the project. The Commission found that they could not make the findings to approve the project and deferred a motion to deny the project subject to the following draft findings:

1. That the project was inconsistent with the General Plan and Loyola Corners Specific Plan;
2. That the size and massing appeared too large in scale with the surroundings;
3. That the project lacked an appropriate attention to providing human scale elements;
4. That the design needed to be unpretentious and unified in its design character; and
5. That the design would benefit from a unified architectural character, more sloping roof elements, more commercial space, a reduction in the number of building materials used, a greater attention to the immediate neighborhood character, greater setbacks from the street for the second and third stories, and more sensitivity to the gateway site setting tone for the area's future commercial development.

At their next meeting on January 15, 2015, the applicant approached the Commission with a desire to revise the plans and requested that the Commission reconsider their motion. The Commission voted 3-2, with Commissioners Baer and Bressack opposed, to continue the application for further consideration. The minutes for the December 4, 2014 and January 15, 2015 meetings are included as Attachment E and F, respectively.

DISCUSSION

Project Revisions

In response to the Commission's concerns, the applicant made the following substantive revisions to the project:

1. Lowered the apparent building height by seven (7) feet;
2. Reduced the floor area of the third floor by 1,978 square feet;
3. Emphasized the second story with a sloping roof;
4. Deemphasized the third story by using a flat roof;
5. Increased the setback of the third story: from three (3) feet to approximately eight (8) feet from the A Street frontage, from three (3) feet to as much as eight (8) feet from the Fremont Avenue frontage, and from zero to three and a half (3.5) feet from the Miramonte Avenue frontage;
6. Simplified the material palette by eliminating the use of horizontal siding;
7. Relocated the trash room to Miramonte Avenue;
8. Reduced the number of housing units from five to four;
9. Increased the amount of landscaped area and modified to the street trees to be consistent with the Loyola Corners Concept Plan; and
10. Increased the commercial retail area from 1,345 square feet to 1,792 square feet by adding a basement storage area.

General Plan and Specific Plan

The subject property has a Neighborhood Commercial General Plan land use designation. It is also within the Loyola Corners Neighborhood Commercial Center Specific Plan area. As a matter of background, the Specific Plan calls out specific policies for the subject property to develop it as an open plaza, which neither the City nor the property owner have come to terms. The Specific Plan did not identify a financial means to acquire the property. The Specific Plan's traffic circulation changes have remained conceptual and only partially implemented such as a slight relocation of the Foothill Expressway on-ramp and the County's current project to widen the Loyola Drive Bridge.

The General Plan Land Use Element and Specific Plan goals seek to encourage and facilitate the long-term economic viability of the neighborhood serving commercial uses and to maintain and enhance the pleasant, attractive, and pedestrian-scale neighborhood character of Loyola Corners. The mixed-use building is consistent with the General Plan and Specific Plan by providing

additional, more prominent commercial space on the ground floor. Both the improved commercial space and the residential units on the second and third floors should enhance the economic vitality of the area. This project improves the pedestrian environment with wider sidewalks, decorative paving, and additional street trees and landscaping elements. Additionally, the project benefits the bicycle facilities in the district by providing bike lanes on the Miramonte Avenue and Fremont Avenue frontages.

Zoning Compliance

The project conforms to the technical aspects of the zoning code. The project's 30-foot height measured to the roof deck is within the district's 30-foot height limit. Although the Code requires no building setbacks, the project provides two-foot deep planter elements on A Street and a two-foot setback from Fremont Avenue for a wider sidewalk. The project meets the minimum parking requirements by providing six (6) commercial parking spaces and eight (8) residential parking spaces. The residential parking spaces are provided by four (4) individual spaces in the garage (marked with an "X" on plans page A2.0) that each double with an elevator for a second space in the basement (see detail 15, on plans page A5.2). As permitted by code for mixed-use project, the retail overflow parking during the off-hours provides the one required visitor parking space for the residential units. A condition of approval requires a recorded parking management plan to ensure the availability of the commercial parking spaces during normal business hours including hours of the gate operation and the use of the residential parking spaces.

Design Controls and Findings

With the revisions, staff finds that the project is consistent with the Specific Plan's architectural design controls that: encourage an informal and modest character; the size and mass of structures and building elements at a human scale; compatible scale, color and material with surrounding structures; and simple roof forms and uniform roof materials.

In staff's view, the architecture is informal and modest avoiding exaggerated or false elements; the most prominent elements are the stone clad stairways on the longer Miramonte and Fremont Avenue frontages serving to vary the scale of the otherwise horizontal building elements. The building uses relatively low wall plate heights to minimize its height, which relates well to the lower wall heights of the nearest adjacent commercial building across A Street. The building's lower first story, recessed commercial entries, prominent storefront windows, bike rack recess, and planters provide pedestrian scale elements that add interest at the ground level; the second floor balconies and wall recesses add to the human scale elements and include design details that add interest. The balconies serve to enliven the street and signal the residential use.

The simple hip roof on the second story and the flat roof of the third story are uniform elements that serve to establish an identity to the second floor and downplay the massing of the third level. Also, the upper floor windows are subordinated to the wall massing as required by the Specific Plan. The stone, Stucco wall siding, flat tile roof and wooden beam details are informal and rustic. The materials and colors are natural and neutral in tone, which relate to one another and the materials on the surrounding structures. The proposed building materials convey quality, integrity, permanence and durability and effectively define the building elements.

Landscape areas such as planters and climbing vines soften the blank wall elements at the first story around the parking garage. A series of new street trees along all frontages helps establish the new street tree palate for Loyola Corners and to soften the building massing. Staff notes a need to adjust the landscape plan slightly to accommodate the new sidewalk locations at the corner of A Street and Fremont Avenue because of the Loyola Drive Bridge work; this involves moving one of the street trees, bench and planter. In addition, staff notes that the County recently removed two trees on the Fremont Avenue frontage of the site to accommodate their new accessibility improvements; these trees would have otherwise been removed by the project.

The signage areas are modest and in keeping with the informal nature of the building. A window panel is shown for each retail space to contain an area for a sign. Multiple tenant buildings must have master sign programs to ensure that the signs relate in terms of style, materials, colors and proportions. The Loyola Corners Specific Plan suggests that signs should be subordinate to the building architecture, have a pedestrian orientation and avoid internal illumination. Given the limited nature of the signage and the conceptual conformance to the Specific Plan, staff included a condition to clarify the master sign program prior to building permit submittal.

The applicant relocated the trash enclosure to Miramonte Avenue at the northeast corner of the building and appropriately integrated it into the building design. Mission Trail Waste systems confirmed that they could service the internal location and avoid staging the receptacles outside. A condition of approval requires the property owner to maintain the enhanced trash service for the building.

The building design has an appropriately designed mechanical screen located on the roof. A low Stucco wall to match the main building finish would effectively screen the rooftop mechanical equipment from the off-site view. The exterior lighting plan incorporates light fixtures for sidewalks along Fremont Avenue, Miramonte Avenue and A Street, which are consistent with the existing light poles and fixtures in Loyola Corners. The project's second story and third story lighting is unspecified so staff included a condition to require the shrouding of any upper story lighting to minimize glare.

Photo-Simulated Perspectives

The Commission was concerned that the photo-simulated perspectives did not accurately reflect the building's scale, siting, architectural details, and materials. The photo-simulated perspectives have been revised to convey the building's design and architectural details, location, scale, and height. In addition, the photo-simulated perspectives are consistent with the renderings, elevations and color/materials board.

However, the Fremont Avenue perspectives do not accurately convey the hardscape, landscaping, pedestrian amenities and lighting standards for one minor aspect of the project. This occurred due to the new crosswalk locations for Fremont Avenue and A Street for the Loyola Drive Bridge. Staff notes that we raised this concern with the applicant early in the application process; however, the County only recently released the technical files for the plans, which understandably resulted in the inconsistency with the photo-simulated perspective.

Use Permit

Mixed-uses and building additions in the Loyola Corners Specific Plan district require a use permit. In order to approve a use permit, the Commission and Council must make the standard findings that the mixed-use is desirable, in accordance with the zoning objectives, not detrimental to the health, safety, or welfare of persons or property in the vicinity, and that the uses will comply with the district regulations. Additionally, the use permit requires the following special findings for the commercial expansion within Loyola Corners area:

1. That the proposed construction meets the specific purposes of the Loyola Corners Specific Plan zoning district;
2. That the proposed square footage contributes to the desired fifteen thousand (15,000) square foot new ground-level retail; and
3. That the commercial use builds upon the existing strengths of Loyola Corners and adds business which is appropriate in terms of use, physical scale and size of the site.

The location of the use is desirable in that it develops an underdeveloped property with new commercial and residential uses that should help spur the redevelopment of the area. Since adoption of the specific Plan, only 500 square feet of ground floor commercial space has been added to Loyola Corners within its core.

The project replaces the existing commercial building with a net increase of approximately 800 square feet of commercial area. The project will provide approximately 1,800 square feet of better-oriented ground floor commercial space with a prominent frontage along Fremont Avenue. The project will also provide four additional households to the immediate area, which benefits the economic vitality of the area. Overall, staff finds that the project meets the required use permit findings.

Affordable Housing

As revised, the project is exempt from the City's Multiple-Family Affordable Housing regulations (Sec. 14.28) since it has fewer than five housing units.

Subdivision

The project includes a Tentative Map to subdivide the property into condominiums. The map will subdivide the development into four residential condominiums, two commercial condominium suites and common area. The subdivision conforms to the permitted General Plan and zoning as described previously. The subdivision is not injurious to public health and safety, and is suitable for the proposed type of development. The subdivision provides proper access easements for ingress, egress, public utilities and public services.

Transportation Analysis

The General Plan requires a transportation analysis for all development projects resulting in 50 or more net new daily trips. Hexagon Transportation Consultants prepared a trip generation study for the project showing that the project will generate 43 average daily trips over the existing use, thus avoiding the need for such a report (Attachment H). As outlined in the analysis the project will generate a net increase of two (2) AM peak hour trips and four (4) PM peak hour trips. Figure 1 shows the trip distribution for the new traffic and identifies which intersections will experience increased traffic during the AM and PM peak hours. Based on the small increase in peak hour trips, none of the affected intersections or street segments will experience a reduction in the level of service (LOS) due to this project.

Despite the project not resulting in significant traffic impacts, there is a concern regarding sight distance and pedestrian visibility at the driveway opening along Miramonte Avenue. Accordingly, staff included a condition of approval requiring a pedestrian warning system to alert pedestrians on the sidewalk to cars exiting the garage.

There was a concern raised by the Bicycle and Pedestrian Advisory Commission about a potential sight visibility concern at the intersection of A Street and Miramonte Avenue. Staff reviewed this with the City's Transportation Services Manager who identified a minor concern with the landscape plan. While the building itself does not pose a visibility issue, cars queuing in the A Street right turn lane and on the southbound Miramonte Avenue may reduce visibility; however these effects are beyond the control of this project. There is a slight potential, though, for the proposed tree on the Miramonte Avenue frontage closest to A Street to obscure visibility.

Thus, it is staff's recommendation that the landscape plan omit the northernmost tree on Miramonte Avenue to improve visibility and to adjust the plan to move the tree to the north of the driveway entrance farther to the north to improve visibility when cars egress the garage. Additionally, as shown on the plans, staff included a condition limiting the egress from the garage to a right turn only to limit the driveway impacts on Miramonte Avenue.

Construction Management Plan

The applicant's construction management plan limits access to A Street, installs perimeter fencing, and creates off-site parking areas as negotiated with nearby property owners. Limiting the site access to A Street seems reasonable, as that is the lowest volume street. During excavation, however, the dirt hauling should be limited to non-peak traffic hours. The perimeter fencing is desired to minimize the construction impacts and the off-site parking is necessary, as there is limited on-street parking in the area. Staff anticipates conditions on the construction management plan to: a) limit the fencing to provide as much sight visibility as possible b) require screening fabric on the fencing; and c) limit any dirt hauling to non-peak traffic hours.

Environmental Review

As an in-fill site, this project is categorically exempt from environmental review under Section 15332 of the California Environmental Quality Act since it meets certain conditions. As discussed in earlier sections, the project is consistent with the General Plan land use and zoning designations as well as all applicable policies and regulations. The site is less than five acres, surrounded by urban uses, does not provide any habitat for endangered, rare or threatened species, and can be served by all required utilities and public services. In addition, the project will not create any significant effects relating to traffic, noise, air quality or water quality.

The project's noise assessment (Attachment E) indicates that to meet the City's interior noise standards in the General Plan, special windows, ventilation and balcony railings are required. Staff included the pertinent specifications as a condition of approval.

The site was formerly a gas station. There is no record of soil abatement so there is a potential for residual soil contamination. Accordingly, staff included a condition of approval requiring the property owner to perform a soil test prior to the demolition and/or excavation of the site and implement any soil remediation deemed necessary.

Correspondence

Staff received three new letters of concern from residents citing concerns about the project's character and mass, potential traffic impacts and the need for story poles. Staff included copies of the prior correspondence as well for the Commission's reference.

Alternatives

Should the Commission not support the revisions to the project, staff recommends that the Commission recommend denial of the project to the City Council. Alternatively, the Commission could continue their review and direct the applicant to address specific concerns. Staff notes, however, that the project has had the benefit of two reviews by the Commission and substantial changes beyond what is proposed are unlikely.

Public Notification

This project received a notice a public hearing advertisement in the *Town Crier*, mailed notices to the 89 property owners and business tenants within 500 feet of the site, and an onsite billboard posting.

Cc: Alpheus Jessup, Applicant/Architect
Greg Bunker, Property Owner

Attachments

- A. Application and Letter
- B. Area Map and Vicinity Map
- C. Planning and Transportation Commission Minutes Study Session, January 23, 2014
- D. Bicycle and Pedestrian Advisory Commission Meeting Minutes, August 27, 2014

- E. Planning and Transportation Commission Minutes, December 4, 2014
- F. Planning and Transportation Commission Minutes, January 15, 2015
- G. Environmental Noise Assessment
- H. Trip Generation Study
- I. Construction Management Plan
- J. Correspondence

FINDINGS

14-D-04, 14-UP-05 & 14-SD-01 — 999 Fremont Avenue

1. The Planning and Transportation Commission finds in accordance with Section 15332 of the California Environmental Quality Act Guidelines as amended on January 1, 2013 that the following Categorical Exemption findings can be made:
 - a. The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations;
 - b. The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses; The project site has no value as habitat for endangered, rare or threatened species;
 - c. Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality; and
 - d. The site can be adequately served by all required utilities and public services.

2. With regard to commercial design review the Planning and Transportation Commission makes the following findings in accordance with Section 14.78.050 of the Municipal Code:
 - a. The proposal does meet the goals, policies and objectives of the General Plan and Loyola Corners Neighborhood Commercial Center Specific Plan, design guidelines and district design criteria adopted for the area;
 - b. The proposal has architectural integrity and has an appropriate relationship with other structures in the immediate area in terms of height, bulk and design;
 - c. Building mass is articulated to relate to the human scale, both horizontally and vertically. Building elevations does have variation and depth and does avoid large blank wall surfaces. Residential or mixed-use residential projects incorporate elements that signal habitation, such as identifiable entrances, stairs, porches, bays and balconies;
 - d. Exterior materials and finishes convey quality, integrity, permanence and durability, and materials are used effectively to define building elements such as base, body, parapets, bays, arcades and structural elements;
 - e. Landscaping is generous and inviting and landscape and hardscape features are designed to complement the building and parking areas and to be integrated with the building architecture and the surrounding streetscape. Landscaping includes substantial street tree canopy, either in the public right-of-way or within the project frontage;

- f. Signage is provided in the plan set; therefore, the signage is designed to complement the building architecture in terms of style, materials, colors and proportions;
 - g. Mechanical equipment is screened from public view and the screening is designed to be consistent with the building architecture in form, material and detailing; and
 - h. Service, trash and utility areas are screened from public view, or are enclosed in structures that are consistent with the building architecture in materials and detailing.
3. With regard to use permit for a mixed-use building with retail and residential uses the Planning and Transportation Commission finds in accordance with Section 14.80.060 (A-D, J) of the Municipal Code:
- a. That the proposed location of the conditional use is desirable or essential to the public health, safety, comfort, convenience, prosperity, or welfare;
 - b. That the proposed location of the conditional use is in accordance with the objectives of the zoning plan as stated in Chapter 14.02 of this title;
 - c. That the proposed location of the conditional use, under the circumstances of the particular case, will not be detrimental to the health, safety, comfort, convenience, prosperity, or welfare of persons residing or working in the vicinity or injurious to property or improvements in the vicinity;
 - d. That the proposed conditional use will comply with the regulations prescribed for the district in which the site is located and the general provisions of Chapter 14.02.
 - e. That the proposed construction is found to meet the specific purposes of the LCSP zoning district pursuant to Section 14.42.020 of the Los Altos Municipal Code; and
 - f. That the proposed square footage contributes to expansion potential pursuant to Section 14.42.040 of the Los Altos Municipal Code:
 - 1. The square footage contributes to the permitted fifteen thousand (15,000) square foot new ground-level retail; and
 - 2. That the use occupying the proposed square footage builds upon the existing strengths of the Loyola Corners Neighborhood Commercial Center and adds business which is appropriate in terms of use, physical scale, and size of the site.
4. With regard to the subdivision the Planning and Transportation Commission finds in accordance with Section 66474 of the Subdivision Map Act of the State of California:
- a. That the proposed subdivision is consistent with the General Plan and Specific Plan;
 - b. That the site is physically suitable for this type and density of development;

- c. That the design of the subdivision and the proposed improvements are not likely to cause substantial environmental damage, or substantially injure fish or wildlife;
- d. That the design of the subdivision is not likely to cause serious public health problems; and
- e. That the design of the subdivision will not conflict with public access easements.

CONDITIONS

14-D-04, 14-UP-05 & 14-SD-01 — 999 Fremont Avenue

GENERAL

1. Project approval is based upon the plans received on May 20, 2015 except as modified by these conditions.
2. The project shall include a pedestrian warning system to alert pedestrians to garage egress as approved by the Community Development Director.
3. Prior to City Council review, the landscape plan shall omit the northernmost tree on Miramonte Avenue to improve intersection visibility and move the tree located to the north of the driveway entrance farther to the north to improve visibility when cars egress the garage.
4. Prior to City Council review, the landscape plan shall be revised to show the updated crosswalk locations for the Loyola Drive Bridge alignment and adjust the street tree and planter accordingly.
5. Egress from the garage shall be limited to a right turn only to limit the driveway impacts on Miramonte Avenue. The property owner shall install any road markings and signage as required by the Transportation Services Manager.
6. The property owner or designee shall maintain the appropriate trash service avoiding the need to stage any garbage and/or recycling containers outside.
7. All work within the public right of way shall be done in accordance with plans to be approved by the City Engineer.
8. An encroachment permit and/or a permit to open streets shall be obtained prior to any work done within the public right-of-way and it shall be in accordance with plans to be approved by the City Engineer.
9. The developer shall contact electric, gas, communication and water utility companies regarding the installation of new utility services to the site.
10. All improvements shall comply with Americans with Disabilities Act (ADA).
11. The project shall comply with the San Francisco Bay Region Municipal Regional Stormwater (MRP) National Pollutant Discharge Elimination System (NPDES) Permit No. CA S612008, Order R2-2009-0074, Provision C.3 dated October 14, 2009 and show that all treatment measures are in accordance with the C.3 Provisions for Low Impact Development (LID) and in compliance with the December 1, 2011 requirements. The improvement plans shall include the “Blueprint for a Clean Bay” plan sheet in all plan submittals.

12. Any proposed sewer lateral connection shall be approved by the City Engineer.
13. The developer agrees to indemnify and hold City harmless from all costs and expenses, including attorney's fees, incurred by the City or held to be the liability of City in connection with City's defense of its actions in any proceeding brought in any State or Federal Court, challenging the City's action with respect to the applicant's project.
14. The Planning and Transportation Commission may approve minor changes to the development plans. Substantive project changes require a formal amendment application.

PRIOR TO FINAL MAP RECORDATION

15. The applicant shall include provisions in the Covenants, Conditions and Restrictions (CC&Rs) that restrict storage on the private patio and decks and outline rules for other objects stored on the private patio and decks with the goal of minimizing visual impacts.
16. The applicant shall dedicate public utility easements as required by the utility companies to serve the site.
17. The applicant shall dedicate the necessary public right of way along Fremont Avenue and A Street to make sure that the entire sidewalk is within the public right of way.
18. The applicant shall pay all applicable fees, including but not limited to sanitary sewer impact fees, parkland dedication in lieu fees, traffic impact fees and map check fee plus deposit as required by the City of Los Altos Municipal Code.

PRIOR TO BUILDING PERMIT SUBMITTAL

19. The applicant shall prepare and record a final map. Plats and legal descriptions of the final map shall be submitted for review and approval by the City Land Surveyor, and the applicant shall provide a sufficient fee retainer to cover the cost of the final map application.
20. The applicant shall submit a cost estimate for the improvements in the public right-of-way and shall submit a 100 percent performance bond or cash deposit (to be held until acceptance of improvements) for the work in the public right-of-way. The deposit shall also include an additional six percent of the construction cost estimate to cover the City's administration costs.
21. The applicant shall submit calculations showing that the City's existing six-inch sewer line will not exceed two-thirds full due to the project's sewer loads. Calculations shall include the six-inch main from the property along Miramonte Ave. to the point where it connects to the twenty four-inch sewer line on Covington Avenue. For any segment that is calculated to exceed two-thirds full for average daily flow or for any segment that the flow is surcharged in the main due to peak flow, the applicant shall replace the six-inch sewer line with an eight-inch sewer line.
22. The applicant shall provide a Stormwater Management Plan (SWMP) in accordance with the City guidance document showing that 100 percent of the site is being treated and is in

compliance with the MRP. The SWMP must be reviewed and approved by a City approved third party consultant and the City Engineer at the applicant's expense. The recommendations from the SWMP shall be shown on the building plans.

23. Provide verification that the project will comply with the City's Green Building Standards (Section 12.26 of the Municipal Code) from a qualified green building professional.
24. The applicant shall provide an address signage plan as required by the Building Official.
25. The property owner shall provide a master sign program that includes locations, sizes, materials, illumination and colors of all proposed signage for review and approval by the Community Development Director.
26. The developer shall prepare a parking management plan for the project that includes details how parking spaces will be managed and how the gate will be managed to allow access during normal business hours for review and approval by the Community Development Director. Such parking management plan shall be recorded as required by the Community Development Director.
27. Any upper story lighting design shall be shrouded or directed down to minimize glare.
28. A soils report shall be conducted to analyze the soil for any environmental contamination related to the former gas station use. Such report shall be provided to the Community Development Department for review and approval. Any measures to ensure the health and safety of construction workers and occupants of the building shall be performed and incorporated into the construction plans and building design.

PRIOR TO ISSUANCE OF BUILDING PERMIT

29. The applicant shall submit a construction management plan for review and approval by the Community Development Director. The construction management plans shall address any construction activities affecting the public right-of-way, including but not limited to: prohibiting dirt hauling during peak traffic hours, excavation, traffic control, truck routing, pedestrian protection, appropriately designed fencing to limit project impacts and maintain traffic visibility as much as practical, material storage, earth retention and construction and employee vehicle parking.
30. The applicant shall pay the applicable fees before the transportation permit can be issued by the City Traffic Engineer.
31. The applicant shall submit on-site and off-site grading and drainage plans that include drain swales, drain inlets, rough pad elevations, building envelopes, and grading elevations for approval by the of both the Community Development Director and City Traffic Engineer.
32. The applicant shall submit detailed plans for any construction activities affecting the public right-of-way, include but not limited to excavations, pedestrian protection, material storage, earth retention,

and construction vehicle parking, to the City Engineer for review and approval. The applicant shall also submit on-site and off-site grading and drainage plans that include drain swales, drain inlets, rough pad elevations, building envelopes, and grading elevations for approval by the City

33. The applicant shall design ADA ramps per Caltrans standard and consistent with the Loyola Bridge Expansion plans and conform to the existing street at the three corners of Miramonte Ave and A Street, Miramonte Avenue and Fremont Avenue, and Fremont Avenue and A Street, per Caltrans latest Standard.

PRIOR TO FINAL INSPECTION

34. The applicant shall relocate the crosswalk at all corners to conform the new ADA ramps, if necessary, consistent with the expansion of the Loyola Drive Bridge plans.
35. The applicant shall install streetlights with a one-foot candle of illumination each along the frontage of Fremont Avenue, Miramonte Avenue and A Street.
36. The applicant shall install commercial driveway approaches per the City standard, which shall include City approved cast iron truncated domes.
37. The applicant shall employ and/or retain a Qualified Green Building Professional as required by the Building Official.
38. The applicant shall abandon and/or underground all overhead utilities located on-site.
39. The applicant shall remove and replace the entire city sidewalk, curb and gutter along Miramonte Avenue, Fremont Avenue and A Street per City standard details, as directed by the Director of Public Works.
40. The minimum width of all sidewalks shall be five feet, not including curb. The proposed green wall and the cable wire shall be inside the private property.
41. The applicant shall install ADA compliant ramps at the three corners of Miramonte Avenue and A Street, Miramonte Avenue and Fremont Avenue, and Fremont Avenue and A Street, per the latest Caltrans Standard.
42. A one-year, 10 percent maintenance bond shall be submitted upon acceptance of improvements in the public right-of-way.
43. The applicant shall have a final inspection and certification done and submitted by the Engineer who designed the SWMP to ensure that the treatments were installed per design. The applicant shall submit a maintenance agreement to City for review and approval for the stormwater treatment methods installed in accordance with the SWMP. Once approved, the applicant shall record the agreement.

44. The applicant shall label all new or existing public and private catch basin inlets which are on or directly adjacent to the site with the “NO DUMPING - FLOWS TO ADOBE CREEK” logo as required by the City.
45. The developer shall submit verification that the structure was built in compliance with the California Green Building Standards pursuant to Section 12.26 of the Municipal Code.
46. The developer shall provide an acoustical analysis that evaluates interior and exterior noise levels to ensure that the project is in compliance with the City’s General Plan and Noise Ordinance.
47. All on- and off-site landscaping and irrigation shall be installed, as approved by the Community Development Director and the City Engineer.
48. Window controls and general building shell controls, as recommended in the Noise Assessment Study by Charles M. Salter Associates, Inc., shall be incorporated to reduce excessive interior and exterior noise exposures as follows:
 - a. Sound-rated windows and sliding glass doors are required at residences along Fremont Avenue with suggested sound insulation ratings of STC 32 or lower;
 - b. Mechanical ventilation system shall be provided to provide a habitable interior environment with windows closed for sound reduction; and
 - c. An acoustically-effective deck railing shall be provided for all balconies with a direct or side view of Fremont Avenue. To achieve an acoustically-effective deck railing, it must be constructed airtight, i.e., without cracks, gaps or other openings, and must provide for long term durability including the balcony floor. The railings can be constructed of material with a minimum surface weight of three pounds per square feet.



CITY OF LOS ALTOS
GENERAL APPLICATION

Type of Review Requested: (Check all boxes that apply)

Permit # 1106225

<input type="checkbox"/> One-Story Design Review	<input type="checkbox"/> Sign Review	<input checked="" type="checkbox"/> Multiple-Family Review
<input type="checkbox"/> Two-Story Design Review	<input type="checkbox"/> Sidewalk Display Permit	<input type="checkbox"/> Rezoning
<input type="checkbox"/> Variance(s)	<input checked="" type="checkbox"/> Use Permit	<input type="checkbox"/> R1-S Overlay
<input type="checkbox"/> Lot Line Adjustment	<input type="checkbox"/> Tenant Improvement	<input type="checkbox"/> General Plan/Code Amendment
<input type="checkbox"/> Tentative Map/Division of Land	<input type="checkbox"/> Preliminary Project Review	<input type="checkbox"/> Appeal
<input type="checkbox"/> Subdivision Map Review	<input checked="" type="checkbox"/> Commercial Design Review	<input type="checkbox"/> Other:

Project Address/Location: 999 Fremont Ave., Los Altos

Project Proposal/Use: Mixed use retail & residential

Current Use of Property: SALON

Assessor Parcel Number(s) 189-15-092 Site Area: 7929 sf

New Sq. Ft.: 20,949 sf Remodeled Sq. Ft.: NA Existing Sq. Ft. to Remain: 0

Total Existing Sq. Ft.: 524 sf Total Proposed Sq. Ft. (including basement): 23,871 sf

Applicant's Name: ALPHEUS W. JESSUP (Alp Jessup)

Home Telephone #: N/A Business Telephone #: 650 565 9036

Mailing Address: 4546 W. El Camino Real, Suite 223 (cell) 650-283-5902

City/State/Zip Code: Los Altos, CA 94022

Property Owner's Name: Gregg Bunker

Home Telephone #: N/A Business Telephone #: 408 558-3600

Mailing Address: 1900 Camden Ave

City/State/Zip Code: San Jose, CA 95124

Architect/Designer's Name: ALPHEUS W. JESSUP Telephone #: 650 565 9036

*** If your project includes complete or partial demolition of an existing residence or commercial building, a demolition permit must be issued and finalized prior to obtaining your building permit. Please contact the Building Division for a demolition package. ***

(continued on back)



May 20, 2015

Sean Gallegos
City of Los Altos Planning
Los Altos, CA

Subject: Design Review Responses

Re: 999 Fremont Avenue, Los Altos, CA
14-D-04, 14-UP-05 & 14-SD-01

Dear Mr. Gallegos and Members of the Planning and Transportation Commission:

This letter describes – briefly - the modifications made to the subject project in response to the comments and concerns voiced by members of the Commission and Planning Staff. Mr. Bunker, our client, is grateful for the City's continuing willingness to work with us to create a high-quality mixed-use project that can help in the movement to revitalize Loyola Corners. In addition, at the end of this letter please find our responses to staff comments we received early in May.

We believe that the modifications we've made in response to the Commission's feedback and Staff's recommendation are positive. We're hopeful that Staff and the Commission will be supportive of the project and that the Commission will pass it to Council with the Commission's support.

The original motion by Commissioner Baer to deny the project included five items. They are listed as follows along with a discussion of what's been done to address them:

- That the project was inconsistent with the General Plan and Loyola Corners Specific Plan. (We understand that some of the Commissioners would like to revisit the General and Loyola Corners Specific Plans. However, our plan complies with the General and Specific plans as they exist today.);
 - We would submit that the proposed project is wholly consistent with the General Plan and the Loyola Corners Specific Plan. At the PTC meeting in December, Mr. Elie Alcheck reminded the Commission and the attendees that a project like Mr. Bunker's was exactly what the Specific Plan Committee had in mind when the Specific Plan was developed for Loyola Corners and that it would have been approved without question then;
 - Since that meeting, the design for 999 Fremont includes, in addition to the two distinct retail venues at the ground floor totaling approximately 1,350 sf, 450 sf of basement storage to support the retail use. The existing retail facility on the

property is approximately 1,000 sf. The new 999 Fremont is increasing the retail onsite by approximately 40%. (This is a positive contribution to the commercial development component identified in the Specific Plan.)

- The design also includes 4 residential living units which add to the housing stock and help reinforce the idea that Loyola Corners will continue to be pedestrian and bicycle friendly.

(It should also be noted that the existing site consists of one retail space and an open air parking lot with less-than-ideal ingress and egress. The proposed facility will increase the retail component, shield the parking areas and improve safety, add to the parking stock, and create more visual interest around the building);

- That the size and massing appeared too large in scale with the surroundings;
 - We've taken the comments from the Commission and Staff to heart. The 3rd floor has been reduced in size by approximately 1,987 sf. There is now a shallow, vertical parapet at the 3rd floor roof. The roof garden and the elevated solar arrays have been eliminated. (It is the owner's intention to wire for the future installation of flat, roof-mounted PV panels.) A sloping roof has been incorporated at the second floor and serves as a guardrail for the 3rd floor roof decks;
 - The major finish materials have been simplified and are very modest in tone and consequently more in keeping with the village character that the Commission wanted us to incorporate.
- That the project lacked an appropriate attention to providing human scale elements. (This project is different from most others in that we have a significant hardship compared to the other properties in the vicinity. Mr. Bunker has already dedicated portions of his property to the City which constitute a public benefit. Given the constraints to the property we feel we have contributed significantly to providing human scale elements.);
 - While all three frontages include 5-ft sidewalks, landscape planters, street trees, the judicious use of decorative paving, the Fremont Avenue frontage is the most "public" of the building's facades and includes planter boxes, seating, bicycle parking, entrances to the retail facilities and public access to the residences above the ground floor. The other facades include street trees, planting areas as well.
 - The site is too compact to provide ramped underground parking. We are providing indoor protected parking for residents and the public. Public safety is a concern so ingress and egress of the parking garage is right turn in only and right turn out only.
- That the design needed to be unpretentious and unified in its design character;
 - The building has been simplified significantly. There are two primary building finishes (stucco and stone); colors are, across the board, more earth-toned and reflective of other older buildings in the greater Loyola Corners. The building is, we feel, unique but does, through the use of colors and textures, retain the village character, but, at the same time, contributing to the renewal of Loyola Corners.
- That the design would benefit from a unified architectural character, more sloping roof elements, a more prominent commercial appearance, a reduction in the number of building materials used, a greater attention to the immediate neighborhood character,

greater setbacks from the street for the second and third stories, and more sensitivity to the gateway site setting tone for the area's future commercial development.

- Much of what's listed in this bullet item is repeated from what came earlier. Nonetheless, there's merit to addressing the concerns listed.
- This is a mixed-use project. If executed well as we think we have, there will always be some positive tension between the uses, but we feel that the use of similar materials and colors (for example: in the finishes of the storefront sections and the residential windows and doors; the use of stone as both a field material and accent) does work to form a unified character;
- The sloping roof element from the original design is incorporated here but at the second, rather than the third floor. The colors of the concrete roof tile are also compatible with the rest of the building and with those remaining existing buildings along Fremont between A and B Streets
- The 3rd floor has significant setbacks along A and Fremont. There is also a setback along Miramonte though it is less. The square footage has been reduced by approximately 1,987 sf which has had a significant impact on the perceived bulk and mass. The sloping roof element has helped with defining and softening the 2nd floor. The landscaping has helped as well.
- Loyola Corners is truly the gateway to Los Altos from the south. As such, 999 Fremont is even more significant than the Clocktower building because the exit from the Expressway going north or crossing the bridge from the southbound direction puts one face to face with the building. The revised design is now prominent without being overbearing. It's colors and finishes are relatively modest and both "modern" and in keeping with the original character of the immediate environment.

In addition to the items noted above, we have been working with Staff to ensure consistency between the disciplines and obtain clearances from other agencies. It has, we feel, been a useful process.

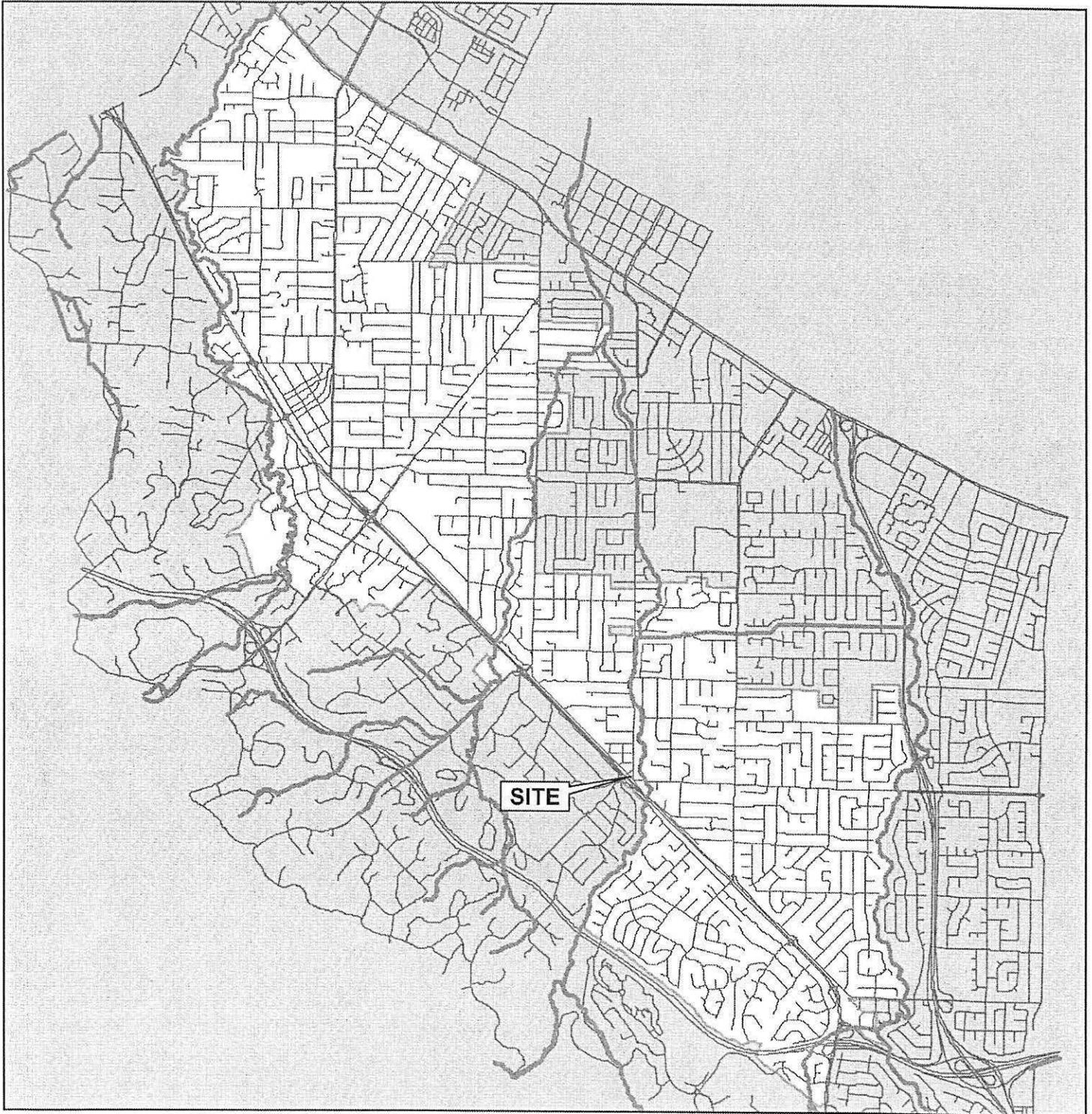
We really do appreciate the time and energy you have all devoted to working with Mr. Bunker and ourselves on this project. We think it's improved significantly through the combined efforts of all the participants. We're hopeful that you will be favorably disposed to approving our submittal.

Sincerely,

Alpheus W. Jessup, AIA
Architect

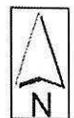
CC: Gregg Bunker

AREA MAP



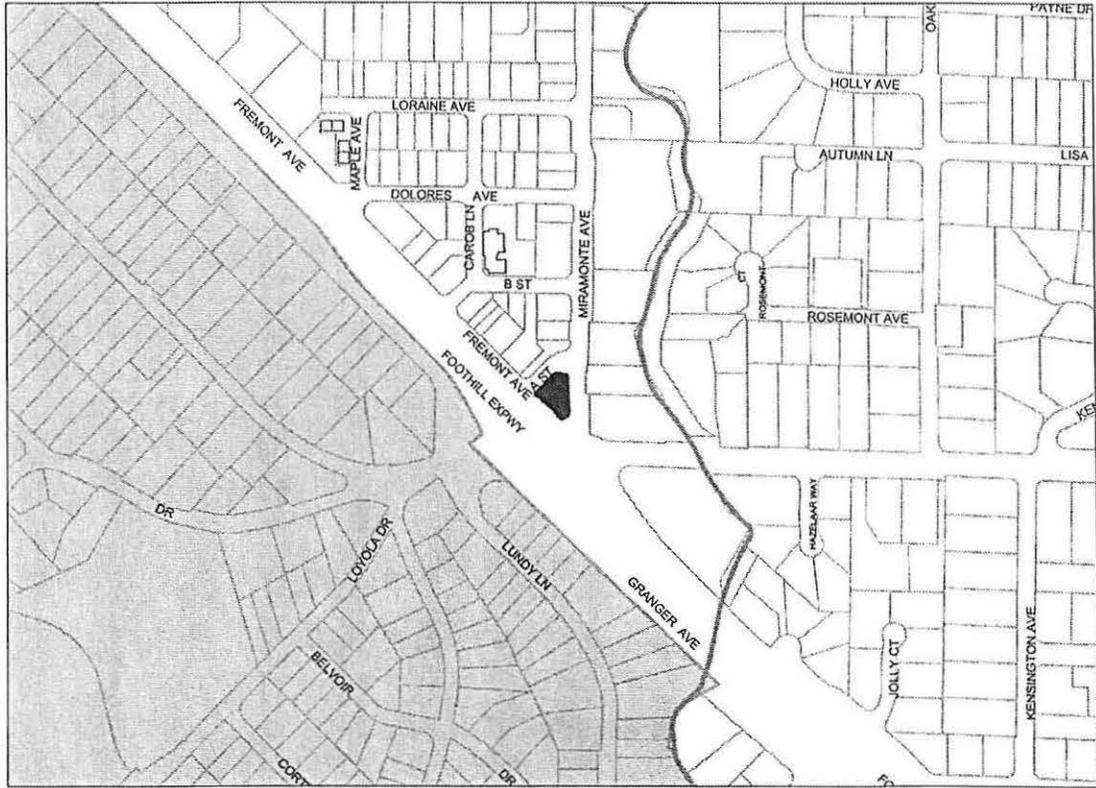
CITY OF LOS ALTOS

APPLICATION: 14-D-04, 14-UP-05, and 14-SD-01
APPLICANT: A. Jessup / G. Bunker
SITE ADDRESS: 999 Fremont Avenue

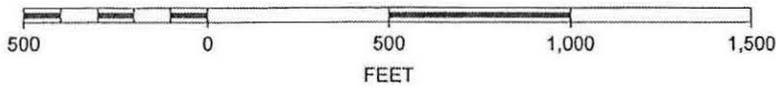


Not to Scale

VICINITY MAP



SCALE 1 : 6,000



CITY OF LOS ALTOS

APPLICATION: 14-D-04, 14-UP-05, and 14-SD-01
APPLICANT: A. Jessup / G. Bunker
SITE ADDRESS: 999 Fremont Avenue

APPROVED

ATTACHMENT C

Planning and Transportation Commission
Thursday, January 23, 2014
Page 1 of 2

MINUTES OF A STUDY SESSION OF THE PLANNING AND TRANSPORTATION
COMMISSION OF THE CITY OF LOS ALTOS, HELD ON THURSDAY, JANUARY 23,
2014, BEGINNING AT 6:00 P.M. AT LOS ALTOS CITY HALL, ONE NORTH SAN
ANTONIO ROAD, LOS ALTOS,
CALIFORNIA

ESTABLISH QUORUM

PRESENT: Chair MOISON, Vice-Chair BODNER, Commissioners BRESSACK, BAER,
LORELL and McTIGHE
ABSENT: Commissioner JUNAID
STAFF: Planning Services Manager KORNFIELD

DISCUSSION ITEM

1. 14-PPR-01 – G. Bunker – 999 Fremont Avenue

Pre-application design review for a mixed-use building concept including: a) 1,000 square feet of retail building area and 15 parking spaces at the ground level; and b) six, multiple-family condominiums at the second and third stories. *Project Planner: Kornfield*

Planning Services Manager KORNFIELD introduced the project and summarized the study session agenda report and noted that the intent of the meeting was to provide preliminary feedback to the applicant on the proposed architectural design and site planning for the project.

The project architect, Daryl Fazekas, explained the rustic Craftsman design concept and stated that he could provide parking sensors at the ground level to indicate whether the garage was full or not. Property owner and applicant, Gregg Bunker, stated that he was flexible on the driveway access, could provide a driveway on Fremont Avenue, could consider using lifts to increase the parking if necessary.

Four residents spoke about the project with the following comments:

- Issues with scale and mass;
- Should be low/rustic;
- Keep options open for a four lane County bridge;
- Unique parcel and opportunity for Loyola Corners;
- Low economy is a result of underdevelopment and encourages investment; and
- The building is too large for the lot, does not fit in, needs setbacks, and work out right-of-way issues.

There was no other public comment.

Commissioner BRESSACK stated that she had an ex parte meeting with the applicant.

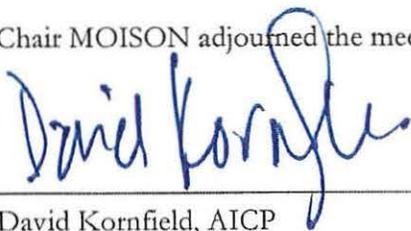
APPROVED

The Commission discussed the project and offered the following comments:

- Commissioner BRESSACK:
 - Good functions proposed (retail and housing) and area needs redevelopment;
 - The project needs some relief in the structure to fit-in better with the surroundings;
 - It is a landmark property needs “jewel” of a building;
 - The parking plan is marginal and creates a difficult circulation pattern;
 - Greater residential density should be considered and the potential for affordable housing;
 - The proposed design is too voluminous and anonymous and needs a recognition of the character of Loyola Concerns;
 - The project should provide better sidewalk and pedestrian access;
- Commissioner BAER:
 - Supports redevelopment at Loyola Corners;
 - Likes the design and mixed-uses but not as proposed on the site because the project is too large and out of character;
 - The design should be reconsidered from scratch;
- Commissioner LORELL:
 - Supports the design idea for mixed-use and creative parking solution, but the project appears too large and massive;
- Commissioner McTIGHE
 - The site is a gateway to Loyola Corners and needs special approach;
 - Supports a mixed-use concept at the site;
- Chair MOISON:
 - The retail should be limited to day-only use since the residential use relies on the commercial parking for visitors; and
 - If the building was set back and re-massed, then it would appear even taller in context.

ADJOURNMENT

Chair MOISON adjourned the meeting at 6:55 P.M.



David Kornfield, AICP
Planning Services Manager

ATTACHMENT D

MINUTES OF THE BICYCLE AND PEDESTRIAN ADVISORY COMMISSION OF THE CITY OF LOS ALTOS, HELD ON WEDNESDAY, AUGUST 27, 2014 AT 7:00 P.M. AT THE LOS ALTOS CITY HALL-COMMUNITY CHAMBERS, ONE NORTH SAN ANTONIO ROAD, LOS ALTOS, CALIFORNIA

PRESENT: Wes Brinsfield (Chair), Jim Fenton (Vice-Chair), Suzanne Ambiel, Chris Hlavka, Bill Crook, Bill Sheppard, Richard Baer, Cedric Novenario (Staff Liaison)

ABSENT: None

PUBLIC COMMENTS

None

ITEMS FOR CONSIDERATION/ACTION

1. Minutes

On a motion by Bill Crook, seconded by Suzanne Ambiel, the minutes of the Regular Meeting on June 25, 2014 is approved as amended. Passed 7-0.

2. Commercial Design Review for a new mixed use building at 999 Fremont Avenue

Planning staff and the developer provided a presentation and overview of the proposed redevelopment at 999 Fremont Avenue. The discussion focused on the potential improvements and impacts to bicycle and pedestrian facilities by the proposed development. Planning staff specifically requested for direction regarding accommodating bike and pedestrian facilities on Fremont Avenue.

The BPAC provided comments for consideration which include:

- Concerned that development will present sight distance issues
- Request improvements for bikes and pedestrians benefit to routes to school
- Want bike lanes on Fremont Avenue in Loyola Corners
- Want widest sidewalk possible on Fremont
- Bike rack capacity concerns and locations
- Realign ADA ramps with path of travel
- Bicycle and Pedestrian quality of service concerns
- Shadowing of buildings on traffic
- Red light running
- Concerned about use/placement of arc bike racks

Summary of BPAC direction

- The project should include wider sidewalks;
- The project should include bicycle lanes;
- The project should improve and realign ADA ramps;

- The project should improve line-of-sight issues at corners;
- The project should clarify the details/locations of Class I and II bicycle facilities; and
- TIA should evaluate the following:
 - Parking estimates to determine whether parking is sufficient for proposed uses.
 - Bicycle and Pedestrian Quality of service
 - A determination whether A Street should be one-way. If so, what is the impact to bicyclists and pedestrians, and line-of-sight.

Public Comments included:

- Bike crossing at A/Miramonte is a concern
- School commute from unincorporated Los Altos is a concern
- The building would impact line of sight at the intersection of A/Miramonte
- Red light running at intersection of Fremont/Miramonte impacts pedestrians and cyclists
- Review of vehicle crashes at the area, impact of sight distance of building

3. Springer Traffic Calming

Staff provided an overview and summary of the Springer Traffic Calming Community meeting on June 17, 2014. The BPAC reviewed a summary of comments received and group exercises. The BPAC provided comments ranging from:

- Concerned about how the data was collected and summarized
- Concerned about sample size of data (number of residents participating)
- Improve pedestrian facilities where possible
- Desire for sidewalks on Collector Streets
- No to 2-way bike lanes
- Parking enforcement needed
- Recommend implementing mock-up improvements as a test
- Consider improvements for route to schools
- Concerned of integrated wall look

Public Comment included

- Concerned about how the data was collected and summarized
- Concerned about how the meeting was conducted
- Concerned about medians narrowing the road and blocking access to driveways
- Request for improved speed enforcement
- Add more sidewalks
- Speed humps/tables are a hazard

Staff will consider these comments before making a presentation to the PTC.

4. Suggested Routes to Schools Map

Staff presented a draft of the all LASD and Montclair Elementary Suggested Routes to School Map. The commission provided general comments regarding:

- Adding bike lane and sidewalk facilities
- Add in BCS site locations
- Clarify where path connectors are located
- Improve map's color contrast
- Show bike/walk radius as circles
- Improve/ verify school boundary contrast

The remaining LASD schools and Montclair Elementary school will also reflect the comments provided. Revised maps will be presented to each school for additional comments.

5. BPAC Calendar

Staff presented the revised BPAC Calendar and provided overview into the upcoming meetings potential agenda items.

INFORMATIONAL ITEMS

6. Monthly Staff Reports

Staff liaison updated Commission on related City Projects.

ADJOURNMENT

Chair Wes Brinsfield adjourned the meeting at 10:16 p.m.

APPROVED

ATTACHMENT E

Planning and Transportation Commission
Thursday, December 4, 2014
Page 1 of 4

MINUTES OF A REGULAR MEETING OF THE PLANNING AND
TRANSPORTATION COMMISSION OF THE CITY OF LOS ALTOS, HELD ON
THURSDAY, DECEMBER 4, 2014, BEGINNING AT 7:00 P.M. AT LOS ALTOS CITY
HALL, ONE NORTH SAN ANTONIO ROAD, LOS ALTOS,
CALIFORNIA

ESTABLISH QUORUM

PRESENT: Chair BODNER, Vice-Chair JUNAID Commissioners BAER, MOISON,
BRESSACK, McTIGHE and LORELL

STAFF: Assistant City Manager WALGREN, Planning Services Manager KORNFIELD, and
Assistant Planner GALLEGOS

PUBLIC COMMENTS ON ITEMS NOT ON THE AGENDA

None.

ITEMS FOR CONSIDERATION/ACTION

CONSENT CALENDAR

1. Planning and Transportation Commission Minutes

Recommendation to approve the minutes of the November 20, 2014 regular meeting.

MOTION by Commissioner BRESSACK, seconded by Commissioner McTIGHE, to approve the minutes of the November 20, 2014 Planning and Transportation Commission regular meeting.
THE MOTION PASSED BY A 6/0/1 VOTE, WITH COMMISSIONER BAER ABSTAINED.

Chair BODNER recused herself due to financial interest at 1000 Fremont Avenue, which is within 500 feet of the subject property, and handed the gavel to Commissioner MOISON.

Vice-Chair JUNAID recused herself due to her design firm's involvement in the plans.

PUBLIC HEARING

2. 14-D-04 and 14-UP-05 – A. Jessup – 999 Fremont Avenue

Commercial Design Review, Use Permit, and Tentative Subdivision Map for a mixed-use project with five below-grade parking spaces, 1,345 square feet of commercial space and ten parking spaces on the ground floor, and five multi-family residential condominiums on the second story and third story. *Project Planner: Gallegos*

Assistant Planner GALLEGOS presented the staff report recommending continuance of Design Review, Use Permit and Subdivision applications 14-D-04, 14-UP-05 & 14-SD-01, subject to the recommended direction. He also made note of the late correspondence received.

Applicant and owner Gregg Bunker explained the project objectives and stated he would reduce the dwelling units to four to avoid the affordable housing requirement. Project architect Chip Jessup talked

APPROVED

about the pending changes to the neighborhood and discussed how the project met the required design review findings. Traffic engineer Brian Jackson from Hexagon spoke about the traffic trip generation study and noted that the parking layout was a good use for an awkward parcel and that the project improves safety by reducing the number of driveway access points. Noise consultant Joshua Roper made himself available to answer questions.

Residents Barbara Loebner, Katherine Wurzburg, Rick Walleigh, Andrew Pejack, Linda White, Pat Marriot, Kitty Uhler, Tom Ferry, Debbie Skelton, Teresa Morris, Richard Redelfs, and Maria representing Tom's Depot spoke in opposition to the project. The comments of concern included: the building is too massive and out of character; the retail spaces seemed small and the residential is not appropriate; the three-story character would set a bad precedent for the area; the purpose of the Specific Plan is to enhance the neighborhood character with commercial uses, not to providing housing with a parking lot; the project is located at an unsafe traffic intersection; the flat roof is out of character and would set a precedent of flat, boxy, three-story buildings; story poles should be used; the building would block views of the mountains and acts as a barrier that is not welcoming; the design should be smaller and more continuous in design; that noise should be carefully considered due to the echo created by the creek channel that affects the nearby residents; and the property owner did not respond to any neighbor concerns.

Resident Bill Shepparo spoke in support of the project citing minimal impacts to traffic and that the project would be good for the viability of the area.

Resident and property owner of 1000 Fremont Avenue, Elie Alcheck spoke in support of the project stating that he co-chaired the Loyola Corners Specific Plan Zone (LCSPZ) Committee; that this project is how to make the area more vibrant; that the project enhances the area; that there are no traffic or accident issues greater than other busy intersections; and that the times are different and the City should welcome the new development.

Property owner of 1000 Fremont Avenue, Mike Alcheck stated that the public input opposes change and the Specific Plan has failed. He supported the project and stated that we should embrace new buildings that will help meet the City's housing goals.

Project landscape architect Leah Drake spoke in support and discussed the planter boxes, flowering trees, and pavers that enhance sidewalk, and the roof water runoff will be filtered by the planter boxes. There was no other public comment.

Commissioners McTIGHE, LORELL, BAER, MOISON and BRESSACK all conveyed they had *ex-parte* meetings with the applicant and project architect.

The Commission discussed the project with the following concerns: that a project of this scale sets precedent for all CN districts (Foothill Crossings, Woodland Plaza, Loyola Corners and Rancho Shopping Center) that are mostly one-story; specific concerns about the prominence of the three-story massing; that the design is clever design and an interesting use of the property; that the Commission needs photo-realistic renderings showing the full context of the area; that this is challenging, prominent gateway site where the design guidelines trump the zoning requirements; that this is effectively the Commission's second meeting on the project and it has the same issues of bulk and mass; that the project fails to meet compatibility criteria in the Specific Plan and the General Plan; that the parking requirements constrain the development to a small retail building; that the Miramonte Avenue vehicular access is appropriate and that the retail frontage along Fremont Avenue was appropriate; that the landscaping concept was great, but the building needed more buffering; that the pedestrian and bike amenities were appropriate; that the small amount of retail may not serve the neighborhood well; and

APPROVED

that the applicant should return with a more appropriate development that is smaller, with fewer materials use, with more sloped roof elements and with more of a transition in massing.

Following discussion, Commissioner BAER asked whether the project should be denied or continued. Assistant City Manager WALGREN stated that if there are minor changes needed the project should be continued, but if major changes are needed then it would be appropriate for the Commission to deny it.

After receiving clarification the Commission continued its comments: that the project lacks coherent architectural style; that it looks too residential for commercial content; that too many materials are used; that it needs more of a sloped roof; that it needs to reflect an appropriate character as required by the Specific Plan but that the building limits of the Specific Plan were outdated; that it needs more setbacks from the sidewalk and from the second and third floors; that it needs more landscaping; that the bulk and mass needed reduction; that the project should have more retail; that the project should have more compatibility with the adjacent buildings such as with an overhang on Fremont; that the photovoltaic structure is too prominent on top of a building that is otherwise too tall; that locating the building's trash service on A Street is not appropriate; and that it is a gateway site that needs a more interesting design.

MOTION by Commissioner BAER to deny Design Review, Use Permit and Subdivision applications 14-D-04, 14-UP-05 & 14-SD-01 per:

- That the project was inconsistent with the General Plan and Loyola Corners Specific Plan;
- That the size and massing appeared too large in scale with the surroundings;
- That the project lacked an appropriate attention to providing human scale elements;
- That the design needed to be unpretentious and unified in its design charter;
- That the design would benefit from a unified architectural character, more sloping roof elements, more commercial space, a reduction in the number of building materials used, a greater attention to the immediate neighborhood character, greater setbacks from the street for the second and third stories, and more sensitivity to the gateway site setting tone for the area's future commercial development.

Through the Chair, Assistant City Manager WALGREN suggested that the Commission continue its review to the next meeting to allow staff the opportunity to prepare the denial findings.

MOTION by Commissioner BAER, seconded by Commissioner BRESSACK, to deny Design Review, Use Permit and Subdivision applications 14-D-04, 14-UP-05 & 14-SD-01 per negative findings to be prepared for review at the next meeting:

THE MOTION CARRIED UNANIMOUSLY.

COMMISSIONER'S REPORTS AND COMMENTS

Commissioners McTIGHE reported on the November 25, 2014 City Council meeting regarding the medical office moratorium and the December 2, 2014 City Council transition meeting.

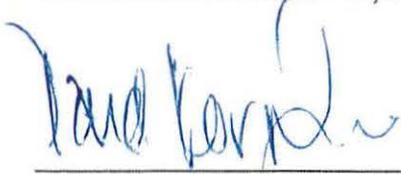
POTENTIAL FUTURE AGENDA ITEMS

Commissioner McTIGHE, and a majority of the Commissioners, suggested adding an item related to reviewing the traffic accident hot spots with input from Traffic Engineering Services Manager NOVENARIO to the 2014-2015 Planning and Transportation Commission Work Plan as well as updating the Loyola Corners Specific Plan including clarifying the application of the streetscape plan and the plan for A Street.

APPROVED

ADJOURNMENT

Commissioner MOISON adjourned the meeting at 9:38 P.M.



David Kornfield
Planning Services Manager

APPROVED

ATTACHMENT F

MINUTES OF A REGULAR MEETING OF THE PLANNING AND
TRANSPORTATION COMMISSION OF THE CITY OF LOS ALTOS, HELD ON
THURSDAY, JANUARY 15, 2015 BEGINNING AT 7:00 P.M. AT LOS ALTOS CITY
HALL, ONE NORTH SAN ANTONIO ROAD, LOS ALTOS,
CALIFORNIA

ESTABLISH QUORUM

PRESENT: Chair Bodner, Vice-Chair Junaid and Commissioners Baer, Bressack, McTighe, Moison and Lorell (arrived at 7:14 PM).

STAFF: Community Development Director Walgren and City Attorney Houston.

PUBLIC COMMENTS ON ITEMS NOT ON THE AGENDA

None.

ITEMS FOR CONSIDERATION/ACTION

CONSENT CALENDAR

1. Planning and Transportation Commission Minutes

Approve the minutes of the December 4, 2014 and December 18, 2014 regular meetings.

Motion by Commissioner Bressack, seconded by Commissioner Baer, to approve the minutes of the December 4, 2014 meeting with changes. The motion passed by a 4/0/2 vote, with Chair Bodner and Vice-Chair Junaid abstaining.

Motion by Commissioner Bressack, seconded by Commissioner Baer, to approve the minutes of December 18, 2014 meeting as drafted. The motion passed by a 5/0/1 vote, with Commissioner Moison abstaining.

Commissioner Lorell arrived.

PUBLIC HEARING

2. 14-UP-10 – R. Newman – 235 First Street

Consideration of a use permit for a wine and beer lounge in an existing commercial building.

Community Development Director Walgren presented that staff report recommending approval of Use Permit application 14-UP-10 for a new wine and beer lounge subject to the findings and conditions.

The project applicant gave a presentation on his business proposal and made himself available for questions. Jason Strubing (downtown business owner of Skate Works), and Los Altos residents Rosalind Bordo and Dik Lagerwerff spoke in support of the project. There was no other public comments.

APPROVED

Motion by Commissioner Moison, seconded by Commissioner Bressack, to approve Use Permit application 14-UP-10 per the staff report findings and conditions, with the following additional condition to identify the proposed hours of operation as a set restriction:

- The lounge hours of operation, per the applicant's business description, shall not be open before 7 am or after 1 am.

The motion carried unanimously (6/0).

DISCUSSION

3. 14-D-04 and 14-UP-05 – A. Jessup – 999 Fremont Avenue

Consideration of findings, continued from the last meeting on December 18, 2014.

The applicant presented draft revised project plans and requested a further continuance versus a motion to deny.

Following public comment by Los Altos residents Henry More and Teresa Morris in opposition to the project, the Commission deliberated on the process options available to them and voted 3-2 to continue the application for further consideration. Commissioner Baer and Bressack voted against the continuance, and Chair Bodner and Vice-Chair Junaid were recused because of project conflicts.

It was noted that the project would be re-advertised once complete plan submittals were provided and that future public hearings would be required for resident input.

COMMISSIONERS' REPORTS AND COMMENTS

Commissioner McTighe reported on the January 13, 2015 City Council meeting.

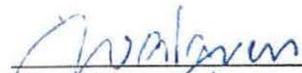
POTENTIAL FUTURE AGENDA ITEMS

Chair Bodner requested that a policy for successive Chair representation, in the case of both the Chair and Vice-Chair being unavailable, be scheduled for the next Planning and Transportation Commission meeting.

Commissioner McTighe requested that a draft 2015 Commission work program be scheduled for the next Planning and Transportation Commission meeting in order to prepare for the March 2015 City Council meeting.

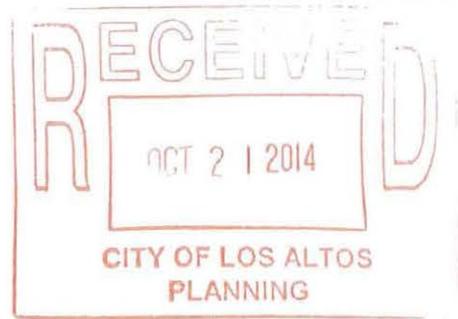
ADJOURNMENT

Chair BODNER adjourned the meeting at 9:30 PM.



James Walgren
Community Development Director

ATTACHMENT G



MEMORANDUM

date: 21 October 2014 **pages:** 5

name: Gregg Bunker **company:** Silicon Valley Business Center **email:** gregg@greggbunker.com
Cc: Chip Jessup **M Design Architects** awj@mdesignsarchitects.com

from: Shanna M. Sullivan and Joshua M. Roper, PE, LEED AP

subject: Loyola Corners, 999 Fremont Avenue – Los Altos, California
Environmental Noise Assessment
CSA project number: 14-0544

This memo summarizes our environmental noise assessment for the Loyola Corners mixed-use project in Los Altos, California. It quantifies the noise environment at the site and outlines the general level of mitigation that will be needed to meet City and State noise goals. Following is a summary of our findings:

1. Sound-rated windows and sliding glass doors will be needed at residences along Fremont Avenue to reduce transportation noise to the DNL 45 dB criterion in residences. Preliminary estimates suggest that sound insulation ratings may be STC 32 or lower.
2. Interior noise levels in the retail spaces are expected to meet the CALGreen criterion of Leq(h) 50 dB or lower with typical storefront systems.
3. Estimated future environmental noise levels for seated persons on balconies, and for people on the rooftop garden, are CNEL 65 dB or less, except at second floor balconies along Fremont Avenue. Transportation noise can be reduced at this location by increasing the height of glass noise barriers.
4. Estimated noise levels from the six rooftop air condensing units are within the City's Municipal Code limits at the nearest adjacent commercial and residential properties.

DESCRIPTION

The project consists of a mixed-use building with underground parking and storage, ground floor retail and parking, and five residential units divided between the second and third floors. Outdoor use space will be provided via a rooftop garden (including turf, walkways, and seating areas) and balconies on the second and third floors. The triangular site is located at the intersection of Fremont Avenue, Miramonte Avenue, and A Street. Foothill Expressway is parallel to and beyond Fremont Avenue, at a lower elevation that crosses below A Street. The site is currently occupied by a commercial building with at-grade parking.

ACOUSTICAL CRITERIA

- The General Plan identifies CNEL¹ 60 dB² and below as *normally acceptable* for residential land use, and areas with CNEL between 60 and 70 dB as *conditionally acceptable*. For commercial projects, the *normally* and *conditionally acceptable* categories are identified as CNEL 65 dB and below, and between CNEL 60 and 75 dB, respectively.
- The General Plan identifies CNEL 45 and 65 dB as indoor and outdoor noise goals for multi-family residential projects.
- Section 6.16.050 of the Los Altos Municipal Code limits noise levels at residential and commercial property lines to 50 and 60 dB, respectively. Equipment operating during daytime hours only may generate noise levels 5 dB higher.
- Section 5.507.4 of the California Building Code limits hourly average interior noise levels to $L_{eq}(h)$ ³ 50 dB in non-residential spaces.

NOISE ENVIRONMENT

Existing Noise Environment

The primary noise source at the site is traffic from Foothill Expressway and the surrounding streets. To quantify the existing noise environment, a multi-day monitor continuously measured noise levels at the site between 29 September and 1 October 2014. In addition, short-term measurements were conducted at two heights to determine how the noise levels vary at different locations and elevations. Table 1 (below) summarizes measured noise levels, and Figure 1 (attached) shows the approximate measurement locations.

Table 1: Measured Noise Levels⁴

Monitor	Location	CNEL	$L_{eq}(h)$ Range During Business Hours (7am-10pm)
L1	25' northeast from Fremont Ave 25' southeast from A Street 12' above grade	68 dB	56 to 69 dB*
S1	95' northeast from Fremont Ave 40' west from Miramonte Ave 5'/16' above grade	62/64 dB	-

*An hourly $L_{eq}(h)$ of 74 dB was measured when an emergency vehicle drove past the site during the 14:00 hour on 29 September. Since this appears to be atypical, it was excluded from this analysis.

Acoustics
Audiovisual
Telecommunications
Security

100 W. San Fernando
Suite 430
San Jose, CA
95113
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www.cmsalter.com

- 1 Community Noise Equivalent Level (CNEL) – A descriptor for the 24-hour A-weighted average noise level. The CNEL concept accounts for the increased acoustical sensitivity of people to noise during the evening and nighttime hours. Sound levels during the hours from 7 pm to 10 pm are penalized 5 dB; sound levels during the hours from 10 pm to 7 am are penalized 10 dB. A 10-dB increase in sound level is perceived by people to be a doubling of loudness.
- 2 A-Weighted sound pressure level (dB) represents the noisiness or loudness of a sound by weighting the amplitudes of various acoustical frequencies to correspond more closely with human hearing. A 10-dB (decibel) increase in noise level is perceived to be a doubling of loudness. A-Weighting is specified by the U.S. EPA, OSHA, Caltrans, and others for use in noise measurements. All noise data in this report are A-Weighted.
- 3 $L_{eq}(h)$ – The equivalent steady-state A-weighted sound level that, in an hour, would contain the same acoustic energy as the time-varying sound level during the same hour.
- 4 Noise measurements were conducted with Rion NL-22 and NL-32 Class 2 sound level meters.

Future Noise Environment

The Noise Section of the Los Altos General Plan includes Tables NEH-2 and NEH-3, which present existing and future year 2025 traffic noise contours. Based on these tables, it appears that transportation noise will increase by 1 dB or less in the future.

ANALYSIS AND RECOMMENDATIONS

Estimated future noise levels at the site range from below CNEL 60 dB on the shielded rooftop garden to CNEL 70 dB at the upper floors along Fremont Avenue. Future hourly average noise levels range from Leq(h) 57 to 70 dB at the retail locations along Fremont Avenue. These are within the *normally* and *conditionally acceptable* categories for land use compatibility, as outlined in the Los Altos General Plan. Environmental noise should be reduced to CNEL 45 dB or lower in residences, Leq(h) 50 dB or lower in retail spaces, and CNEL 65 dB or lower in outdoor use spaces. Outdoor mechanical equipment should be selected and designed to meet the criteria outlined in the City's Municipal Code.

Exterior-to-Interior Noise

Residences

Based on the noise levels identified above, and architectural drawings dated 10 September 2014, sound-rated windows and exterior doors will be needed at residences along Fremont Avenue to reduce traffic noise to the DNL 45 dB criterion indoors. Preliminary estimates suggest that windows and doors with sound insulation ratings up to approximately STC 32 will be needed. For reference, typical dual pane construction-grade windows and sliding glass doors provide sound insulation of STC 26 to 28. Preliminary estimates suggest this will suffice at units in other areas of the site. These preliminary estimates assume that exterior walls will be equivalent to three-coat stucco over wood sheathing, wood studs with insulation in stud cavities, and one layer of gypsum board. The design team will determine final window and door sound insulation ratings prior to permit submittal.

Commercial Retail

Drawings show two retail spaces along Fremont Avenue. As indicated above, average hourly noise levels are expected to be approximately Leq(h) 57 to 70 dB at these locations. For reference, typical storefront glazing assemblies reduce noise by 20 to 25 dB, and exterior walls generally provide a higher level of sound insulation. Therefore, typical storefront glazing assemblies, in combination with commercial exterior walls, are expected to reduce transportation noise to the Leq(h) 50 dB criterion indoors.

Outdoor Noise

Outdoor noise levels will vary, depending on location and exposure to the adjacent roadways. Drawings show solid glass railings at second and third floor balconies, and a five-foot tall parapet at the roof level.

- *Balconies* – Based on the noise levels identified above, and shielding from the planned glass railings, estimated future noise levels for seated persons on balconies are approximately

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CNEL 67 dB at the second floor balconies along Fremont Avenue, and CNEL 65 dB or lower in other locations. Outdoor noise levels can be reduced by increasing the height of glass barriers.

- *Rooftop Garden* – Based on the noise levels identified above, and shielding from the planned parapet wall, estimated future transportation noise is CNEL 65 dB and below on the rooftop garden. This is consistent with the City's goal for this type of space as designed.
- *Noise Barriers* – Effective noise barriers are generally solid with no cracks or gaps, and have a surface density of approximately three pounds per square foot. The design team will review the planned glass barriers during the design phase to be sure the desired noise reduction will be provided.

Mechanical Equipment Associated with the Project

Drawings show a mechanical equipment area near the center of the roof. The mechanical engineer has indicated that it will house six split system air condensing units, each generating an A-weighted sound power level of 69 dB. The nearest residential and commercial receiving properties appear to be approximately 300 and 60 feet from these units. Including shielding from the roof parapet, estimated noise levels from these condensing units when operating simultaneously is 35 dB or below at the adjacent properties, which is within the Municipal Code limit. Note that the parking areas will be ventilated naturally (without fans).

Please call with any questions.

*

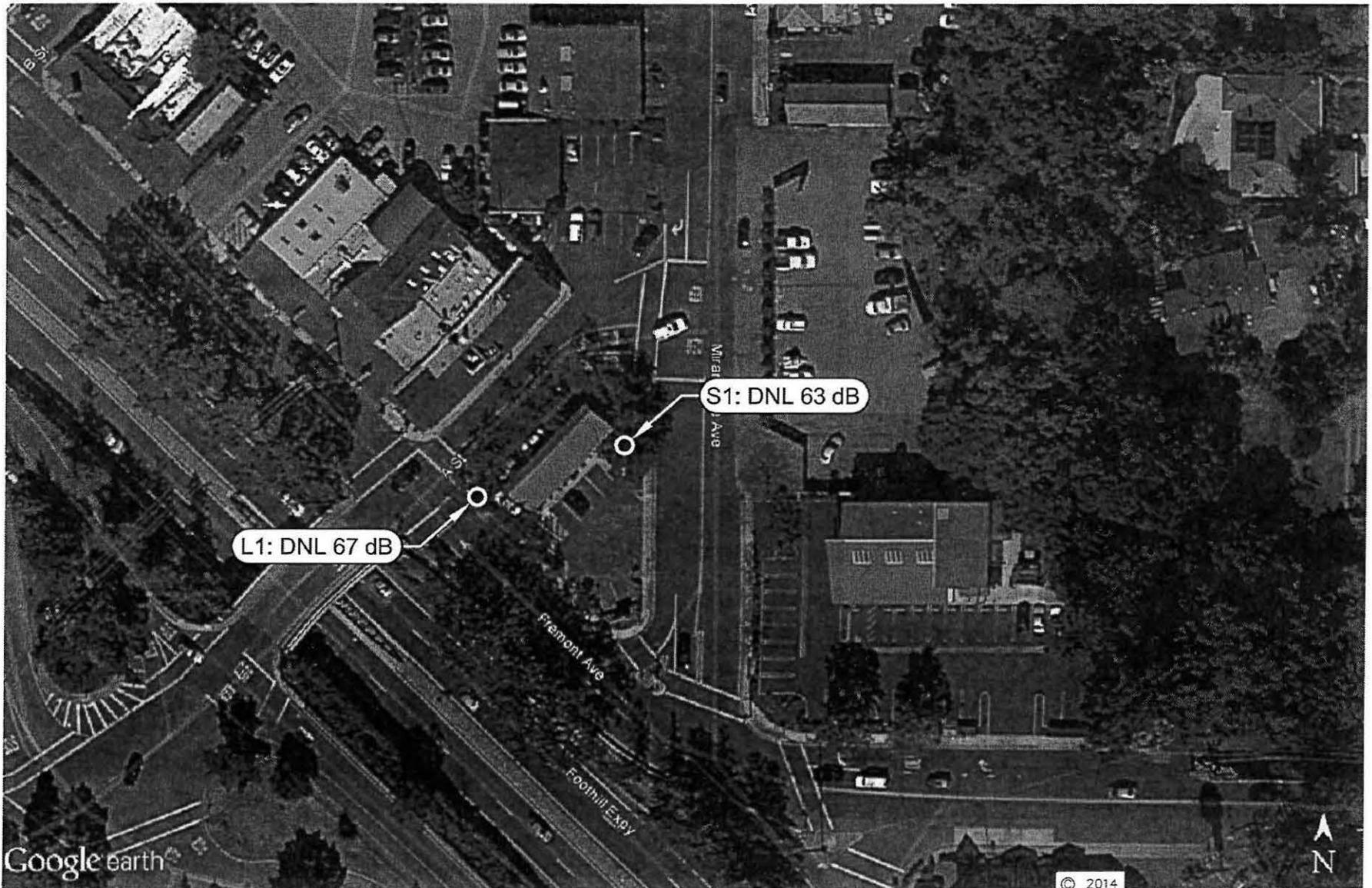
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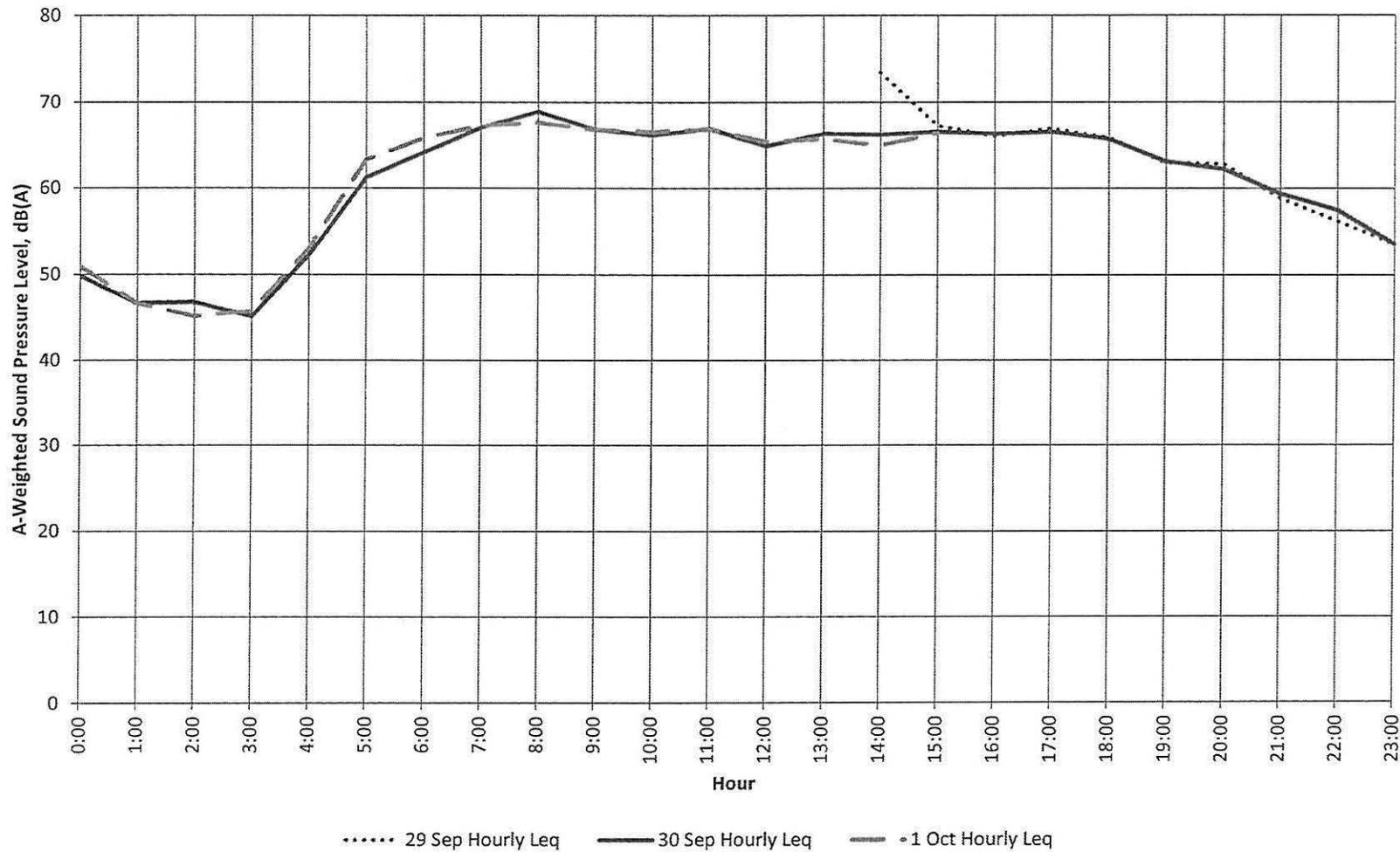
LOYOLA CORNERS (999 FREMONT AVENUE)
MEASUREMENT LOCATIONS AND MEASURED DNL

FIGURE 1

CSA #
14-0544

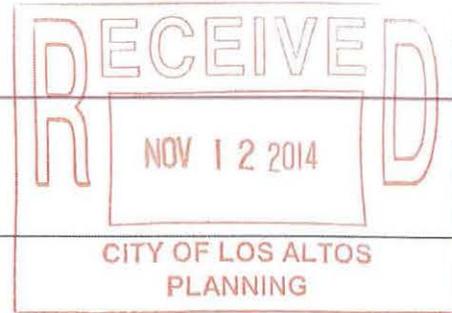
SS/PNS
10.09.14

Loyola Corners - 999 Fremont Avenue, Los Altos, CA
29 September to 1 October 2014, Hourly Leq
Corner of Fremont Avenue and A Street Monitor





HEXAGON TRANSPORTATION CONSULTANTS, INC.



MEMORANDUM

TO: Mr. Gregg Bunker
 FROM: Brian Jackson
 DATE: September 10, 2014
 SUBJECT: *Trip Generation Study for the Proposed Mixed-Use Project at 999 Fremont Avenue in Los Altos, California*

Hexagon Transportation Consultants, Inc. has completed a trip generation study for a proposed mixed-use development in Los Altos, California. The triangular-shaped project site is located at 999 Fremont Avenue. The proposed project would construct 5 condominium units and 1,425 square feet (s.f.) of retail space. The project would receive credit for the removal of the 1,100 s.f. Loyola Beauty Salon, which currently operates on the site.

The City of Los Altos typically does not require a comprehensive traffic study that includes an intersection level of service analysis if a project is projected to generate fewer than 50 daily vehicle trips, as identified in Section C.8 of the circulation element of the General Plan. For projects that would generate fewer than 50 daily trips, a simple trip generation analysis usually will suffice. The reason the City typically does not require more extensive traffic analysis for "small" projects, including intersection level of service, is because once the project-generated peak hour trips are assigned to the roadway network based on the inbound/outbound splits, the trips disperse and the number of new trips added to any intersection is effectively negligible. This approach to intersection level of service analysis has become standard procedure in the City of Los Altos.

Hexagon prepared project trip estimates based on trip generation rates obtained from the *ITE Trip Generation Manual, 9th Edition*. After applying the ITE rates and trip credits for the existing use to be removed, the project would be expected to generate 43 daily vehicle trips, with 2 trips occurring during the AM peak hour of traffic, and 4 trips occurring during the PM peak hour of traffic. Based on the ITE-recommended inbound/outbound splits, it is estimated that the project would generate 0 inbound trips and 2 outbound trips during the AM peak hour, and 2 inbound trips and 2 outbound trips during the PM peak hour (see Table 1 below).

**Table 1
Project Trip Generation Estimates**

Land Use	Size	Units	Daily Rate ¹	Daily Trips	AM Peak Hour			PM Peak Hour				
					Rate ¹	In	Out	Total	Rate ¹	In	Out	Total
Condominiums ²	5	Units	5.81	29	0.44	0	2	2	0.52	2	1	3
Retail ³	1,425	SF	42.70	61	0.96	1	0	1	3.71	2	3	5
Gross Trips:				90		1	2	3		4	4	8
Existing Salon ³	1,100	SF	42.70	47	0.96	1	0	1	3.71	2	2	4
Net Project Trips:				43		0	2	2		2	2	4

Notes:

- ¹ Rates expressed in trips per dwelling unit for residential and trips per 1,000 square feet for retail.
- ² Source: "Residential Condominium/Townhouse" (230) *ITE Trip Generation, Ninth Edition, 2010, average rates.*
- ³ Source: "Shopping Center" (320) *ITE Trip Generation, Ninth Edition, 2010, average rates.*



Mr. Gregg Bunker
September 10, 2014
Page 2 of 3

Figure 1 shows the estimated project trip distribution patterns and the peak-hour project trip assignment at the intersections of Miramonte Avenue/A Street, Fremont Avenue/A Street and Miramonte Avenue/Fremont Avenue. The trip distribution patterns were estimated based on existing travel patterns on the surrounding roadway system and the locations of complementary land uses. Since the amount of estimated peak-hour vehicle trips added to the roadways in the vicinity of the project site would be small, the project would not produce a noticeable change in traffic volumes in the study area. Thus, it is our professional opinion that this project does not warrant preparation of a comprehensive traffic study.

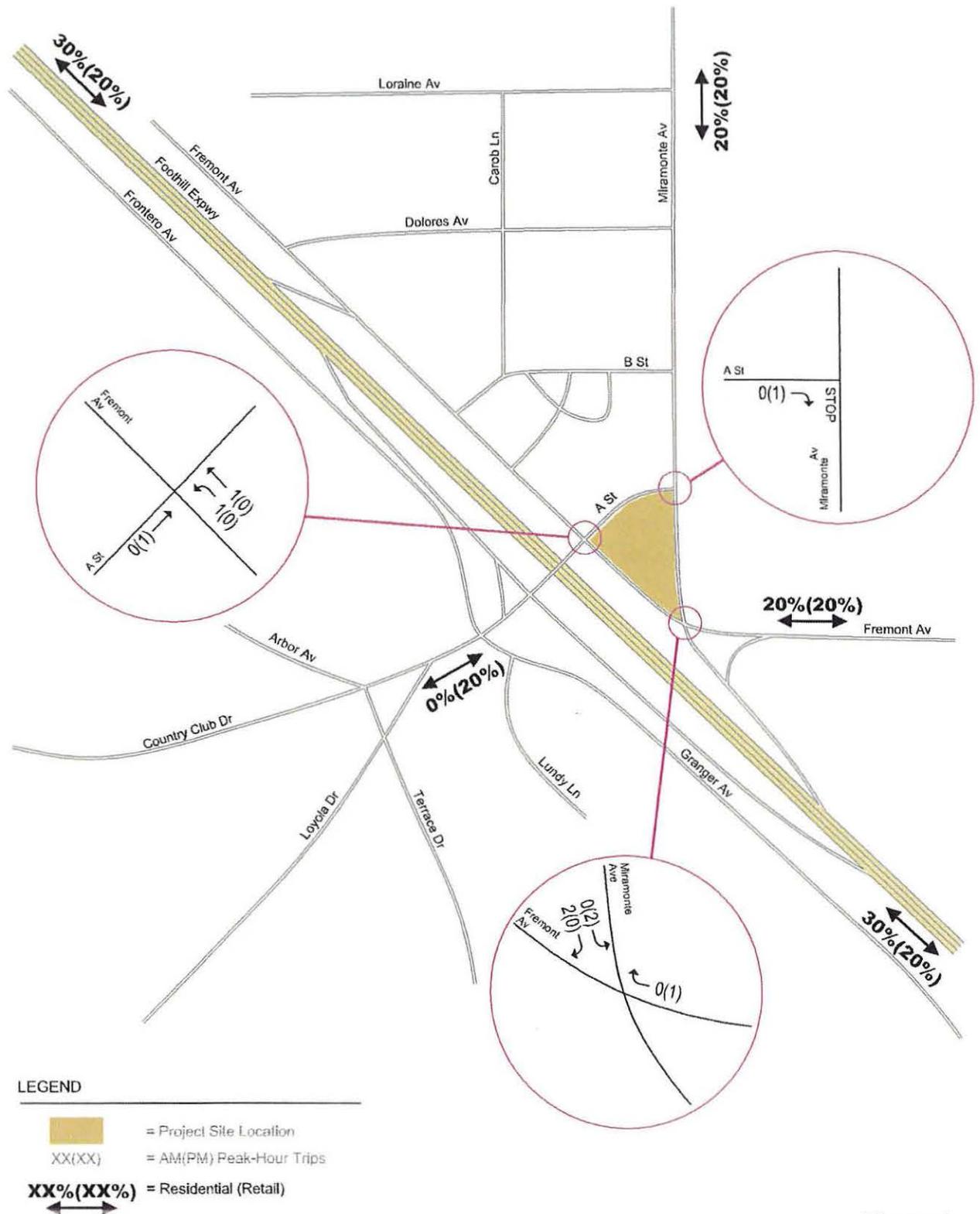
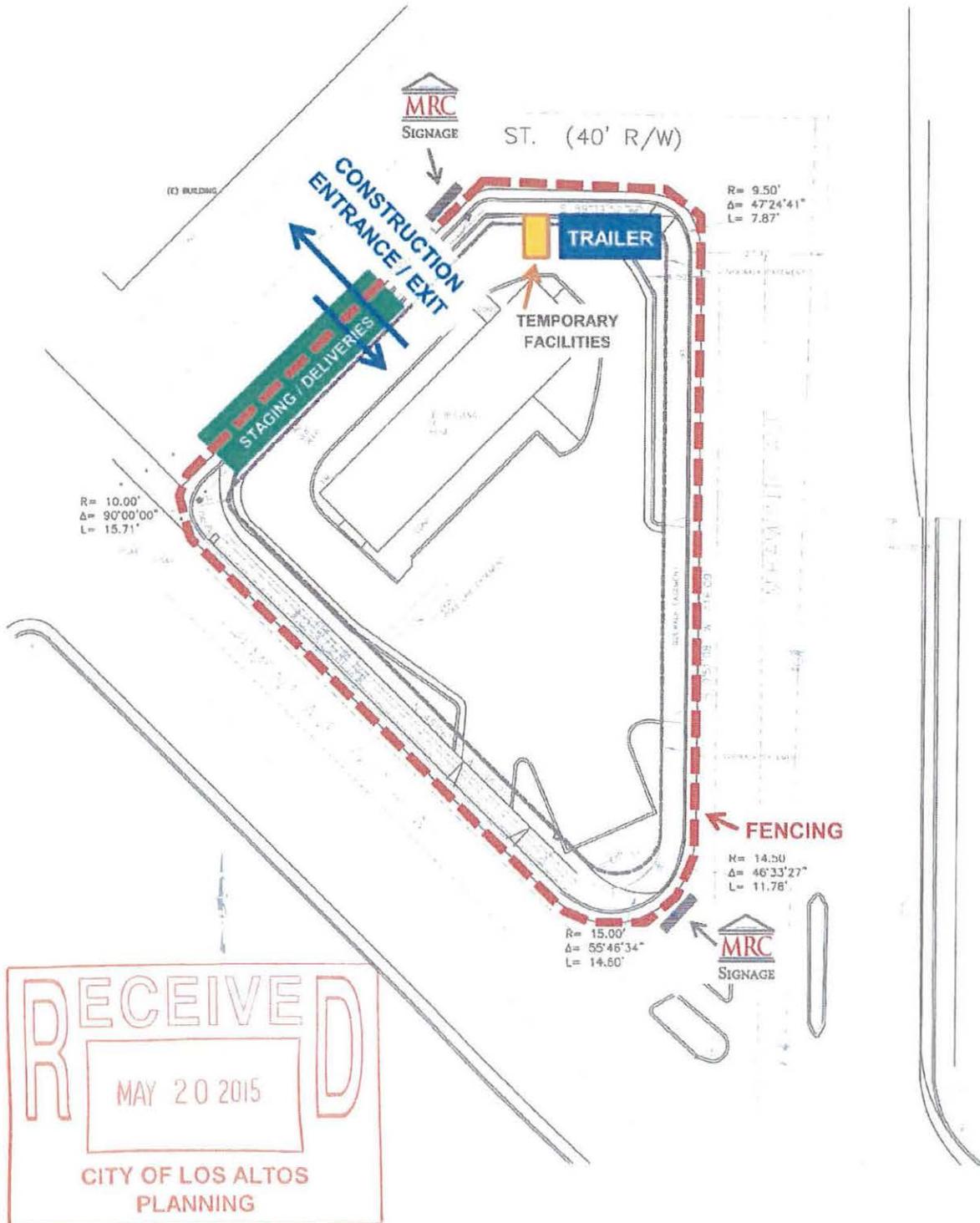


Figure 1
Project Trip Distribution and Trip Assignment

ATTACHMENT I

	DATE: MAY 19 th , 2015
	AUTHOR: VILMOS PALKÓ, JR.
	SITE LOGISTICS PLAN
	LOYOLA CORNERS, 999 FREMONT AVENUE LOS ALTOS, CA 94024



RECEIVED
MAY 20 2015
CITY OF LOS ALTOS
PLANNING

	<p>DATE: MAY 19TH, 2015 AUTHOR: VILMOS PALKÓ, JR. OFF SITE PARKING PLAN</p>
	<p>LOYOLA CORNERS, 999 FREMONT AVENUE LOS ALTOS, CA 94024</p>

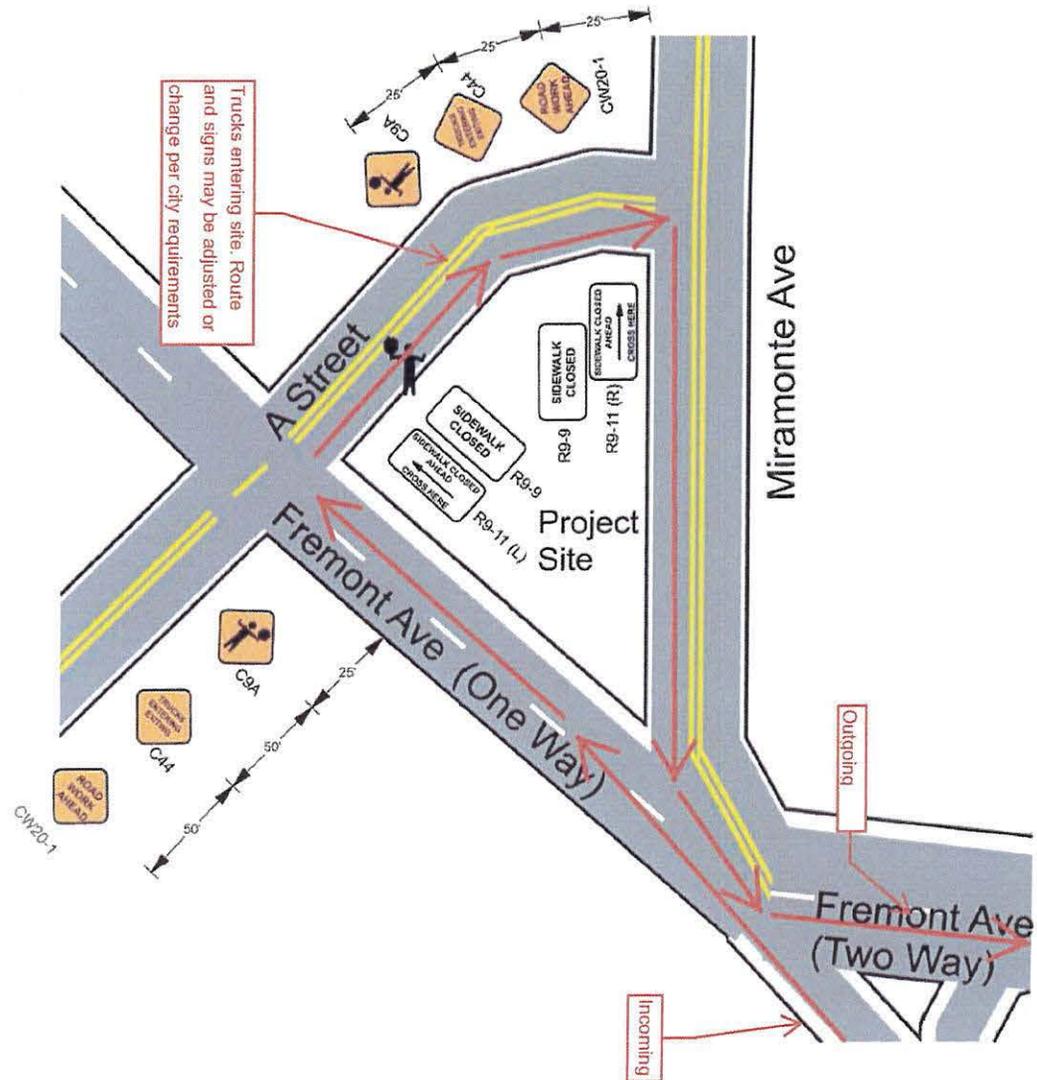



 = PROPOSED OFF-SITE PARKING, PENDING PERMISSION FROM BUILDING OWNER(S)



DATE: MAY 19TH, 2015
 AUTHOR: VILMOS PALKO, JR.
 SITE TRAFFIC FLOW

LOYOLA CORNERS,
 999 FREMONT AVENUE
 LOS ALTOS, CA 94024



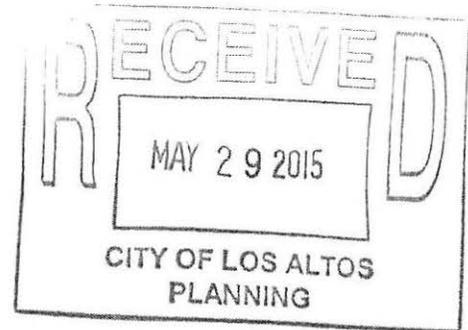
ATTACHMENT J

May 28, 2015

Richard C. Walleigh

Los Altos, CA 94024

Los Altos Planning and Transportation Commission
C/O Mr. James Walgren
Los Altos City Hall
1 North San Antonio Road
Los Altos, CA 94022



Dear Commissioners:

My wife and I will be traveling and unable to attend the public hearing on June 4th regarding the proposed development at 999 Fremont Avenue, but we want to clearly communicate our objections to the proposed project.

Since Mr. Bunker has made only minor revisions to the plans that have been twice rejected by the Commission, I don't want to waste your time. If you are already committed to rejecting the proposed development, you don't need to read further. Otherwise:

The proposed structure is totally unsuited to the location. It's mass and bulk jutting straight up from the edge of the sidewalk are totally inappropriate and totally inconsistent with the neighborhood. It violates the Loyola Corners Specific Plan and appears to be illegal relative to the Municipal Code. The area is designated primarily for retail and the proposed building has minimal space allocated for retail. If a reasonable portion of the building were designated for retail, the parking could not comply with code requirements. Many people think that the proposed parking is already inadequate, and the proposed spaces are only able to be provided through the trick of stacked parking. This contortion to apparently squeeze into the code requirements will cause delays for residents entering the building, resulting in traffic backups and increased traffic confusion in a location that is already the site of frequent accidents.

If you are not yet convinced to reject the proposed structure, we suggest that you require the erection of story poles on the lot to demonstrate to yourselves and anyone who passes by how unreasonable the construction of a building of this height and bulk would be.

Assuming the city will never be able to purchase the site, we are not opposed to reasonable development. However, regarding any future development, it should be pointed out that this site was once a gas station with underground tanks, and most underground tanks leak. So before any future excavation starts spewing carcinogens into the air, there should be a thorough investigation of the soil to determine the level of contaminants.

Thank you for your attention,

A handwritten signature in cursive script that reads "RC Walleigh".

David Kornfield

Subject: FW: Loyola Corners Proposal

From: Heather Larkin [<mailto:hmlarkin@sbcglobal.net>]

Sent: Thursday, May 28, 2015 4:33 PM

To: James Walgren

Cc: Jan Pepper

Subject: Loyola Corners Proposal

Dear Mr. Walgren and Commissioners,

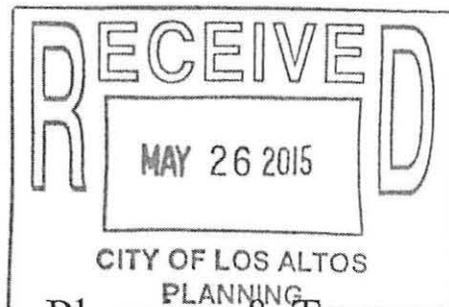
Public Notice in the Los Altos Town Crier indicates that the building proposed for the Loyola Corners triangle will be on the June 4 agenda.

I am writing to request that the Planning and Transportation Commission, once again, strongly deny the proposal. The applicant continues to propose a massive 3 story structure. This large structure is the opposite of the character of Loyola Corners. I have lived in the Loyola Corners neighborhood long enough to know that, because this a historical area, a specific plan was designed. The plan's focus was not only to retain the historical quaint character of the area, but to add even more charm-banners, parklets.. The proposed structure would destroy any hope of maintaining and celebrating the historical character of Loyola Corners. Four condominiums= 4-8 residents. A massive 3 story structure for 4-8 residents!

Draeger's Market has large signage/illustration to show the proposed development at that site. The Loyola Corners site has only a small faded letter sized paper. I request that a large sign with illustration be placed at Loyola Corners. Anything less, makes the upcoming proposal seem like a covert operation.

Sincerely,
Heather Larkin
Oakhurst Avenue

To:



May 26,2015

Los Altos ~~Planning & Transportation~~

Commission Members,

Well, nothing has changed on the Commercial Design Review of A.Jessup building on 999 Fremont Ave. Property. It's almost identical to the design of July 2014. It's still the same massive building design as before. Except for what looks like some trees in pots (I hope) on one side of the roof, some taller trees across the Fremont side, hiding the building. A couple of new windows on the second story in the corner of A Street and Fremont Ave. Inside there's on less Condo, one less parking space, and four hundred & forty four square feet of additional commercial space. This building is still to BIG for a fit in the Loyola Corners area. It has no consistency with other buildings near by. Renters would be limited to one car a piece and very few people in this day & age have one car per family. Also other businesses in the area would suffer because of parking, as their's would probably be used by the new occupants of this large building. This owner should NOT be given a permit to build a three story building on this site or any in the Loyola Corners area. Build your large or massive building in Down-Town Los Altos where there is more parking.....

Unless you live in the Loyola Corners area you

don't really know what the traffic is like. It's terrible
In the AM & PM between 7am-9am and again at about
2:45pm-6pm. The signal light only lets a few cars go
Across the Bridge at a time & many cars go through
RED lights. It's been a short while since someone has
driven into Tom's Depot on the corner of Fremont Ave.
next to where this proposed building site ,but it has
happened before & probably will again. I've lived in
this area since 1965 and there's always some kind of
accident in the area around Loyola Corners. Many of
them didn't require the Police, Fire, or Ambulance
to be there. They settled it between the drivers
themselves because they were just fender benders. We
hear them all the time and neighbors meet on the
Bridge when this happens. Maybe the widening of the
Loyola Bridge will lessen these occurrences.

Any business whether it be hair salon, restaurant,
or whatever needs lots of parking. The existing
businesses of Loyola Corners might be severely hurt
by the proposed new commercial businesses due to
parking shortage. Please request this building site only
build a maximum of two stories only with as much
parking that can be provided.

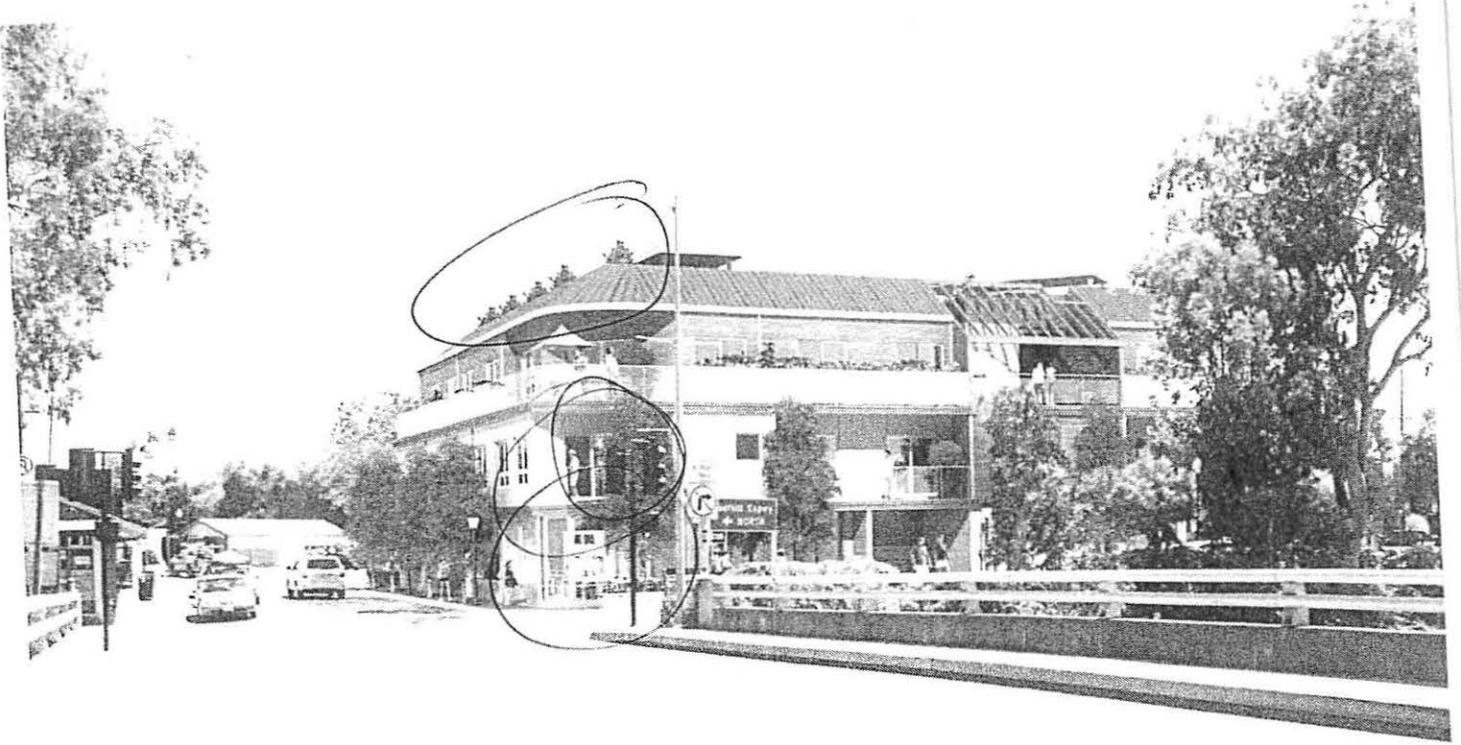
Thank You,

Virginia Johnson
a Lundy Lane
resident

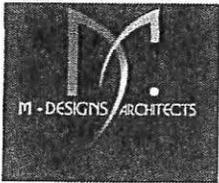


M
M - DESIGNS ARCHITECTS

June 4, 2015



*trees on roof & extra window
on second floor*

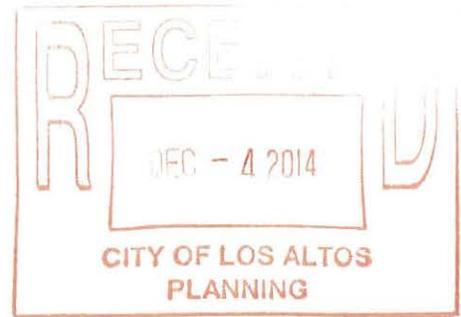


Tree on roof & more trees down Fremont Ave



Debbie Skelton

Los Altos, Ca 94024



Dear Commissioners and Mr. Walgren:

I am sending you this letter to express my concern about the developer's plans for 999 Fremont Avenue. My home is less than one block from the proposed development.

I am concerned with the overall height of the building. The other commercial buildings in the area have peaked roofs, with the height maximum only at the peak. The developer is proposing a flat top 30' roof (33' in some areas), from corner to corner. Once this is approved, precedence is set. Other property owners then will propose 30' tall, 3-story buildings, with flat top roofs so they too can maximize their profit. Within several years, Loyola Corners will lose its quaint feel. It will become a triangle of tall boxy buildings. Loyola Corners itself is small in scale. Please consider current precedence and the Loyola Corners Specific Plan. The tallest buildings hit their height with a very small percentage of their roofline.

The proposed building is too bulky. The building has many large, flat, wall surfaces. This is something we want to stay away from. Most 2-story buildings in Loyola Corners have a second story setbacks and peaked roofs. These features add to the quaint feel and charm of Loyola Corners.

The staff report states, "the project proposes a relatively high floor area ratio of 279 percent." The staff report also states, "While the project has design integrity and high quality materials, staff could not make the design review findings that the project has an appropriate relationship to other structures in the immediate area in terms of scale and bulk." And, "the three-story massing continues to appear significantly out of character with the predominantly one- and two-story high structures in the immediate context."

This means to me that City Staff believes this building is too bulky and too tall, and won't fit in at Loyola Corners. This is the same message Mr. Bunker has heard repeatedly - both from City Staff and from the local residents. Mr. Bunker owned the property when the Specific Plan was drafted. There were many meetings and much publication during the process. It should be no surprise to Mr. Bunker that the community would be more accepting of a two-story building with a second story setback, and an attractive roofline. A building of this size and density would fit in with its surroundings and enhance the community.

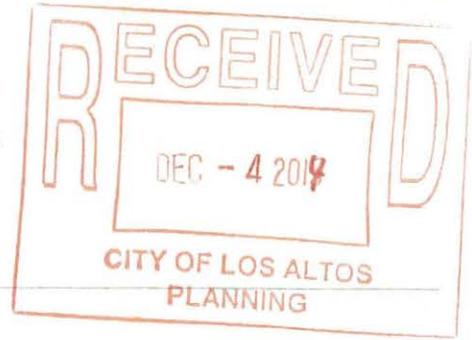
Although the proposed plan meets parking requirements, there are unique issues including no adjacent street parking and awkward circulation problems.

- There is no adjacent street parking and nearby street parking is limited. Any street parking will require pedestrians to walk a fair distance, and cross streets. The proposed crosswalks are not convenient to street parking on Miramonte, (in front of the Water District or Post Office). So naturally, people will dart across the street outside of the designated crosswalk.
- I am opposed to the garage lifts that provide double car parking. There is limited space to maneuver. Vehicles will be backed up on Miramonte waiting for the lift to operate, and the vehicle to turn around in limited space. Parking and entering/exiting the garage will be awkward.
- I am also opposed to the lifts because I expect the building will stand 50-75 years. The lifts will need to be replaced several times during the life of the building. Retrofitting cannot be predicted. There is a good chance these double car parking spaces could be converted to single car spaces sometime during the life of the building.
- There is only one entrance/exit to the parking garage. The layout causes circulation issues. If spaces are full, a vehicle that enters will have to back out onto Miramonte, then find alternative parking.

You may know that the lot held a gas station in the '50s and '60s. Once the station closed, the lot stood empty for over 15 years. About 32 years ago, Mr. Bunker bought the lot for a less than \$200,000 after it had been on the market many years. He developed it, and it has been home to several businesses. Given his investment, Mr. Bunker should be able to reduce the mass of his proposed development, and submit a plan that enhances Loyola Corners with an appropriate relationship to the surrounding buildings. Two businesses, and four or five condominiums on a 7,348 square foot lot is too much density for the area. When I stand on the lot, it's hard to imagine that 20,949 square feet could ever fit on this postage stamp sized lot. This proposed development is too tall, too bulky and too dense for this lot, and it does not fit in on Loyola Corners.

Sean Gallegos

From: awj@mdesignsarchitects.com
Sent: Thursday, December 04, 2014 3:44 PM
To: Sean Gallegos
Subject: RE: 999 Fremont Ave. Please forward to PTC, etc



Thank you, Sean.

From: Sean Gallegos [mailto:sgallegos@losaltosca.gov]
Sent: Thursday, December 04, 2014 1:11 PM
To: Alpheus W Jessup (awj@mdesignsarchitects.com); 'Gregg Bunker'
Subject: FW: 999 Fremont Ave. Please forward to PTC, etc

Chip and Gregg,

I have received a letter of support for your project, and it's attached for your review.

Thanks,

Sean

From: Kacey Fitzpatrick
Sent: Thursday, December 04, 2014 9:39 AM
To: David Kornfield; Sean Gallegos
Subject: re: 999 Fremont Ave. Please forward to PTC, etc

Dear David, Sean, Planning staff and PTC members:

I support your approval of the mixed use project at Loyola Corners/ 999 Fremont Ave. Please consider.

I think this is a great project that will enhance the neighborhood and the retail presence in South Los Altos.

- Loyola Corners needs upgrading and redevelopment
- A quality mixed-use building would be a needed impetus for encouraging additional investment in this southern gateway to Los Altos
- It replaces a tired and worn-out building and makes better use of the difficult property to create improvements in the area:
 - It simplifies access to the parcel and limits conflicts between motorists, pedestrians and bicyclists
 - It increases the available parking and takes it off the street
 - It improves the retail component
 - It adds to the housing stock
 - It improves walkability in this area with pedestrian amenities and feet on the street that will support the local businesses
 - It has a great mix of materials including Hardi siding that will blend with the local palette while elevating design appeal
 - The green walls and other landscape elements are innovative and appealing on many levels
 - The green building elements and roof top space are community benefits though privately used
 - This project will spur additional investments, which will ultimately improve vibrancy and activity in the district, necessary for sustainable and thriving businesses and community

The design is appealing, well-considered and innovative. I imagine that some in the neighborhood might be concerned or fearful of the impact of a 3-story building. This building size is permitted under the current zoning. In my opinion, it will ultimately bring great benefit to the community, and is the perfect type of location to add more density (we do need to do our share to house more people who keep coming to the area, after all). It is located near retail and major thoroughfares- ideal for a little density. And I strongly believe the mixed use nature of the building is a blessing, and something we need to see more of in our commercial areas.

Sincerely,

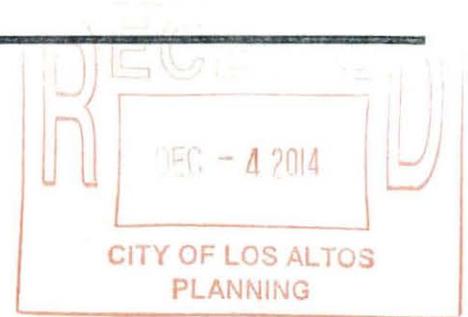
Kacey Fitzpatrick

Los Altos, CA 94024

Sean Gallegos

From: Pat Marriott
Sent: Thursday, December 04, 2014 2:34 PM
To: Planning (FAX)
Cc: Sean Gallegos; Zach Dahl; David Kornfield
Subject: 999 Fremont

Importance: High



Dear Commissioners:

I have read the staff report prepared for your meeting tonight. I live just a few blocks south of the proposed development.

My concerns.

1. The building is completely out of scale with the rest of the area. The architect's renderings deceptively show the building in a park-like area:



This does not remotely resemble Loyola Corners now nor in the foreseeable future.

I thought a basic tenet of architecture requires consideration of existing space. This building does not respect the surrounding area. It stands out as a massive structure on a small island.

2. The building does not adhere to the city-adopted Loyola Corners Specific Plan, which states: "The City staff, Planning Commission and City Council will use the Plan as a comparative 'yardstick.'"

The staff report rightly notes, "The proposal does *not* meet the goals, policies and objectives of the General Plan and Loyola Corners Neighborhood Commercial Center Specific Plan, design guidelines and ordinance design criteria adopted for the specific district or area."

3. Parking, while apparently meeting the letter of the law, seems inadequate. 10 spaces for 5 residential units is reasonable – IF we forget about guests (book clubs, play-dates, lunches, housecleaners, etc.). 5 spaces for 2 businesses is not reasonable. We don't know what kinds of businesses will occupy the building initially or in the future. Each business could easily have 2 employees at any given time. If employees use the on-site spaces, there's only one spot for a customer. Otherwise, employees park on surrounding streets and even then, only 5 customers will have a place to park.

I know from experience that parking is already a problem in the lots behind the existing businesses along Fremont, and there's minimal on-street parking in the area.

4. In a November 24, 2014 letter, Mr. Bunker says his building will "be a community/neighborhood benefit by both creating a first class introduction to Los Altos for anyone driving into the City from Silicon Valley and establishing new easily accessible retail and commercial space."

Most community members do NOT consider this project a benefit for the reasons stated above. I have circulated this email to several of my neighbors, who agreed to co-sign this letter with me. Their names and addresses are below.

5. We already have an example downtown of an out-of-scale building shoehorned into a small triangle of land: the hotel at Main Street and San Antonio Road. Please let's not make that same mistake in Loyola Corners.

Thank you for considering our comments.

Pat Marriott

Leonard Yool

Camille Casale

Jan and Maria Tavenier

Joan Takenaka

Kevin Hatch

Paul and Shirley Tavenier

Linda Newton

Richard and Phyllis Godfrey

Julie Caulfield

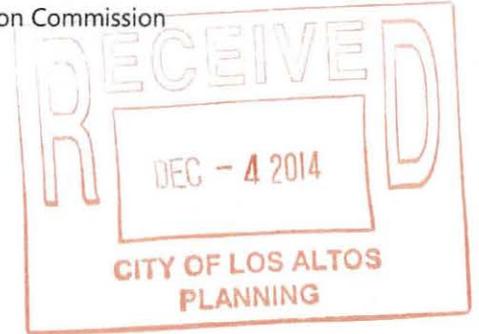
Kevin Wandryk

Lee Stivender

Jim Jolly

Sean Gallegos

From: James Walgren
Sent: Thursday, December 04, 2014 3:45 PM
To: Planning Transportation Commission
Cc: Yvonne Dupont; David Kornfield; Sean Gallegos
Subject: FW: Letter to the Los Altos Planning and Transportation Commission



Another letter for tonight. Yvonne, please see that the applicants get this too.

James

-----Original Message-----

From: J H
Sent: Thursday, December 04, 2014 3:16 PM
To: James Walgren
Subject: Letter to the Los Altos Planning and Transportation Commission

Dear Commissioners,

We are longtime residents of Loyola Corners. We strongly oppose the massive 3-story building proposed by Bunker/Jessup for the small triangular island at the end of Miramonte Avenue, because:

-- It would be completely out of place in Loyola Corners. It is too big and does not fit appropriately into the existing space. It would overwhelm the area and significantly detract from the unique rural feel of the area.

-- It would create additional traffic congestion and air pollution at the crowded Miramonte/Fremont intersection.

-- It would ruin the beautiful view of Rancho San Antonio Open Space Preserve, a unique and invaluable natural resource.

-- The planned parking spaces for the building are inadequate. Residents' guests and customers of the businesses will try to park nearby, exacerbating the parking problems in the small lot behind the businesses on Fremont Avenue.

-- The generic, modern architectural style of the building clashes with the existing low-key, modest structures in the area.

This project is inappropriate for Loyola Corners. We urge you to reject this proposal.

Sincerely,

Terry Fong
Jessica Hirschfelder
Jim Lempke
Jeff Wildfogel

Attachment 4
Planning and Transportation Commission Minutes,
June 4, 2015

APPROVED

MINUTES OF A REGULAR MEETING OF THE PLANNING AND
TRANSPORTATION COMMISSION OF THE CITY OF LOS ALTOS, HELD ON
THURSDAY, JUNE 4, 2015, BEGINNING AT 7:00 P.M. AT LOS ALTOS CITY HALL,
ONE NORTH SAN ANTONIO ROAD, LOS ALTOS,
CALIFORNIA

ESTABLISH QUORUM

PRESENT: Chair McTIGHE, Vice-Chair LORELL, Commissioners, BRESSACK, BAER,
MOISON, BODNER and JUNAID

STAFF: Community Development Director WALGREN, Planning Services Manager
KORNFIELD and Senior Planner DAHL

PUBLIC COMMENTS ON ITEMS NOT ON THE AGENDA

None.

ITEMS FOR CONSIDERATION/ACTION

CONSENT CALENDAR

1. **Planning and Transportation Commission Minutes**
Approve the minutes of the May 21, 2015 regular meeting.

MOTION by Commissioner BAER, seconded by Commissioner BODNER, to approve the minutes of the May 21, 2015 regular meeting as written.

THE MOTION PASSED BY A 5/0/2 VOTE, WITH BRESSACK AND MOISON ABSTAINED.

PUBLIC HEARING

2. **15-SD-01 – B. Gorrell and S. Wedding – 2050 and 2051 Madelaine Court**
Subdivision map modification to abandon a scenic easement encumbering parcels 5 and 6 adjacent to Permanente Creek in the Madelaine Court Subdivision, Tract No. 3107. *Project Planner: Dahl*

Senior Planner DAHL presented the staff report recommending to the City Council abandonment of a scenic easement on the Madelaine Court subdivision, Tract No. 3107, subject to the listed findings and conditions.

The project applicant/owner Brent Gorrell stated that he wanted to make minor changes to the rear yard including decks/patios. Resident Libby Lucas spoke with concern that the Commission needed more information in order to make a decision on the application, asked about the inappropriate development in the County up on Quail Road, and said that there should have been a contour map. There was no other public comment.

APPROVED

The Commission discussed the project and expressed their general support. Commission discussion included the nature of the easement, location of the top of the creek bank, review of the City's creek protection regulations, and the tree ordinance.

MOTION by Commissioner BAER, seconded by Commissioner JUNAID, to recommend approval to the City Council of an abandonment of a scenic easement on the Madelaine Court subdivision, Tract No. 3107, per the staff report findings and conditions.

THE MOTION PASSED BY A 6/1 VOTE, with CHAIR MCTIGHE opposed with concerns about negating the purpose of the easement by removing it.

Commissioners BODNER recused herself for agenda item No. 3 due to her owning property within 500 feet of the following project site and Commissioner JUNAID recused herself because her architectural firm worked on the following project.

3. 14-D-04, 14-UP-05 and 14-SD-01 – A. Jessup – 999 Fremont Avenue

Commercial Design Review, Use Permit, and Tentative Subdivision Map for a mixed-use project with four below-grade parking spaces, 1,792 square feet of commercial space and ten parking spaces on the ground floor, and four multi-family residential condominiums on the second story and third story. *Project Planner: Gallegos*

Planning Services Manager KORNFIELD presented the staff report recommending approval of Design Review, Use Permit and Subdivision applications 14-D-04, 14-UP-05 & 14-SD-01 to the City Council subject to the findings and conditions.

Project architect Chip Jessup spoke in support of the project contrasting the revised project with the prior plans, said the intention of the project was to revitalize, and there is a 40 percent increase in retail area.

Residents Donna Poulos, Gail Ostendorf, Jan Thomas, Greg Hoberg, Ron Meserve, John Fenwick, Darwin Poulos, Henry More, Richard Newton, Teresa Morris, Katherine Wurzburg, Tom Ferry, Kris Olson, Teresa Ullmann, Pat Marriot, Andrew Pejack, Benjamin Berman, Stephen More, Steve Wurzburg, Maria Gonzales, Barbara Loebner, Brett Beedle, Nancy Martin, Dr. Catherine Athans, and Chris Hoberg spoke in opposition to the project citing such concerns as inappropriate size and height, the need for story poles to determine the impacts, bicycle and traffic safety, lack of retail emphasis, lack of building setbacks, lack of on-street parking, potential soil contamination, noise impacts and water use impacts.

Project investor Ken Ravon, business owner of SNAP Fitness Allen Hall, residents Dick Kenarney, Judy Simes, Gary Tjader, and Michael Alcheck (owner of neighboring parcel at 1000 Fremont) spoke in support of the project and revitalization of the area. Resident Rita Chuang spoke in support of the building design, but not on this lot.

The Commission discussed such concerns as the viability of the proposed retail area, the need for redevelopment at Loyola Corners, the perceived insufficient parking ratio for all uses (1/300), the site constraints affected the parking potential and the ability to provide building setbacks, the overall character appearing bulky and massive in context, the need to refine the design on the sides of the building, the appropriateness of larger "family-size" units and the parking circulation.

APPROVED

MOTION by Commissioner BRESSACK, seconded by Commissioner BAER, to recommend denial of Design Review, Use Permit and Subdivision applications 14-D-04, 14-UP-05 & 14-SD-01 to the City Council per the following:

- The mixed-use is not desirable in accordance with the Specific Plan use permit requirements;
- The subdivision is not appropriate based on the use permit concerns; and
- The design does not meet the standard of high quality design and is not in keeping with the character of Loyola Corners.

Commissioner BAER added a friendly amendment that:

- The project is inconsistent with the General Plan and Loyola Corners Specific Plan;
- The size and massing appear too large in scale with the surroundings;
- The project lacks appropriate attention to provide pedestrian/human scale elements;
- The project needs to be unpretentious and unified in its design, greater attention to the immediate neighborhood character, needs greater setbacks from the street on all floors, and more sensitivity to the gateway site setting tone for the areas future commercial development.

Commissioner BRESSACK accepted the amendment and added that:

- The Parking and access to the parking on the property were additional concerns.

Commissioner BAER seconded the amendment.

THE MOTION PASSED BY A 4/1 VOTE, with Commissioner MOISON opposed because he wanted to send a message to the applicant that the project was close and wanted to encourage him to consider further revisions and that he had concerns about the scale of the project and the perceived lack of parking.

COMMISSIONERS' REPORTS AND COMMENTS

Reports were given on the Downtown Committee and the potential Downtown Plan update.

POTENTIAL FUTURE AGENDA ITEMS

Potential agenda items included timing on the Loyola Corners Specific Plan update, a moratorium till the plan is updated, and guidelines for Fremont Avenue. The Commission unanimously agreed to put the Loyola Corners Specific Plan update discussion on the next meeting agenda.

ADJOURNMENT

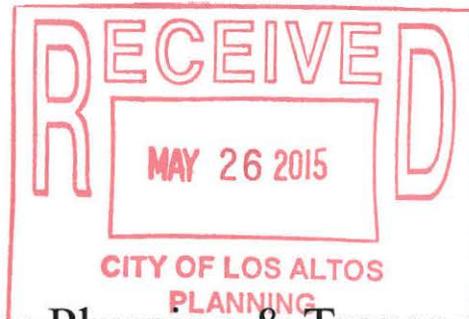
Chair McTIGHE adjourned the meeting at 10:09 P.M.



David Kornfield
Planning Services Manager

Attachment 5
Correspondence

To:



May 26,2015

Los Altos Planning & Transportation
Commission Members,

Well, nothing has changed on the Commercial Design Review of A.Jessup building on 999 Fremont Ave. Property. It`s almost identical to the design of July 2014. It`s still the same massive building design as before. Except for what looks like some trees in pots (I hope) on one side of the roof, some taller trees across the Fremont side, hiding the building. A couple of new windows on the second story in the corner of A Street and Fremont Ave. Inside there`s on less Condo, one less parking space, and four hundred & forty four square feet of additional commercial space. This building is still to BIG for a fit in the Loyola Corners area. It has no consistency with other buildings near by. Renters would be limited to one car a piece and very few people in this day & age have one car per family. Also other businesses in the area would suffer because of parking, as their`s would probably be used by the new occupants of this large building. This owner should NOT be given a permit to build a three story building on this site or any in the Loyola Corners area. Build your large or massive building in Down-Town Los Altos where there is more parking.....

Unless you live in the Loyola Corners area you

don't really know what the traffic is like. It's terrible
In the AM & PM between 7am-9am and again at about
2:45pm-6pm. The signal light only lets a few cars go
Across the Bridge at a time & many cars go through
RED lights. It's been a short while since someone has
driven into Tom's Depot on the corner of Fremont Ave.
next to where this proposed building site ,but it has
happened before & probably will again. I've lived in
this area since 1965 and there's always some kind of
accident in the area around Loyola Corners. Many of
them didn't require the Police, Fire, or Ambulance
tobe there. They settled it between the drivers
themselves because they were just fender benders. We
hear them all the time and neighbors meet on the
Bridge when this happens. Maybe the widening of the
Loyola Bridge will lessen these occurrences.

Any business whether it be hair salon, restaurant,
or whatever needs lots of parking. The existing
businesses of Loyola Corners might be severely hurt
by the proposed new commercial businesses due to
parking shortage. Please request this building site only
build a maximum of two stories only with as much
parking that can be provided.

Thank You,

*Virginia Johnson
a Lundy Lane
resident*



June 4, 2015



*trees on roof & extra windows
on second floor*



Tree on roof & more trees down Fremont Ave

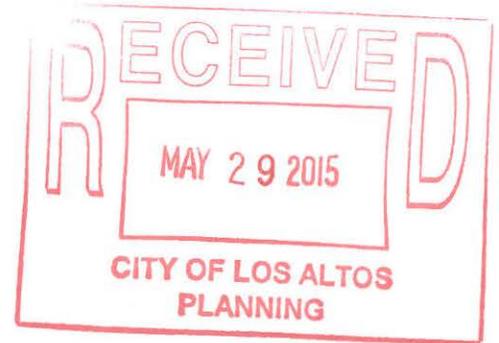


May 28, 2015

Richard C. Walleigh

Los Altos, CA 94024

Los Altos Planning and Transportation Commission
C/O Mr. James Walgren
Los Altos City Hall
1 North San Antonio Road
Los Altos, CA 94022



Dear Commissioners:

My wife and I will be traveling and unable to attend the public hearing on June 4th regarding the proposed development at 999 Fremont Avenue, but we want to clearly communicate our objections to the proposed project.

Since Mr. Bunker has made only minor revisions to the plans that have been twice rejected by the Commission, I don't want to waste your time. If you are already committed to rejecting the proposed development, you don't need to read further. Otherwise:

The proposed structure is totally unsuited to the location. It's mass and bulk jutting straight up from the edge of the sidewalk are totally inappropriate and totally inconsistent with the neighborhood. It violates the Loyola Corners Specific Plan and appears to be illegal relative to the Municipal Code. The area is designated primarily for retail and the proposed building has minimal space allocated for retail. If a reasonable portion of the building were designated for retail, the parking could not comply with code requirements. Many people think that the proposed parking is already inadequate, and the proposed spaces are only able to be provided through the trick of stacked parking. This contortion to apparently squeeze into the code requirements will cause delays for residents entering the building, resulting in traffic backups and increased traffic confusion in a location that is already the site of frequent accidents.

If you are not yet convinced to reject the proposed structure, we suggest that you require the erection of story poles on the lot to demonstrate to yourselves and anyone who passes by how unreasonable the construction of a building of this height and bulk would be.

Assuming the city will never be able to purchase the site, we are not opposed to reasonable development. However, regarding any future development, it should be pointed out that this site was once a gas station with underground tanks, and most underground tanks leak. So before any future excavation starts spewing carcinogens into the air, there should be a thorough investigation of the soil to determine the level of contaminants.

Thank you for your attention,

A handwritten signature in cursive script that reads "RC Walleigh".

From: James Walgren
Sent: Monday, June 01, 2015 4:06 PM
To: Planning Transportation Commission; David Kornfield
Cc: John Birk
Subject: RE: 999 Fremont Design Plan

Please see below, James



James Walgren, AICP
Community Development Director
650.947.2635

City of Los Altos
One North San Antonio Road
Los Altos, California 94022

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From: John Birk
Sent: Monday, June 01, 2015 3:36 PM
To: James Walgren
Subject: 999 Fremont Design Plan

Please do not approve the proposed three story building with no setback from the sidewalks.

This kind of development is wrecking Los Altos. Please leave Loyola Corners small.

We don't see any room for parking the extra cars. It's already a problem parking in Loyola corners.

With this direction for development, Foothill Expressway will become another El Camino.

Please stop this developmental greed, at the expense of the existing character of our community.

John and Allela Birk

Los Altos

From: James Walgren
Sent: Monday, June 01, 2015 8:28 AM
To: Planning Transportation Commission
Cc: David Kornfield; maxoccupancy@sbcglobal.net
Subject: FW: Loyola Corners new building

From: Denise and Marc
Sent: Sunday, May 31, 2015 5:13 PM
To: James Walgren
Subject: Loyola Corners new building

Hi James,

We got quite the email from neighbors concerned about the new building proposed at Loyola Corners.

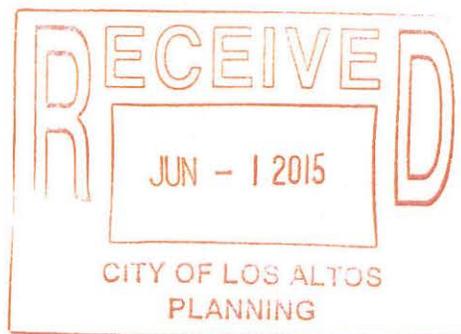
I'm not even sure we have a voice in the matter because we happen to be Mountain View residents who live nearby and go to Los Altos schools (Oak, Blach & MVHS), but I would like to make one common sense suggestion about this place.

The one thing I agree with those who object to the construction of something so large in this commercial center, is that there is *just* enough parking in the area for the current businesses. Something of this size would really make it inconvenient to shop or dine in Loyola Corners pending any development in the area's parking overall. It doesn't seem that in a project this size with both residential and commercial space, that 14 parking spaces will be sufficient not to impact surrounding available parking. Please make sure there is sufficient parking for this project within its bounds, so those of us using the neighboring businesses are not put off shopping there.

Thanks for listening!

-Denise Dagan

Mountain View, CA 94040



Los Altos Planning and Transportation Commission
One North San Antonio Road
Los Altos, CA 94024
May 30, 2015



Dear Planning and Transportation Commissioners:

We have serious reservations about the proposed project at 999 Fremont Ave. We are not Los Altos residents but live in the unincorporated area between Foothill Blvd. and I 280. We have lived here for 40 years and have used the bridge over Foothill and the Loyola Corners area thousands of times for access to shopping, work and road trips. It is a funnel not only for us but for pedestrians, bicyclists and schoolchildren.

One of us has witnessed a child on a bicycle hit by a car at Fremont and A street, and we have been rear-ended while turning left from A Street onto Miramonte. Sight lines are not good now and may be worsened by building large structures so close to the road edge. Cars on Fremont accelerate as they get the green light at Miramonte and try to get up to the 45 mph speed limit before they reach Foothill so they can merge. Pods of bicyclists are at this intersection, coming and going from the bicycle shop. Something needs to be done about traffic calming here, and something needs to be done about the dangerous left turn from A onto Miramonte, where incoming traffic from the left is totally obscured by cars in line to turn onto A street and the parking lot at Tom's, and the traffic coming through is accelerating to make the green left turn light at Fremont. Close calls are common here. A three story building out to the edge of a narrow sidewalk will solve none of this.

We live in a semi-rural suburb by choice, and an overpowering 3 story building on a tiny triangular property is in a clashing contrast to all other building in the area. If it is built, will not the other properties at Loyola Corners follow suit over time? And then it will look like Brooklyn. If we wanted to live in Brooklyn we would move there. This project is being proposed not to enhance the esthetics of the area or to provide residents with opportunities for shopping or service, but for maximum profit.

Please use your best judgment and deny the application for this poorly conceived project.

Sincerely,

Handwritten signatures in cursive script. The first signature is "Neil W. Hornor" and the second is "Marilyn G. Hornor".

Neil and Marilyn Hornor

Los Altos, CA 94024

From: James Walgren
Sent: Tuesday, June 02, 2015 9:02 AM
To: Planning Transportation Commission; David Kornfield
Cc: Anne Hecht
Subject: RE: Letter to Oppose Development at 999 Fremont
For Thursday night, James

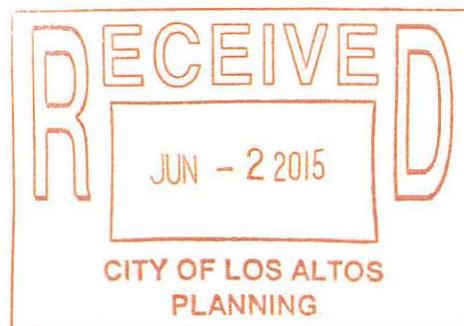
James Walgren, AICP
Community Development Director
650.947.2635

City of Los Altos
One North San Antonio Road
Los Altos, California 94022

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From: Anne Hecht
Sent: Monday, June 01, 2015 9:14 PM
To: James Walgren
Subject: Letter to Oppose Development at 999 Fremont

Los Altos Planning and Transportation Commission
One North San Antonio Road
Los Altos, CA 94024



Dear Planning and Transportation Commissioners.

I am writing to let you know that I am opposed to the proposed mixed-use project at 999 Fremont Ave. The project is not in compliance with the Loyola Corners Specific Plan, and is not appropriate for the location.

This is just another example of high-density, bulky housing. Our local schools are increasingly over-crowded as result of this type of poor city planning. As you know, the Los Altos School District (LASD) is not able to find an additional school site despite the fact that residents have passed a bond measure to fund a new site and clearly support addressing this issue. Before you approve more high-density housing which only exacerbates the problem, I strongly recommend that you address the over-crowding at our local schools and work with the Los Alto School District to identify a site for another school. Approving additional high-density housing projects without a known solution to our over-crowded schools is reckless and irresponsible.

The proposed project does not fit the character of the neighborhood, and is not human scale. Bulky three-story buildings should not be introduced to the quaint, small-scale district.

The ratio of retail to residential is too small. The property is within the retail district, yet only 6% of the structure is designated retail. According to the Loyola Corners Specific Plan, there should be an emphasis on retail with offices permitted on the second floor.

I am also opposed to the lift style parking more commonly seen in densely populated cities such as New York and Chicago. The concept is not in keeping with small town living when applied to a mixed-use project.

6/2/2015

The plan does not assure adequate parking. The plan includes 8 (stacked) residential spaces, 5 retail spaces, and 1 ADA space. This allows for 2 vehicles per condo, with no guests or housekeepers; and 5 total for all employees and customers. The current tenant, (one business), has 6 to 12 vehicles parked during hours of operation.

The configuration of the triangle already causes problems with traffic flow. There are accidents at the corner of A Street and Miramonte Ave., and cars have crashed through the front of Tom's Depot multiple times over the years. The addition of a loading/unloading zone as well as the garage entry/exit on Miramonte Ave. would greatly exacerbate the traffic problem at the three intersections.

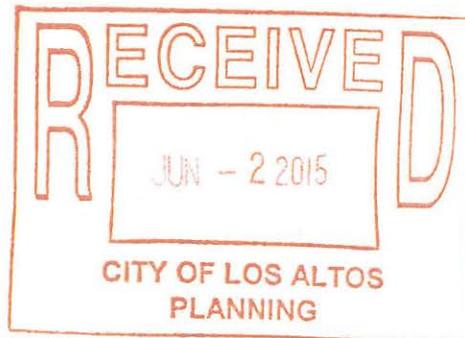
Further, I am very concerned about the safety of the children who bike to and from school and already have problems navigating that congested, poorly designed intersection. The new building creates even more traffic and blind spots at an intersection frequented by pedestrians, runners and bikers.

I respectfully request the Commission deny the application for the mixed-use project at 999 Fremont Avenue.

Sincerely,

Tom and Anne Hecht

From: James Walgren
Sent: Tuesday, June 02, 2015 9:04 AM
To: Planning Transportation Commission; David Kornfield
Cc: LuAnne Graves
Subject: RE: 999 Fremont development
For Thursday night, James



James Walgren, AICP
Community Development Director
650.947.2635

City of Los Altos
One North San Antonio Road
Los Altos, California 94022

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From: LuAnne Graves
Sent: Tuesday, June 02, 2015 8:16 AM
To: James Walgren
Subject: 999 Fremont development

I wish to voice my concern over the planned development at 999 Fremont, in Loyola Corners. The size of this project does not fit with the feel of this community. A one story building would blend in much better with the existing businesses. Please do not urbanize this community. The new Safeway in downtown Los Altos does not fit with the businesses there, and this project on Fremont has the same potential. Please consider the rural aspect of the area. This 3 story building will be an eyesore, as it will not blend in seamlessly, as is the problem with the new Safeway.

Thank you,
LuAnne Graves

Los Altos

From: James Walgren
Sent: Tuesday, June 02, 2015 9:00 AM
To: Planning Transportation Commission; David Kornfield
Cc: John Graves
Subject: RE: 999 Fremont Project
For Thursday night, James

James Walgren, AICP
Community Development Director
650.947.2635

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From: John Graves |
Sent: Monday, June 01, 2015 7:53 PM
To: James Walgren
Subject: 999 Fremont Project

I'm very much opposed to the proposed development at 999 Fremont. It's still much too big and out of character with the community. Please continue to push back on such aggressive developments in our neighborhoods.

thanks,
John Graves

Los Altos

From: James Walgren
Sent: Tuesday, June 02, 2015 8:59 AM
To: Planning Transportation Commission; David Kornfield
Cc: Connie Mariottini
Subject: RE: Fremont Ave/Loyola Corners #999
For Thursday night, James



James Walgren, AICP
Community Development Director
650.947.2635

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-----Original Message-----

From: Connie Mariottini
Sent: Monday, June 01, 2015 5:21 PM
To: James Walgren
Subject: Fremont Ave/Loyola Corners #999

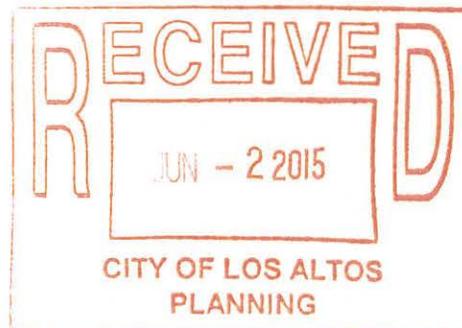
I just looked @ the drawings up for discussion this thursday 6/4 and I find nothing that makes sense for that site re. the residential units of 5 b.r., and 3 bedrooms for remaining units.

I would like more retail in that area and possibly offices above or a restaurant but certainly not units w/ so many bedrooms and therefore more parking requirements and demands on schools.

Thank you for reading & hopefully not approving this plan.
Connie Mariottini

Los Altos CA 94024

From: James Walgren
Sent: Tuesday, June 02, 2015 9:05 AM
To: Planning Transportation Commission; David Kornfield
Cc: Ron Meserve
Subject: RE: Please deliver to PTC Commissioners
For Thursday night, James



James Walgren, AICP
Community Development Director
650.947.2635

City of Los Altos
One North San Antonio Road
Los Altos, California 94022

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From: Ron Meserve
Sent: Tuesday, June 02, 2015 8:59 AM
To: James Walgren
Subject: Please deliver to PTC Commissioners
Importance: High

Dear Commissioner

I respectfully request the Commission to deny the application for the mixed-use project at 999 Fremont Avenue.

When my wife and I purchased our home in Los Altos...near the LA Country Club...our decision to buy here was influenced by our attraction to the Loyola Corner ambiance. The little shopping strip seems to us to make a statement ... "this is a mature, comfortable, family neighborhood". This sets our neighborhood apart from many others that don't seem to have an inviting character. It is a mark of a well-established and appealing neighborhood to see people walking to breakfast or to workout at the Snap fitness facility...or riding their bicycle to visit the bike shop. The new restaurant has been a welcome addition providing a great place to go for dinner and a glass of wine. And of course Tom's Depot which we love for a great breakfast or lunch.

My opposition to this development is NOT a rejection of progress. The proposed project does not fit the very appealing character of the neighborhood. This three story structure has a vastly different elevation than other buildings at Loyola Corners. A one or two story structure designed to be compatible with the other properties there would NOT be out of character. It is likely that this project, if approved, would lead to development of other structures that further drastically alter the character of the Corners.

The plan for parking and traffic flow, in my opinion, will become increasingly more problematic as the residential population increases in this area. Especially when schools are in session, the traffic congestion is staggering. Cars attempting to enter or exit from this new project will be impacted...and will adversely impact...the increasing volume of traffic. The configuration of the triangle already causes problems with traffic flow. There are accidents at the corner of A Street and Miramonte and cars have crashed through the front of

6/2/2015

Tom's Depot multiple times over the years. The addition of a loading/unloading zone as well as the garage entry/exit on Miramonte would greatly exacerbate the traffic problem at the three intersections.

I implore you to focus on improvement of traffic flow, walking and biking safety, while approving development that retains the current character of Loyola Corners. Reject the proposed project at 999 Fremont Avenue.

Ron Meserve and Mardell (Dell) Larcen

Los Altos, CA 94024

From: James Walgren
Sent: Tuesday, June 02, 2015 9:03 AM
To: Planning Transportation Commission; David Kornfield
Cc: Jim Alsup
Subject: RE: Loyola Corners / 999 Fremont
For Thursday night, James



James Walgren, AICP
Community Development Director
650.947.2635

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One North San Antonio Road
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From: Jim Alsup
Sent: Monday, June 01, 2015 11:58 PM
To: James Walgren
Subject: Loyola Corners / 999 Fremont

Hello James Walgren,

I'm writing to express my opposition to the current development plan for 999 Fremont in Loyola Corners. I am not against any redevelopment of this site and I'm happy to see effort underway. However, I find the current plan deplorable. It takes a current small area with a open feel to it and transforms it into a monolith that does fit in with the surroundings. There needs to be some area of larger setback on at least one corner providing green space. I also find the three stories creating a monolith like feel. This is not the nature of Loyola corners. Even two stories is still going to have this effect, but if the current third story is collapsed onto the second story such that their are second story setbacks this will help with the impression. How do you address the loss of parking this monolith would impart?

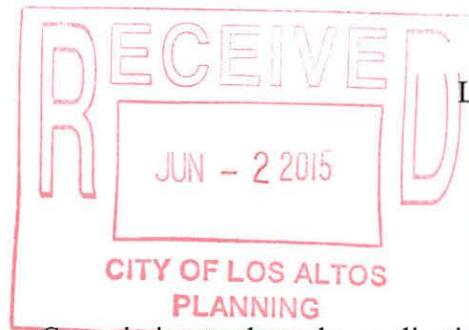
I feel a lower scale redesign is needed providing ample green space and parking. Everyone I've talked with about this proposal has similar feelings.

Would you please be so kind as to pass this along to the complete planning and transportation department staff for review?

Thank you,
-Jim

Los Altos Planning and Transportation Commission
1 North San Antonio Road
Los Altos, CA 94022

Los Altos, CA 94024



01 June 2015

Dear Planning and Transportation Commissioners,

I am writing to urge the Planning and Transportation Commission to deny the application for a massive and bulky development at 999 Fremont Avenue in Loyola Corners.

My wife and I have been residents near Loyola Corners for more than 30 years and we have taken an interest in the local government and the planning commission that serve our community.

In the period 1985-1990, there was a development proposed on the 1577 Carob Lane property that involved a large two-story structure with underground parking, referred to at the time as the "Pollace Building". There was such an uproar from the local residents at the bulk and mass of the building that the hearings at City Hall were packed to overflowing, with residents even standing in the orchard outside, almost all being outraged that such a project was even being considered. The City got the message that over-development at Loyola Corners was going to be resisted and a division in the city between the local residents and the City Council was rapidly being erected.

The Pollace Building project eventually failed and the existing building at 1577 Carob Lane was proposed and passed by the City with the support of local residents – an appropriate and sensible development. However, the City realized that there needed to be a Specific Plan overlay in the Loyola Corners area to handle the special nature of the small Loyola Corners area which closely bordered on predominantly single-story residential homes.

The City formed "The Loyola Corners Study Advisory Committee" under the chairmanship of the late Sherrill Walker, a local resident, who was both fair and competent, and they worked with Donald A. Wolfe & Associates to generate the Specific Plan for The Loyola Corners Neighborhood Commercial Center that was published on 15 November 1990 (available on the City of Los Altos website). This was to be a guide and roadmap for any development in the Loyola Corners District and was warmly received by the local community.

The composition of the Advisory Committee was chosen to represent as many interested parties as possible, with two persons representing each group. The local residents were represented by Sherrill Walker and Michele Coldiron, with pairs for the Los Altos City Council (Theodore Laliotis and Denny Spangler), Los Altos Planning Commission (Catherine Iu and Patricia Williams), the Los Altos Hills City Council (including Ed Barnes), the Loyola Corners business-owners (including the leader of the Loyola Corners Business Association, Terry Krivan and Greg Rivera of JP Liquors), property owners in the Loyola Corners district (Tom Andrews and Elie Alcheck), a judge retired from Washington DC (Charles Halleck), and other professionals with an interest in Loyola Corners. Some committee members represented more than one group, for example Tom Andrews was both a property owner and a business owner at Loyola Corners.

The Specific Plan was therefore the product of a great deal of discussion and balancing of interests to come up with a Specific Plan that would mostly satisfy all the interests with no interested party getting its every wish. But it was never forgotten by the community that the Advisory Committee was formed to prevent the Loyola Corners neighborhood from becoming over-developed with multi-story buildings with excessive bulk and mass.

The City took the salient recommendations of the Advisory Committee (in the form of the Specific Plan) and created and adopted Title 14 Chapter 42 of the Los Altos Municipal Code, defining "The LC/SPZ Loyola

Corners Specific Plan Zone District”. This has served the local residents well and given stability to Loyola Corners. The City Council has (unfortunately) not seen fit to follow through on many of the recommendations but then the City moves at its own pace. At least the County has been working to improve the traffic, bicyclist and pedestrian aspects of the Loyola bridge. The present application, if built, will prevent traffic improvement on the Loyola Corners end of the bridge.

At the hearing for the 999 Fremont Avenue proposal on 04 December 2014, a resident made the statement that he had lived most of his life in Los Altos and had an office in the clock-tower building for 35 years. He also stated that he had been a co-chair of the committee that had worked on the Specific Plan - although the Specific Plan shows only the Chair as being held by Sherrill Walker, all other members being listed alphabetically. He also made the claim that the 999 Fremont Avenue proposal was exactly what the Specific Plan Committee had in mind when the Specific Plan was developed for Loyola Corners and it would have been approved without question then. This statement was re-iterated by the Applicant for the 999 Fremont Avenue project in his letter to the Los Altos planners and the Planning and Transportation Commission dated 20 May 2015.

The resident and former member of the Advisory Committee was incorrect when he stated that the 999 Fremont Avenue proposal “was exactly what the Specific plan Committee had in mind when the Specific Plan was developed for Loyola Corners and it would have been approved then”. This statement could be easily refuted by other members of the original committee because such massive and bulky projects were intended to be prevented.

Subsequent research by the writer indicated that the resident above had an undeclared financial interest in the clock-tower building at 1000 Fremont Avenue. The minutes of the public hearing were altered before approval to show this financial interest but it was never stated during the hearing and it was not right of the Commission to approve the minutes since they did not reflect what actually occurred during the hearing. The argument can be made that where there is no harm there is no foul. However, if the bald statement that the 999 Fremont Avenue was exactly what the Specific Plan Committee had in mind for approval, and the statement does seem to have swayed the Los Altos planning staff, then there is indeed irreversible harm that has been done.

As a resident that has attended the public hearing on 04 December 2014 and 15 January 2015, I am surprised that the planning staff at the City of Los Altos is not taking a more pro-resident attitude towards these controversial developments. The recent appallingly unattractive downtown 1st Street developments could hardly have had any contribution from the residents of Los Altos so I have to assume that the developers were relatively unchallenged.

I was extremely disappointed that after voting to deny the current project at the 04 December 2014 meeting of the PTC, that the commissioners did not formulate the language of the denial there and then. Delaying the voting on the language allowed the Applicant to improperly bring up new material at the 15 January 2015 meeting of the PTC (material not shared previously with either the Commission or the public) that changed their minds, with the Commission voting for a continuation of the approval process. A large number of local residents were under the impression that the project had been denied – and therefore did not attend the meeting. It would be shameful if the public cannot trust our City to manage its affairs according to the law.

I strongly urge the commissioners to make a stand against this massive and bulky development that lies so close to our homes, and to set an example to other developers and our own planning staff, that such developments are not wanted in our neighborhoods. Please pass only sensible and appropriate proposals.

Yours sincerely,

Henry & Adelle More

From: James Walgren
Sent: Tuesday, June 02, 2015 9:05 AM
To: Planning Transportation Commission; David Kornfield
Cc: S Boadwee
Subject: RE: Project at 999 Fremont Ave.
For Thursday night, James



James Walgren, AICP
Community Development Director
650.947.2635

City of Los Altos
One North San Antonio Road
Los Altos, California 94022

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From: S Boadwee
Sent: Tuesday, June 02, 2015 8:45 AM
To: James Walgren
Subject: Project at 999 Fremont Ave.

To: James Walgren, Staff Liaison
Planning and Transportation Commission

Planning and Transportation Commission:
Michael McTighe, Chair
Ken Lorell, Vice Chair
Jonathan Baer
Phoebe Bressack
Jerry Moison

Dear Mr. Walgren and Planning and Transportation Commissioners,

I'm writing to express my opposition to the proposed mixed-use project at 999 Fremont Avenue. This massive three-story project is inappropriate for this site and does not comply with the Loyola Corners Specific Plan.

If the committee approves this project, such approval will signal either a haphazard, building-by-building approach to the renovation of this area, or an unwritten private plan for this area to become a mini-Santana Row, Los Altos First Street Corridor, or San Antonio Center.

With its view of the hills and charming bungalows, the Loyola Corners neighborhood has assets that should be appreciated and enhanced for future generations. Please do not sacrifice what people enjoy about this area: its human scale and neighborhood shops and atmosphere.

Regarding a few specifics of the proposed project:

The artist rendering does not show the new bridge over Foothill Expressway, which is already under construction. The rendering is therefore deceptive about what the immediate area will be like if the 999 Fremont project is built.

The garage with its motorized lift does not provide adequate or convenient (i.e. useable) parking for retail business. If the building houses retail businesses, customers will park elsewhere. The parking scheme suggests that the commercial space is intended for businesses with few visitors, offices that will not enhance Loyola Corners as a neighborhood retail area.

6/2/2015

Regarding pedestrian and bike safety:

The Planning and Transportation Commission should be looking at ways to make Loyola Corners more walkable and bike-accessible, rather than approving a project that will only add to traffic confusion and congestion. The project's driveway and loading zone on Miramonte will only decrease the safety for walkers and bikers in this area.

I am sure I am not the first neighbor of Loyola Corners to point out that the proposed project does not have the qualities of a pedestrian-scale retail area as called-for by the Loyola Corners Specific Plan, and as called for by neighbors themselves.

Thank you so much for your consideration.

Sincerely yours,

Sara Boadwee

Los Altos, CA 94024
(Heritage Oaks neighborhood)

From: James Walgren
Sent: Tuesday, June 02, 2015 1:38 PM
To: Planning Transportation Commission
Cc: Yvonne Dupont; Bill Sheppard
Subject: FW: 999 Fremont Ave

Additional correspondence. We intend to have these emails compiled in the lobby, but not copy them for the dais. Please let Yvone know if you would like copies at your desks.

James

James Walgren, AICP
Community Development Director
650.947.2635

City of Los Altos
One North San Antonio Road
Los Altos, California 94022



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From: Bill Sheppard
Sent: Tuesday, June 02, 2015 1:03 PM
To: James Walgren
Subject: 999 Fremont Ave

Dear Planning Commissioners:

I'm writing in support of the proposed building at 999 Fremont Ave. My family has lived on Miramonte Ave a few blocks north of the proposed development for eight years. We have 8- and 11-year old daughters and have tried to instill in them an appreciation for walkable neighborhoods, and as such often walk to Rancho to shop or dine. We would love to see Loyola Corners return to the vibrant mix of retailers and services it enjoyed in earlier decades, and believe that this project will spur continued investment. It is an attractive project which makes smart use of a challenging lot, while the overall mix of housing and retail is not projected to add appreciably to existing traffic. During my tenure as a commissioner BPAC found that an earlier revision of this project was largely consistent with good bicycle and pedestrian design practices, and concerns we raised during consideration of the project have been addressed through updates and staff commentary.

While concerns have been expressed that this building is out of scale and/or character with nearby structures, it is unfair to compare the only proposed new construction in decades with 50-year old buildings. The proposed structure does not exceed height regulations and is significantly shorter than the clocktower across the street. Further, it is my understanding that the California Water Service Company intends to build a new office building of similar height on their parking lot across Miramonte from the proposed structure, and that the Tom's Diner building immediately adjacent to 999 Fremont is also under consideration for replacement with a 30' structure. Should both of these projects occur the 999 Fremont project will be entirely consistent with this redevelopment. Even in the absence of these projects, I don't find that the proposed structure in any way overwhelms or dominates the existing streetscape. It will provide a far more attractive and welcoming gateway to Loyola Corners than the current unimproved and undistinguished salon.

6/2/2015

I urge PTC to approve this project and in so doing provide incentive for additional investment in the revitalization of Loyola Corners.

Best regards,

Bill Sheppard

Los Altos, CA 94024

David Kornfield

From: David Roberson
Sent: Tuesday, June 02, 2015 3:52 PM
To: David Kornfield
Cc: Gregg Bunker
Subject: 999 Fremont Project at Loyola Corners

Dear Planning Commissioners:

My name is David Roberson. I am a local real estate attorney, a local business owner, and a frequent user of and visitor to Loyola Corners.

I have reviewed all of the materials which have been submitted to the City for this development project and find it incredibly exciting that the City has an opportunity to finally begin to improve this area along with the County of Santa Clara bridge improvement.

I understand the development team has made significant strides to address any and all issues related to conformance with the City's ordinances.

I also fully understand that an owner of real property has an unfettered legal right to develop their property as long as that development is in conformance with the zoning, building, and planning guidelines set forth within that jurisdiction and that particular parcel. Any opposition to this fact should be met with deaf ears as denial of a conforming project based on unsubstantiated neighbor opposition has little or zero weight in the eyes of the law.

I am in full support of the Planning Commission moving forward with approving this project and I look forward to the day when we can all be proud of the development progress the City is making.

Sincerely,

David Roberson, Esq.



David Kornfield

From: Chris Clancy
Sent: Tuesday, June 02, 2015 3:36 PM
To: James Walgren; David Kornfield; Sean Gallegos
Subject: 999 Fremont Avenue

June 02, 2015

Dear Planning Commissioners:

I am writing this letter in support of the proposed new building at 999 Fremont Avenue that is coming up for review. I am familiar with the project because I am working with the developer on a different project in San Jose as the architect and I have seen the 999 Fremont design evolve and improve in response to planning department comments. I am also the architect for the new TURN Restaurant that is almost completed at 295 Main Street.

I believe this project will be a great asset to Loyola Corners, will create a visual gateway entry to the area, is modest in materials, and is neighbor friendly. I think the design team has done a great job in responding to planning direction and comments and adjusting the design to be appropriately scaled to the neighborhood.

I have seen the photo-realistic renderings and I think they speak for themselves as to the quality and appropriateness of this project. I read the planning staff report and agree that the 3rd floor is significantly reduced in its visual impact with the re-design.

The retail at street level will provide great amenities for the neighborhood. This project will provide additional housing in Los Altos as well.

My understanding is that the proposed structure fully complies with the zoning regulations.

I can understand that some residents don't want change, and that the area is master planned to be a park, but my understanding is also that the master plan is out of date, and no money has been allocated by the City to purchase the land to create this park.

I would hope that the Planning Commission would approve this exciting project and that the project moves forward.

Sincerely,

Christopher Clancy AIA
Christopher Clancy | Architecture
2 n 1st Street
San Jose, CA 95118



From: James Walgren
Sent: Tuesday, June 02, 2015 9:04 AM
To: Planning Transportation Commission; David Kornfield
Cc: Anna De Giuli
Subject: RE: Project at 999 Fremont Ave, - Los Altos
For Thursday night, James



James Walgren, AICP
Community Development Director
650.947.2635

City of Los Altos
One North San Antonio Road
Los Altos, California 94022

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From: Anna De Giuli
Sent: Tuesday, June 02, 2015 12:28 AM
To: James Walgren
Subject: Project at 999 Fremont Ave, - Los Altos

Mr. Walgren,

I'm writing to support the proposal of a new building at 999 Fremont avenue. It would be a plus to the neighborhood to have a few more residential opportunities (unfortunately already reduced to 4) and more modern commercial spaces. Loyola Corners could become way more attractive for neighbors (as I am) if there were a few new stores and activities. That would benefit the existing commercial operations as well as the pleasantness of the neighborhood..

Nuchi

From: James Walgren
Sent: Tuesday, June 02, 2015 3:21 PM
To: Planning Transportation Commission
Cc: steve@stephenpappas.com
Subject: FW: Loyola Corners project

Please see below, James

James Walgren, AICP
Community Development Director
650.947.2635

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One North San Antonio Road
Los Altos, California 94022

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From: Steve Pappas
Sent: Tuesday, June 02, 2015 3:14 PM
To: Sean Gallegos; James Walgren
Subject: Loyola Corners project

Dear City Planners:

I am writing in support of the project as currently proposed for 999 Fremont Avenue in Los Altos. I am a resident nearby in Mountain View and I pass by the Loyola Corners area frequently. I used to go to the Wolf Camera shop when it was located there, and the grocery that used to be in the building across the street. Every now and then I go to the liquor store. I truly wish there were more services offered in the neighborhood, which is desperately in need of revitalization, and this project promises that with the dedicated retail space on the first floor. It also provides some much-needed housing, and, as scaled back now, the building fits within the neighborhood (although, as noted, other commercial buildings in the neighborhood could also use a bit of modernization as well). The current renderings are very attractive. I do hope the council votes to approve the plan.

Thank you for your consideration of my opinion.

6/2/2015

Best regards,

Steve Pappas

Stephen R. Pappas, Esq.

550 S. California Avenue

Suite 320

Palo Alto, CA 94306

(650)858-8400

(650)858-8411 fax

(650)804-9299 cell

Alternate email:

From: James Walgren
Sent: Tuesday, June 02, 2015 3:20 PM
To: Planning Transportation Commission
Cc:
Subject: FW: 999 Fremont Ave
Please see below, James



James Walgren, AICP
Community Development Director
650.947.2635

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One North San Antonio Road
Los Altos, California 94022

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From: Brian Farley
Sent: Tuesday, June 02, 2015 2:28 PM
To: Sean Gallegos; James Walgren
Subject: 999 Fremont Ave

To: The Los Altos Planning Commission

From: Brian and Debbie Farley, 1534 Kathy Lane

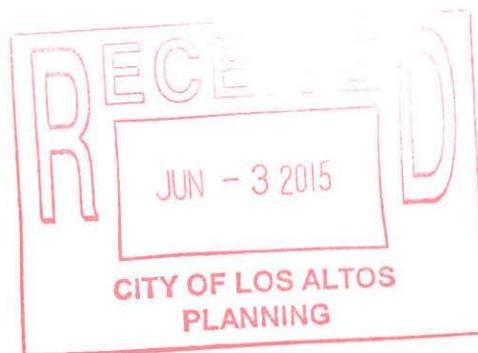
We would like to voice our support for the proposed new commercial and residential building at 999 Fremont Ave. Our home is located in a cul-de-sac on Fremont avenue and we have lived at that location for the past 19 years and have been residents of Los Altos for 26 years. We've always thought that Loyola Corners needed redevelopment and are happy to see the proposed building.

Understanding that there were some voiced concerns about the size of the building and potential traffic impact, I read with interest the Hexagon Consultants report saying that the new building would only generate 6 incremental trips during peak hours from cars coming and going from the proposed building. Having driven past this location countless times over the past decades, six additional cars is the equivalent of about one stop light cycle at the corner. Therefore there appears to be no significant issue with traffic changing due to this building.

Regarding the look of the building, it is far better than the building that is presently on the site. We also need more residential units in Los Altos and I welcome the combined use. At 30 feet high, this seems reasonable. Statements that the building design is large and not consistent with the architecture of the other Loyola Corners buildings states what is clearly true. However, this is a good thing since most of the Loyola Corner buildings are old and not aesthetically pleasing. Perhaps the approval of this development will spur other property owners to develop their sites. If this was done, the neighborhood could gain a nicer commercial destination for residents to walk to, or ride bikes to. Conversely, a denial of the proposed project at 999 Fremont could hinder other property owners from pursuing welcomed improvements at Loyola Corners. It would sure be nice to see development progress in this neglected corner of Los Altos.

6/2/2015

From: James Walgren
Sent: Wednesday, June 03, 2015 8:57 AM
To: Planning Transportation Commission
Cc:
Subject: FW: 999 Fremont



Please see below, James

James Walgren, AICP
Community Development Director
650.947.2635

City of Los Altos
One North San Antonio Road
Los Altos, California 94022

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From: phishomi@aol.com
Sent: Wednesday, June 03, 2015 12:02 AM
To: James Walgren
Subject: 999 Fremont

Hello Mr. Walgren,

I am a Loyola Corners resident and am vehemently opposed to the proposed mixed use development being proposed for 999 Fremont.

Every time I cross over the bridge, several times per day, I imagine the proposed three story structure looming on the small parcel in front of me. A lovely designed building but too massive for that small property. I cringed when I saw the trees being removed; not a wonderful trade off, nature versus development. And to think there is mention of removing another tree on the Miramonte side so as to create more visibility makes me cringe again.

I have purchased a copy of the Loyola Corners Specific Plan and several points in the plan favor the maintaining of the character of the area and the feelings of the residents as being paramount. Here are a few pertinent quotes from the plan which I encourage you to take to heart. Some of the goals of the plan are:

On Page 6:

1) Create attractive and functional shopping and commercial use facilities in order to increase use and provide for long term viability- Having Mr. Bunker's property management as one office does not serve the residents of Loyola corners (compare this to the nail salon that serves many of the women and some of the men residents of the vicinity).

5) Provide for a mix of appropriate commercial uses which provide a range of desirable neighborhood-serving commercial uses-Only two businesses are slated for this development, again one of them being self serving for Mr. Bunker. I don't feel this is appropriate or desirable for the residents of Loyola Corners.

Furthermore, on Page 38-9, "Since LC is essentially a neighborhood business center, it is anticipated that the focus of the marketing efforts and product/service mix of any new establishments will be predominantly oriented to serve nearby residents." and "LC is, in terms of logical land use planning, a neighborhood retail center whose function is to provide retail services to the surrounding community. Correspondingly, this area is not well suited to domination by administrative office uses " (Bunker's office).....

3) Recognize the need to preserve and protect adjacent residential neighborhoods for traffic, noise and visual impacts- the visual impact will be a negative impact in terms of the bulk of the building. The traffic which is a challenge now, especially during work and school commute times will be further exacerbated especially on Miramonte Avenue.

6/3/2015

On Page 7:

2) Retention of small scale development consistent with neighborhood commercial environment-I don't consider this small scale; and

On Page 36 Expansion: Establish a maximum total size and scale that will not substantially change the character of the center and the neighborhood. LC needs to expand to remain competitive but it also needs to retain its neighborhood character-Neighborhood character is key here.

I encourage you to go to Loyola Bridge and imagine a 30' plus building on that small island.

If you feel the building is in accordance with the other buildings, I urge you to look at the amount of land the other buildings are on versus 999 Fremont.

Thank you for your consideration!

Sheri Shemanski

Los Altos, CA 94024

David Kornfield

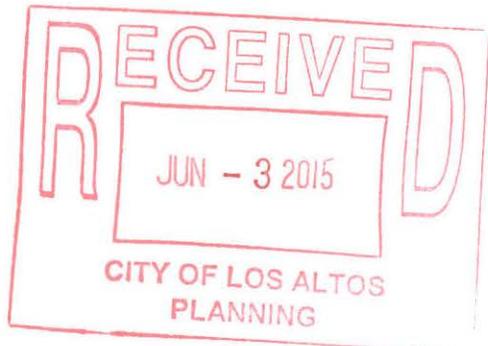
From: James Walgren
Sent: Wednesday, June 03, 2015 4:13 PM
To: David Kornfield; Jerry Moison; Jim Chiang; Jon Baer; Malika Z. Junaid; Michael McTighe; Phoebe Bressack; Ronit A. Bodner
Cc:
Subject: FW: Please deliver to PTC Commissioners

Please see below, James

James Walgren, AICP
Community Development Director
650.947.2635

City of Los Altos
One North San Antonio Road
Los Altos, California 94022

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From: _____
Sent: Wednesday, June 03, 2015 4:10 PM
To: James Walgren
Subject: Fwd: Please deliver to PTC Commissioners

Dear Commissioner

I respectfully request the Commission to deny the application for the mixed-use project at 999 Fremont Avenue.

When my wife and I purchased our home in Los Altos...near the LA Country Club...our decision to buy here was influenced by our attraction to the Loyola Corner ambiance. The little shopping strip seems to us to make a statement ... "this is a mature, comfortable, family neighborhood". This sets our neighborhood apart from many others that don't seem to have an inviting character. It is a mark of a well-established and appealing neighborhood to see people walking to breakfast or to workout at the Snap fitness facility...or riding their bicycle to visit the bike shop. The new restaurant has been a welcome addition providing a great place to go for dinner and a glass of wine. And of course Tom's Depot which we love for a great breakfast or lunch.

My opposition to this development is NOT a rejection of progress. The proposed project does not fit the very appealing character of the neighborhood. This three story structure has a vastly different elevation than other buildings at Loyola Corners. A one or two story structure designed to be compatible with the other properties there would NOT be out of character. It is likely that this project, if approved, would lead to development of other structures that further drastically alter the character of the Corners.

The plan for parking and traffic flow, in my opinion, will become increasingly more problematic as the residential population increases in this area. Especially when schools are in session, the traffic congestion is staggering. Cars attempting to enter or exit from this new project will be impacted...and will adversely impact...the increasing volume of traffic. The configuration of the triangle already causes problems with traffic flow. There are accidents at the corner of A Street and Miramonte and cars have crashed through the front of Tom's Depot multiple times over the years. The

addition of a loading/unloading zone as well as the garage entry/exit on Miramonte would greatly exacerbate the traffic problem at the three intersections.

I implore you to focus on improvement of traffic flow, walking and biking safety, while approving development that retains the current character of Loyola Corners. Reject the proposed project at 999 Fremont Avenue.

Kenneth and Margaret Kwan

Los Altos, CA 94024

From: David Kornfield
Sent: Wednesday, June 03, 2015 3:58 PM
To: Planning Transportation Commission
Subject: FW: 999 Fremont Avenue Project
Commissioners:

More correspondence.

David

From: Cameron Bunker
Sent: Wednesday, June 03, 2015 10:23 AM
To: David Kornfield
Subject: 999 Fremont Avenue Project

TO: Los Altos City Planning Commission

FROM: Cameron Bunker

Campbell, CA 95008

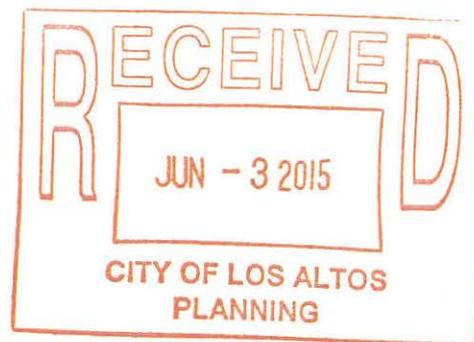
I would like to instill my support for the proposed new commercial and residential project at 999 Fremont Ave. My home is located in Campbell, but I have been traveling to Loyola Corners my entire life while my father, Mr. Gregg Bunker, ran his company Photo-Drive-Up at this location for 20+ years. I have always thought that Loyola Corners needed redevelopment and I am happy to see that is happening.

I understand that there were some concerns about the size of the building and potential traffic impacts, I read over the reports saying that the new building would only generate 6 incremental trips during peak hours from cars coming and going from the proposed building. Having driven past this location thousand of times over the past decades, six additional cars is the equivalent of about one stop light cycle at the corner. Therefore there appears to be no significant issue with traffic due to this building.

Regarding the look of the building, after going threw many design reviews I believe that this building is perfect for the community and has been scaled down appropriately to meet the opinions of this community. This addition will benefit the community whether people in it may think that or not. It is far better than the building that is presently on the site. Los Altos needs more residential units and I know combined use is new, but that does not mean it is going to negatively effect the community in anyway. The new addition to this piece of property will bring in two small businesses in the retail spaces. These will be small, most likely family owned, businesses. 4 new families will be joining this sub community. Flyers were posted around the property saying, " let's keep Loyola Corners charming and stop this development!" Loyola Corners is not charming, I am charming, and after this development and many others, then and only then will Loyola Corners be, charming.

Statements were made that the building design is large and not consistent with the architecture of the other Loyola Corners buildings. This statement is entirely not true. New plans were developed to match older buildings in hope that when newer buildings are developed around the area, Loyola Corners will not loose that small town village vibe that everyone enjoys. After this development that vibe will not change. This is because the people in the town make it what it is, not its buildings.

--
Cameron Bunker
Marketing Manager
Silicon Valley Business Center



From: James Walgren
Sent: Wednesday, June 03, 2015 8:55 AM
To: Planning Transportation Commission
Cc: Larry Bjork
Subject: RE: Loyola Corners Building
Please see below, James



James Walgren, AICP
Community Development Director
650.947.2635

City of Los Altos
One North San Antonio Road
Los Altos, California 94022

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From: Larry Bjork
Sent: Tuesday, June 02, 2015 7:13 PM
To: James Walgren
Subject: Loyola Corners Building

Jim,

This building is over sized and out of place with the ambience of Loyola Corners.

It should be turned down for the simple reason of not enough parking on or around the premises of the building. There is already enough traffic around that area, we don't need more.

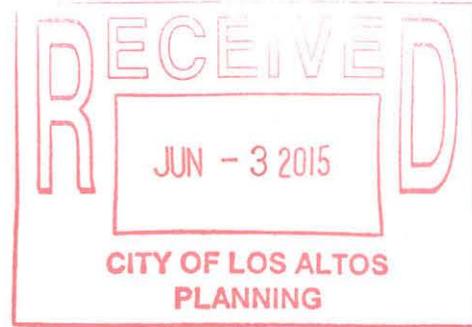
I hope you and the city staff will vote to turn down this proposal.

My vote is no.

Larry Bjork

Los Altos, CA 94024

June 1, 2015



To: Members of the Planning and Transportation Commission
City of Los Altos, California

Re: 999 Fremont Avenue – Proposed Mixed-Use Project

I support the Mixed-Use Project proposed for 999 Fremont Avenue as revised and under consideration at your meeting on June 4, 2015.

The revised elevation makes for a very attractive building and it helps to revitalize Loyola Corners without overpowering the small commercial district in the midst of the surrounding residential area.

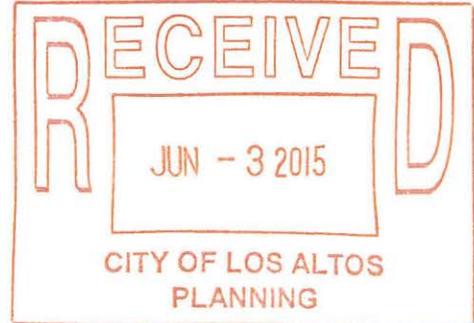
Sincerely,



Christine Dailey

Los Altos, CA 94022

From: Sean Gallegos
Sent: Wednesday, June 03, 2015 11:34 AM
To: Yvonne Dupont
Subject: FW: Fremont 999



Yvonne,

Please forward to David. Thanks.

From: Anna De Giuli
Sent: Tuesday, June 02, 2015 1:52 PM
To: Sean Gallegos
Subject: Fremont 999

Mr. Gallegos,

I'm writing to support the proposal of a new building at 999 Fremont avenue. It would be a plus to the neighborhood to have a few more residential opportunities (unfortunately already reduced to 4) and more efficient use of space.. Loyola Corners could become way more attractive for neighbors (as I am) if there were a few new stores and activities. That would benefit the existing commercial operations as well as the pleasantness of the neighborhood.

Revitalizing the area does not means transform it in a Santana Row - as some critics of the project think - the mixed destination of the building will enhance the life of the street.

Thanks for taking the time and read this.

Anna De Giuli
Granger Avenue



Dear Planning Commissioners:

I support the proposed building at 999 Fremont Ave. We have an office in Loyola corners. I would like to see Loyola Corners to be mix of retailers and services with sidewalks and landscape . We like to see people walking and window shopping, a more family oriented type of center . This project is an attractive project which makes the entrance to Loyola corners more beautiful and can bring the walking shopping center feeling to the Loyola corners.

I suggest strongly to the Commission to approve this project and also encourage other projects for the Loyola too.

Best regards,

Saeid Razavi

Los Altos, CA 94024

May 30, 2015

City of Los Altos Building Department
1 N. San Antonio Road
Los Altos, Ca. 94022
Attn: David Cornfield



Dear David,

I understand that Gregg Bunker's project is scheduled for Planning Commission review in the coming days and wanted to voice my support and commend the changes he's made to the project. In my last letter to the commission I expressed particular concerns regarding massing, aesthetics and screening with the use of trees along A Street.

I have had the opportunity to review Mr. Bunker's latest submission and was very pleased to see the architectural terracing that is now in place. Their architect created a very attractive structure which visually decreased the massing making the building more proportional to the lot. In addition, they have done a great job at changing the overall architectural aesthetic making the building much more "village appealing" and less industrial. I also appreciated the addition of trees on A Street.

Very Truly Yours,

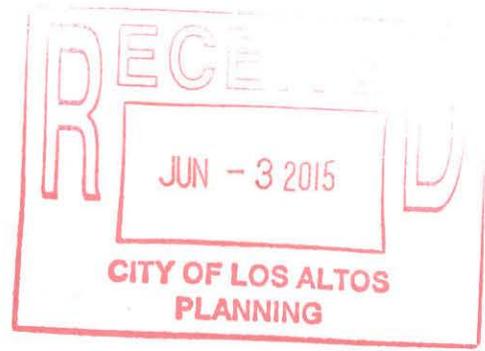
Tracy Ross-Tamasi

From: David Kornfield
Sent: Wednesday, June 03, 2015 9:58 AM
To: Planning Transportation Commission
Subject: FW: 999 Fremont Ave. Loyola Corner's Estates LLC

Commissioners:

An additional letter regarding the subject project.

David



From: Mojgan |
Sent: Tuesday, June 02, 2015 10:03 PM
To: David Kornfield
Subject: 999 Fremont Ave. Loyola Corner's Estates LLC

Dear Mr. Kornfield,

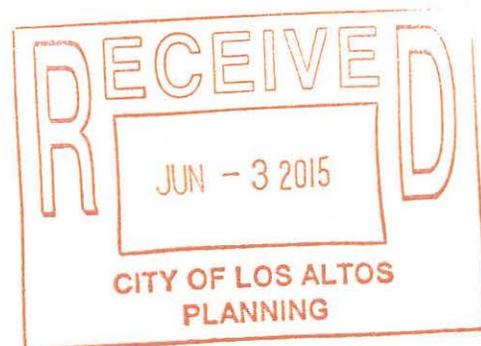
This is in regards to The Loyola Corner Development. We are a resident of Los Altos and strongly support this development. Your office should encourage development like this so the Los Altos residence can spend more time in Down Town Los Altos rather than other cities around us.

This type of development in Los Altos is long overdue. This type of development would increase property values and ultimately tax revenue for the City. We just don't understand why anybody would be against such project. We urge you to approve this project and encourage others to come in to Los Altos and do such development.

Sincerely,

Mojgan Nodoushani
Los Altos Hills Resident

From: David Kornfield
Sent: Wednesday, June 03, 2015 9:57 AM
To: Planning Transportation Commission
Subject: FW: 999 Fremont Avenue - Loyola Corner Project
Commissioners:



A letter below regarding the subject project.

David

From: Ken Ravon
Sent: Tuesday, June 02, 2015 9:25 PM
To: David Kornfield; Gregg Bunker
Subject: 999 Fremont Avenue - Loyola Corner Project

Dear Mr. Kornfield,

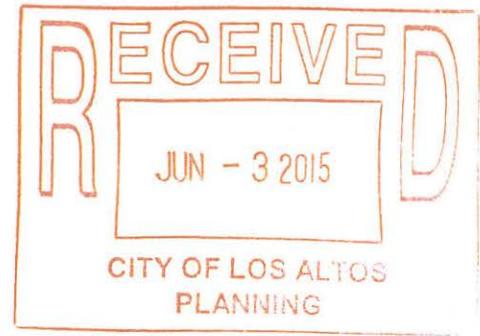
I met you at our last meeting for Loyola Corner Development when you recommended number of improvement to the design in order to be in compliance with your ordinance. I understand that all of those recommendations were incorporated into the updated design and it has been approved by your department. Now it will be presented to the Planning Commission on June 4th.

I am a small investor in that development and of course have a vested interest to see it build as soon as possible. To educate myself I went to your website and saw the community supports and objections for this project. The one that stuck in my mind and I think is a great evidence of changing time is Mr. Mike Alcheck who is also serving as a planning commission in Palo Alto suggested that City of Los Altos to have a vision for the city and welcome change. He said, the fact that a submitted plan for the Loyola corner doesn't look anything like other buildings around it is enough reason to approve this development. He added that his demographic is looking for development like this.

I invested my life saving in this development as I am a 60 years old man who didn't strike it rich in this valley, and hoping you approve this project as soon as possible.

*Sincerely yours
Ken Ravon*

From: James Walgren
Sent: Wednesday, June 03, 2015 1:51 PM
To: Planning Transportation Commission
Cc: bahi@360designstudio.net
Subject: FW: Letter in support of application for 999 Fremont Ave
Please see below, James



James Walgren, AICP
Community Development Director
650.947.2635

City of Los Altos
One North San Antonio Road
Los Altos, California 94022

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From: Bahi Oreizy
Sent: Wednesday, June 03, 2015 1:20 PM
To: David Kornfield; James Walgren; Sean Gallegos
Subject: Letter in support of application for 999 Fremont Ave

Hi David or Sean,

I'm writing this email to voice my support for the project that is being proposed for 999 Fremont Avenue.

I'm an 9+ year resident of Los Altos and also a practicing architect.

I feel like the proposed project fits the scale of this neighborhood very well and will be a great addition to Loyola Corners. Both multifamily housing and retail are of short supply in the South Los Altos area where I live and practice. A project like this is sensitive to our needs and will definitely be welcomed by residents such as myself.

Unfortunately, I'm unable to attend the meeting tomorrow night but wanted to share my thoughts.

Regards,

Bahi Oreizy, Architect.Principal

Los Altos 94024

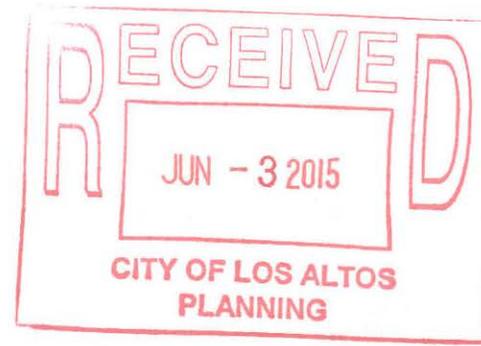
Yvonne Dupont

From: David Kornfield
Sent: Wednesday, June 03, 2015 9:49 AM
To: Planning Transportation Commission
Subject: FW: Loyola Corners
Commissioners:

Correspondence below regarding the 999 Fremont project.

David

From: Richard Stevenson |
Sent: Tuesday, June 02, 2015 5:34 PM
To: David Kornfield
Subject: Loyola Corners



Dear Planning Commissioners:

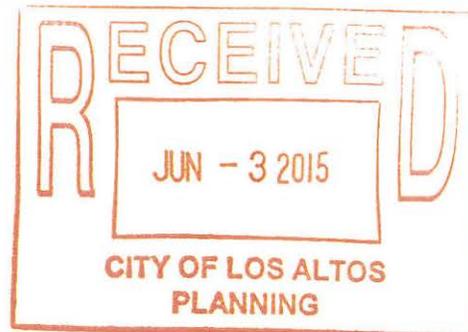
I'm writing in support of the proposed building at 999 Fremont Ave. We have lived in Los Altos for approximately 14 years. We would love to see Loyola Corners return to the vibrant mix of retailers and services it enjoyed in earlier decades, and believe that this project will spur continued investment.

Sincerely,

Carla Stevenson
Rich Stevenson

Los Altos

From: James Walgren
Sent: Wednesday, June 03, 2015 8:42 AM
To: Planning Transportation Commission
Subject: FW: Support for 999 Fremont Ave project



From: Angelo De Giuli
Sent: Tuesday, June 02, 2015 6:15 PM
To: Sean Gallegos; James Walgren
Subject: Support for 999 Fremont Ave project

Dear James, Sean, Planning staff and PTC members:

I came to Los Altos from Italy in 1999 and from 2004 I have been living with my family in my property on Loyola Corner. I write to you in support of the Loyola Corners/ 999 Fremont Ave project. I've been informed about the project only recently and since I cannot participate to the Planning & Transportation Commission meeting at the Los Altos City Hall on June 4, I'm sending this letter with some points I would like you to consider:

- I witnessed with pleasure the revitalization on the small commercial center on Foothill and Homestead corner and I always wondered why there were no projects to enhance the Loyola Corner commercial area that has good potential but it is currently neglected and looks pretty rundown.
- South Los Altos neighborhood will be enhanced with an intelligent requalification and development of the Loyola Corner commercial area.
- I think that the proposed project is a high quality, well design, visually appealing building that hopefully will attract similar additional investments in the area.
- The building that the project replaces is a good example of poor/bad architecture. I'm happy to see it go!
- The addition parking is a welcome bonus
- I do not see any negative impact on traffic or walkability. On the contrary I believe that it simplifies/clarifies circulation for both cars and pedestrians.
- The 4 additional residential units are a small, but still nice contribution to solve the current housing shortage in Los Altos without creating any additional burden to the existing infrastructure.
- I do not like multi story buildings in the pure residential neighborhood but this part of Loyola corner is a mix use, pretty busy area and the vicinity with other commercial structures make this 3 story addition a perfectly good fit.

For the above points I strongly believe that the project will bring good benefit to our south Los Altos Community. I hope that it will be approved and will be followed by other comparable projects till Loyola Corner will be transformed in a little, middle density, vibrant, mix use district.

Sincerely

Angelo De Giuli

From: David Kornfield
Sent: Thursday, June 04, 2015 4:11 PM
To: Planning Transportation Commission
Subject: FW: 999 Fremont Avenue - Loyola Corner
Correspondence for tonight

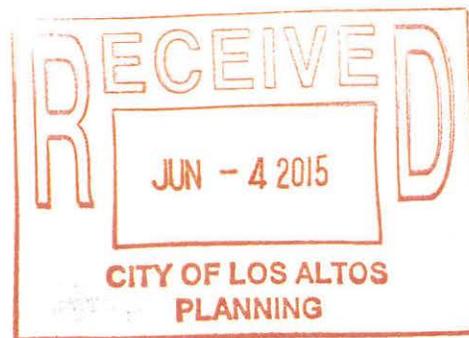
From: Tara Nowroozi
Sent: Thursday, June 04, 2015 12:32 PM
To: David Kornfield
Cc: Ken Ravon
Subject: 999 Fremont Avenue - Loyola Corner

Dear David Kornfield,

I am a resident of Los Altos and I drive pass the Loyola corner on daily basis. I always wondered why no one is doing anything on this prime location. Recently, I heard that someone submitted a plan for developing that corner for residential and commercial. I searched and found the propose drawings and would like to voice my support for this and similar development for down town as the City really needs to start developing this area of Los Altos. I understand that this type of development would even increase property values, which I am a supporter of as a resident of Los Altos.

Best regards,
-- Tara

Tara Nowroozi



From: David Kornfield
Sent: Thursday, June 04, 2015 9:58 AM
To: Planning Transportation Commission
Subject: FW: 999 Fremont Ave. Loyola Corner's Estates LLC

Correspondence regarding the subject property.

From: Ehsan Talebi
Sent: Thursday, June 04, 2015 7:58 AM
To: David Kornfield
Subject: 999 Fremont Ave. Loyola Corner's Estates LLC

Dear Mr. Kornfield,

My name is Ehsan Talebi, a residence of Los Altos for over 30 years since august of 1984. My wife and I, and our three children, are proud of living in this town, but when we have guests and/or when we want to go out to dinner, we go to Palo Alto, Santana Row in San Jose or even Down Town Mountain View. I am sure you have visited Santana Row where they have used the same concept that the Loyola corner's developer is proposing. Yes, Down Town Los Altos also have some attraction; but nothing like what Santana Row is offering. Since the Santana Row development, number of other similar developments are popping up, which is an indication of a new trend. To resist this trend is to stay behind, which will affect the property values and your own tax base.

I travel through Loyola corner at least twice a day and since I am also concern about my property value, I see that by just talk of Loyola corner development the property values in that area are posed to go up. So, why would anybody oppose such development? There were concerns about the tunnel affect and you don't see anybody complaining about that type of affect in Santana Row where they went 4 stories up.

I understand change is difficult, but at a same time it is required. Our town has been the last town in this area to develop their downtown and as a result left behind to attract people to our town. This will have a negative effect and in time we will lose desirability and finally lower property values, which will result in lower taxes revenue.

Therefore, I urge you to approve this project and encourage the planning commissions that the residents of Los Altos are concern with their conservative approach in approving such developments.

Thank you for your time and your consideration.

Thank you

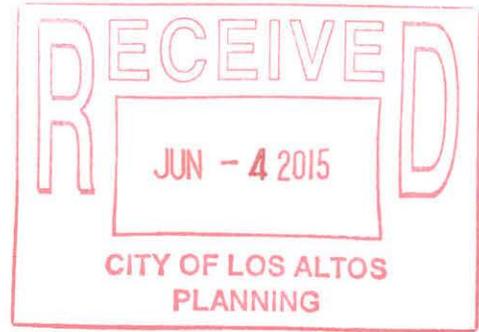
Ehsan Talebi, Ph. D., PE



Los Altos Planning and Transportation Commission
c/o James Walgren, Staff Liaison
One North San Antonio Road
Los Altos, CA 94022

June 2, 2015

Dear Planning and Transportation Commissioners:



I am unable to attend the meeting on June 4. Unfortunately, I am out of town. I am opposed to the proposed mixed-use application for the project at 999 Fremont Ave. We all can agree that Loyola Corners is in need of development. However, the proposed project does not fit the character of the neighborhood. There is too much bulk and mass, and the style is not consistent with the character of Loyola Corners.

Loyola Corners is a quaint, small-scale retail district. The proposed development is not quaint nor is it small-scale, and it is primarily residential. The building offers approximately 1200 sq feet of retail, in two very small units. The design does not allow for these two small units to be combined as one more substantial retail unit. Less than 10% of the building is designated as retail. I would like to see more retail (or services) that are likely to draw residents from other areas to Loyola Corners.

I am not clear why developers are considering three-story buildings for Loyola Corners. Are we also considering three-stories for Rancho, Woodland Acres or Foothill Crossings? It appears as though this property owner has decided that Loyola Corners should be a development similar to Santana Row or some of the other recent developments along the El Camino in Mt. View, Sunnyvale and Santa Clara. If Mr. Bunker is successful with this, other commercial property owners will follow.

A committee made up of residents and business owners developed the Loyola Corners Specific Plan. This development is not in compliance with the Plan. It is not appropriate for one owner to reassign the look and feel of Loyola Corners. It is time to form a committee to reevaluate the Loyola Corners Specific Plan so that both residential and business property owners have a clear and unified vision.

In addition, I am very much opposed to the lift style parking more commonly seen in densely populated cities such as Los Angeles, New York and Chicago. The concept is not in keeping with small town living, especially when applied to a mixed-use project.

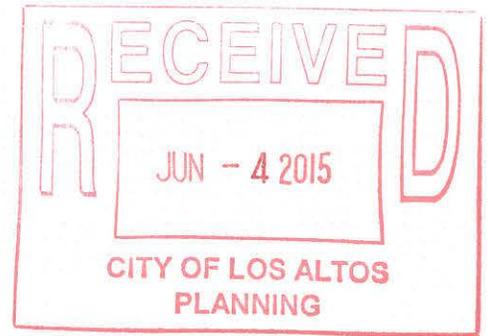
Further, the configuration of the triangle (A Street, Miramonte and Fremont) currently has problems with traffic flow and safety. There are frequent accidents at the corner of A Street and Miramonte Ave., and cars have crashed through the front of Tom's Depot many times over the years. The addition of a loading/unloading zone as well as the garage entry/exit on Miramonte Ave. would greatly exacerbate the traffic problem at the three intersections.

I respectfully request the Commission deny the application for the mixed-use project at 999 Fremont Avenue. I am also requesting that a committee be formed to evaluate and make recommendations to update the Loyola Corners Specific Plan, to help create a unified vision for our cherished Loyola Corners district. I served on the original committee, and would be honored to serve again. Please, do not let this get away from us. This could be a decision we will all regret for decades to come.

Sincerely,
Debbie Skelton

Richard & Linda Newton

Los Altos, CA 94024



June 3, 2015

Los Altos Planning and Transportation Commission
c/o James Walgren, Staff Liaison
One North San Antonio Road
Los Altos, CA 94022

Planning and Transportation Commissioners:

We are opposed to the proposed mixed-use project at 999 Fremont Ave. The project is not in compliance with the Loyola Corners Specific Plan, and is not appropriate for the location. Specifically, the plan calls for a combination of Retail on the first Floor with Offices on the second floor. This proposed development is roughly 90% Residential?

In addition, we oppose the proposed project on the grounds that it clearly does not fit the character of the neighborhood, and is massively out of scale with the surrounding properties. The owner-applicant represents that the presence of the "Clock Tower" office center proves that there is precedence for large multi-story structures at Loyola Corners. However, he ignores the fact that those buildings are far apart from any other buildings and really not even visible other than when sitting at Fremont and Miramonte traffic lights. The buildings blend into their surroundings and pose no sight-line risks nor traffic or parking problems.

One wonders what the negative reaction of the North Los Altos residents would be if this building were proposed for Main Street downtown. Fremont Avenue is Loyola Corners "Main Street" and we have the right to expect and retain the same "village" feel as downtown residents and visitors enjoy.

If this project is approved, every Fremont Avenue land and building owner will want to construct large multi-story buildings to maximize the return on their investment and Fremont Avenue at Loyola Corners (our Main Street) will resemble downtown First Street with its massive Safeway and office and condominium projects!

We respectfully request the Commission deny the application for the mixed-use project at 999 Fremont Avenue.

Richard Newton
Linda Newton



January 8, 2015

To Whom It May Concern:

Following a brief meeting with the developer and Architect relative to the Plan for a new building at Loyola Corners, I would like to offer a few Comments.

First, I think the size and composition of the proposed building is very attractive and would compliment the local area.

Second, from what I was able to gain from the plans shown to me, the addition of retail space and housing, particularly nice I might add, is a bonus for the area as well.

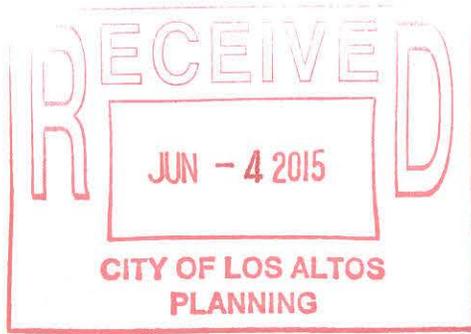
The building this plan would replace is a marked improvement.

I hope my comments are helpful.

Sincerely,

Carrie Lee Mohun
CARRIE LEE MOHUN

LOS ALTOS CA 94022



Katherine Wurzburg

Los Altos, CA 94024

June 4, 2015

jwalgren@losaltosca.gov

Los Altos Planning and Transportation Commission
c/o James Walgren, Staff Liaison
One North San Antonio Road
Los Altos, CA 94022

Dear Planning and Transportation Commissioners:

I have lived in Los Altos for 35 years and seen many changes over the years, most of them positive. I appreciate the work that the Commission does to move Los Altos forward towards more economic vitality. I am writing today about the mixed-use project at 999 Fremont Avenue in Loyola Corners and why I am opposed to this particular plan. Instead of approving new projects in a piecemeal way I would prefer to see a comprehensive plan for Loyola Corners that takes into consideration support for existing businesses, traffic and safety issues, and the unique character of Loyola Corners. If this building is approved it will have a negative domino effect on this historic neighborhood as other building owners will want to capitalize on the opportunity to build 3-story mixed-use buildings (read condos), forever changing the landscape of Loyola Corners. This area could certainly use a face lift but approving massive 3-story buildings along Fremont Avenue is not the way to proceed. Where is the vision?

New development should keep the essence of Historic Loyola Corners

A Google search shows many references to "historic Loyola Corners". The City of Los Altos has recognized the importance of this historical district as an early train stop for Southern Pacific commuters with a plaque on Fremont Avenue at the gas station that clearly states "Historic Loyola Corners" and includes a description of the history of the area.

Other descriptions of this neighborhood include "rustic houses and shops with an old country feel", "quaint neighborhood with a rustic feel, bolstered by many mom and pop shops that retain the old country character of the community's rich history" and "quaint character". Any new development should be compatible in scale and impact with the existing quaint and rustic neighborhood and should be in keeping with the spirit of the Specific Plan for Loyola Corners.

Retail - Short-term and long-term effects on local businesses:

Isn't it the role of the City to help support local businesses and help them thrive? If this building is approved it will have negative short-term and long-term effects on the businesses at Loyola Corners. Approving this building now while businesses are being affected by the work done on the two bridges means more uncertainty for tenants.

Short-term:

- Loyola Beauty Salon will be put out of business, losing a valued and popular service to local residents.
- Tom's Depot will lose customers because it will be right across the street from a noisy construction zone. Where will construction trucks enter the work site? Hopefully not on A Street. Where will the construction workers park? There is no parking along Fremont, Miramonte and A Street and that means using the lot behind Fremont that serves Tom's Depot, Café Vitale, Bicycle Outfitter and the dance studio, meaning their customers will have a hard time parking.
- This plan proposes two small retail spaces more suitable to office space than a store, salon or restaurant. Plus, the proposed parking plan is confusing and hard to find. How does this serve the neighborhood?

Long-term:

- Approval of this 3-story building means that other landlords will want to do the same and that means that the businesses along Fremont Avenue will have to go. Who will come in to replace them? Brand new buildings mean higher rents and we all know the problems that rent increases have had on retailers downtown. Do we want a street full of empty store fronts as we had recently on Main and State Streets downtown?
- What about Tom's Depot, the heart and soul of Loyola Corners? Generations of residents have enjoyed the warm hospitality and familiarity of this neighborhood restaurant, even with the different names over the years. If Tom's goes, there goes the "historic" aspect of this neighborhood. Is that what the City considers progress? The local residents certainly don't.

Housing:

Yes, I am aware that housing in Silicon Valley and in Los Altos - especially affordable housing - is in short supply. If I understand the plans correctly, this building has four condos with a total of 14 bedrooms. That is conceivably an additional 18+ residents in this one building, not including the owners and customers of the two retail spaces on the first floor. That is a lot of people coming and going on a daily basis in an area that has seen more than its share of traffic accidents, many which go unreported. This is a dangerous intersection and may be made worse when the Loyola Bridge is complete.

Have you considered the impact of an influx of 100 people on the neighborhood if this building and others like it are approved?

Also, how responsible is it to be adding more residential units when we are in a serious four-year drought?

In closing, I attended this meeting on December 4, 2014 leaving with the impression that this project had been unanimously denied by the commissioners who voted on it. I personally felt blindsided by what occurred after that meeting and what transpired at the January meeting. I am

not alone in that thinking. Considering that local residents are against this particular project for many reasons (including the fact the builder did not incorporate any of their suggestions at an August 2014 meeting and they voiced their concerns at the public hearing in December) why would the Commission approve the "revised plans" when it is still an enormous condominium building with so little retail space? If you were not in favor of this building why do you think it should be approved now?

I respectfully request that you vote no on this project and work with the local residents on a plan for new buildings that will help revitalize historic Loyola Corners while not destroying the existing businesses and adding to the already dangerous traffic situation.

Sincerely,

Katherine Wurzburg

Los Altos, CA 94024

Los Altos Planning and Transportation Commission
City of Los Altos, California
2 June 2015



Dear Planning and Transportation Commissioner of Los Altos,

We are writing to voice our opposition to the proposed development at 999 Fremont Ave., Los Altos, California. Having had the opportunity to review documents the Commission has made available on line, we wish to raise the following objections and concerns:

1. Although modifications have been made in the plans over the last year, the project as a whole continues to violate the General and Loyola Corners Specific Plans, as pointed out by Commissioner Baer in his motion to deny, in Attachment E, dated, 12-4-14. Further, we are unaware of any legal basis for violating the Plans due to the resoning, as stated in Planning and Transportation Attachment E 12-4-14, that this small, awkward parcel is a "prominent gateway site where design guidelines trump the zoning requirements." While we welcome updating the Loyola Corners area, what is a small island parcel in the midst of an already over-used, awkward, and dangerous intersection should in no way be allowed to contribute to worsening the density and dangerousness of this intersection, and especially should be prohibited from adding residential units, which are contrary to the Plans and the nature of the majority of residences in the neighborhood.

2. The Commission is already aware of the dissatisfaction in the town with recent developments in the downtown area. Repeating and amplifying the mistakes of those projects anew in Loyola Corners, to appease a developer's misplaced sense of "hardship" regarding a parcel that the Plan designates as a plaza, is contrary to both the letter and spirit of the Plans. The owner of the parcel has not made a compelling case for exceptions to the Plan. The current proposed development, however, creates an ongoing hardship for the Loyola Corners area by bringing a degree of urbanization, including noise pollution as detailed below, to an area that has indicated on many occasions its desire not to be urbanized any further. There are other, more acceptable ways to re-develop this area without this degree of urbanization.

3. The parcel at 999 Fremont Ave. is unusual within Los Altos, and thus its uniqueness requires special attention by the Commission to the Los Altos and Loyola Corners Specific Plans. It is a small, irregular parcel with extremely limited access, poor sight lines for ingress and egress on streets where speed limits are often exceeded, and which are already unsafe for both pedestrians and bicyclists. As Commissioner Bressack pointed out in Attachment C, the 1-23-14 Minutes of the

Planning and Transportation Commission on page 2, the "parking plan is marginal and creates a difficult circulation pattern." Nothing that has been proposed substantially changes this finding.

The current proposal, in the letter from M Designs Architects dated 5-20-2015 still has "less than ideal ingress and egress," and that "the site is too compact to provide ramped underground parking." The proposal for "indoor protected" parking without fan ventilation, along with the need to have an audible warning for pedestrians that cars are exiting, is incompatible with the surrounding neighborhood.

There is no indication by noise consultant Joshua Roper in Attachment E of the 12-4-14 Minutes of the Planning and Transportation Commission of the required decibel level of the proposed auditory warning signal. If the ambient noise described in the noise study is already in the 63-67 decibel range, for an elderly pedestrian to hear the signal above not only the ambient noise level, but the passing of heavy trucks from the Loyola Corners branch of the US Postal Service, which arrive both day and night, and other heavy trucks that access the service station and other businesses, the decibel level of the warning would have to be significantly louder than 67 decibels.

The Occupational and Safety Health Administration lists the noise level of a heavy truck at 15 meters (approximately 45 feet) at approximately 85 decibels. For a person with diminished hearing, such as is found among the ambulatory elderly in the community, to have a warning signal be heard above the cumulative ambient 67+ condenser units at 76.8 (see point 3, below) + large truck 85 decibels (i.e., 85.7 decibels, as the formula for calculating them is not linear. 77 decibels is perceived to be twice as loud as 67 decibels, and 87 decibels is twice as loud as 77 decibels so the final environmental perceived noise decibel level would be nearly 4 times the ambient noise). To produce a warning signal above this environmental noise level, the warning signal would have to approach the levels of a jackhammer (see OSHA website: <https://www.osha.gov/SLTC/noisehearingconservation/>).

Further, as designed, the parking will not transform after business hours, so that the auditory warning will have to be in use 24 hours per day, 365 days per year. Assuming that the goal of Loyola Corners redevelopment is to increase pedestrian use of the area, and that persons residing in the proposed condominiums will not be subject to curfews for using their automobiles at night and in the early morning, the need for auditory warning signals that will probably have to be at least 90-95 decibels to be effective, will result in extremely disruptive noise blasts at any and all hours, every time a car leaves the premises. At night, the noise would carry even farther into the surrounding neighborhood and disturb residents' ability to sleep, to carry out activities without noxious auditory intrusions, and to enjoy the peace of their properties. This aspect of the proposal is unacceptable to residents of the neighborhood, and may also violate local noise ordinances. As Commissioner Baer

stated in the minutes for Attachment C, "the design should be reconsidered from scratch."

Further regarding the proposed parking, there is no provision for parking for visitors to the residential units during business hours. This is simply an unrealistic expectation. Stacked parking creates additional hazards in California where earthquakes are likely to render public power sources inoperable for extended periods. There is no provision for residents and business workers to access to automobiles during power outages, significant earthquakes, or other emergencies. As Traffic Engineer B. Jackson of Hexagon stated in Attachment E, 12-4-2014, this is an "awkward parcel." Nothing in the current plans adequately addresses the parking for this site, which is awkward as a business location, and inappropriate for residential use.

4. A further noise problem arises from the description of the proposed rooftop ventilation units in Attachment E point number 4. They are described in a Memorandum designated Attachment G. The proposed rooftop condensing units are described in general, with "estimated" noise levels, which have no guarantee of being the minimum noise levels produced. The proposal is for 6 condensing units to be run simultaneously, each producing 69 decibels for a total of 76.8 decibels. They are proposed for the top of the 30-foot building. This position means that they will likely exceed the 30 foot limit of the building, and their position further means that the noise they generate will be broadcast further into the neighborhoods and contribute to ambient noise pollution.

The Memorandum cites Los Altos Municipal Code Section 6.16.050 that equipment operating in the daytime may only generate noise 5 decibels higher than the ambient noise level, and 10 decibels lower at night. How the 6 units described on p. 4, which are estimated to produce 69 decibels each (total output 76.8 decibels), which will run simultaneously, as described in the memorandum, is clearly going to violate the noise levels of the Code. Although there is a drop-off with distance from the noise source, there is no calculation that demonstrates that the noise from these units will be reduced to 35 decibels or less at the adjacent properties.

Thank you for your consideration of these significant concerns about the proposed development at 999 Fremont Ave., Los Altos, California.

Sincerely,



Teresa Ullmann

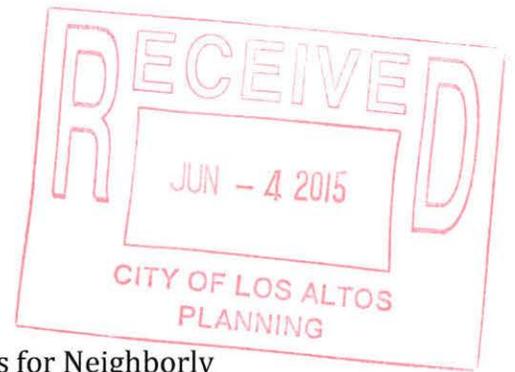


Ronald Ullmann



Emily Ullmann

Los Altos Planning and Transportation Commission
City of Los Altos, California
4 June 2015



Regarding 999 Fremont Ave.

Remarks to Commission Meeting on behalf of Los Altans for Neighborly Development (LAND)

Concerning: Noise

Thank you for the opportunity to present our Noise concerns regarding the proposed development of 999 Fremont Ave.

According to the Loyola Corners Specific Neighborhood Plan, page 6 states that the specific goals of the plan are to "preserve and protect adjacent residential neighborhoods from ...noise ... impacts. Page14, states that one of the "two preeminent concerns articulated in the Plan" is "protecting nearby residents from unreasonable intrusions from the shopping area." Page 17, which deals with implementation of the priorities, includes, "increased buffers to reduce the intrusion of ... noise into the residential areas."

Concerning 999 Fremont, if the proposed development were a Plan-permitted use of the parcel, the following concerns have not been analyzed and addressed:

The letter from M Designs Architects of 5-20-2015 states that the site has "less than ideal ingress and egress." Attachment G, a Memorandum dated 21 October 2014 from S.M. Sullivan and J.M. Roper, states Los Altos Municipal Code "limits noise levels at residential and commercial property lines to 50 and 60 dB, respectively. Equipment operating during daytime hours only may generate noise levels to 5db higher, i.e., 55 and 65 dB. Existing business hour noise levels were measured at 56 to 69 decibels, already higher than permitted.

Proposal calls for 6 air condensing units, each producing 69 decibels. They will be operating simultaneously, and during evening hours. Every 10-decibel increase results in a doubling of perceived loudness by a human listener. Decibels are a log-function, and cannot simply be added together. Six 69-decibel units will produce 76.8 decibels on top of the ambient 67 decibels. The passage of a large truck, such as postal semis, and large gasoline and other supply trucks for the commercial area are estimated by OSHA (Occupational and Safety Health Administration) to be 85 decibels at 15 meters (approximately 49 feet, farther away than the pedestrians experience while on the sidewalk. These three noise levels combine to a minimum of 85.7 decibels.

On June 4, 2015, The Planning and Transportation Commission, on p. 7 under Transportation Analysis stated, "... staff included a condition of approval requiring a

pedestrian warning system to alert pedestrians on the sidewalk to cars exiting the garage." Typically such warning systems combine both light and sound. Given the potential of greater than 85.7 decibels at street level, and the high likelihood of pedestrians including adults with age-related hearing decrements, the warning signal would likely have to be above 90 decibels to be audible in a traffic situation which occurs multiple times per day, at both predictable and random times.

According to the OSHA sound level table (at <https://www.osha.gov/SLTC/noisehearingconservation/>) for an audible warning to be expected to be heard, it would have to be louder than a jackhammer at 15 meters/ 49 feet, except that this warning will be blasting an unsuspecting pedestrian on the sidewalk, closer than 50 feet from the sound source, and therefore much louder. Such sound levels are capable of causing damage to hearing.

In addition, because there is proposed residential use of the parking facilities, there can be no time of day restriction on entrance and exits, so that the surrounding neighborhoods, would be subjected to noise blasts day and night, that are in violation of the municipal code.

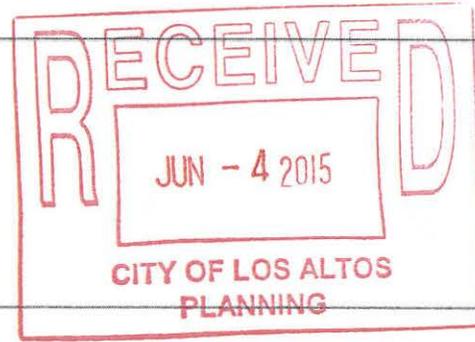
Finally, the proposed placement of rooftop condensor units and uncertain height of the placement of a loud warning signal will, by the nature of their height being well above that of surrounding buildings, be broadcast over a wider geographic area than they would if they were in an enclosed space. Architects and city planners from ancient times have elevated bell towers and minarets as ways of effectively broadening the radius for sound projection. Nothing in the planning documents addresses these noise issues. These noise levels are incompatible with the surrounding residential areas and the enjoyment of sidewalks and benches in the outdoor environment.

I urge the Commission to reject the development proposal for 999 Fremont Ave. on the basis of knowable and predictable noxious increases in noise on the site of the project and in surrounding neighborhoods.

Teresa Ullmann

Los Altos, CA 94024

From: David Kornfield
Date: Thursday, June 04, 2015 6:18 PM
To: Planning Transportation Commission
Subject: FW: 999 Fremont Avenue Proposed Development



From: Lee Lera
Sent: Thursday, June 04, 2015 5:49 PM
To: James Walgren; Sean Gallegos; Zach Dahl; David Kornfield; mmctighe@losaltosca.gov
Cc: leelera@sbcglobal.net; 'Kathy Lera'
Subject: 999 Fremont Avenue Proposed Development

Dear Commissioners,

Unfortunately we are not able to attend tonight's meeting but wanted to provide you with our thoughts.

We reside on Eastwood Drive in Los Altos, a short walk from Loyola Corners. We frequent Loyola Corners almost on a daily basis. Both my wife and I grew up in Los Altos; we work here and we are involved in our community because it's our home, our town and we care about it and its future. We are the first to say that Loyola Corners needs attention. In fact this has been the case for many years, but the current development being proposed is way outside anything we feel is appropriate for that little corner of our town. Although the current proposed project may meet the letter of current design specifications and code, it does not meet the spirit of providing improvements to such a historic neighborhood. Another concern is that approval of a project of this size and scope will just open the floodgates for other developers to propose projects of the same magnitude or maybe, even larger. Loyola Corners does need help, but it needs an updated and more specific plan to determine what will work and bring benefit to the businesses and the neighborhood. We understand a plan does exist, but was created many years ago and, more likely than not, is outdated.

The current design for 999 Fremont Avenue, as proposed, is a definite traffic hazard for those attempting to turn left from A Street onto Miramonte and those attempting to turn right on Fremont from A Street. Based upon proposed pedestrian cross walks, traffic will need to stop in a position which will not allow them to see oncoming traffic to make a safe turn. This could only be mitigated by a much more significant setback of the building than currently proposed. Current photos provided on the City's website do not address these concerns. The position of the renderings don't provide an accurate view of realistic traffic issues and flow within Loyola Corners, for example, it is very rare when there are so few vehicles on the streets of Loyola Corners during the day. Care should be taken to provide safe vehicle, pedestrian and bicycle traffic flow in the area during the busiest times of the day. The worst case scenario, such as commute times, should be the minimum for planning so that the plan can accommodate future expansion.

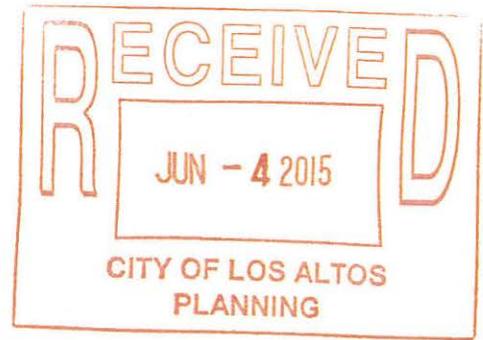
We request that any decision on development of any project/parcel within Loyola Corners, such as this project, be postponed until we have a plan that addresses all the issues that development will bring, including infrastructure, changes of traffic flow due to the current construction to Loyola bridge (lots of unknowns since completion is about a year out), bike and pedestrian safety, keeping the small town look and feel of our little neighborhood, just to name a few.

Sincerely,

Lee & Kathy Lera

Los Altos, CA 94024

6/8/2015



January 10, 2015

To Whom It May Concern:

The project at Loyola Corners to replace the existing beauty salon appears to be a winner.

It is a beautiful building and one long overdue for this area. It will go a long way in connecting all the various areas of Los Altos.

Loyola Corners, because of its proximity to Foothill Expressway and Miramonte Avenue really needs an update, additional retail space and housing. It seems the drawings and elevations I was able to examine accomplish these objectives.

I am most amenable to this project going forward.

Sincerely,

A handwritten signature in black ink, appearing to read "Helene Lam".

Helene Lam

LOS ALTOS, CA 94022.

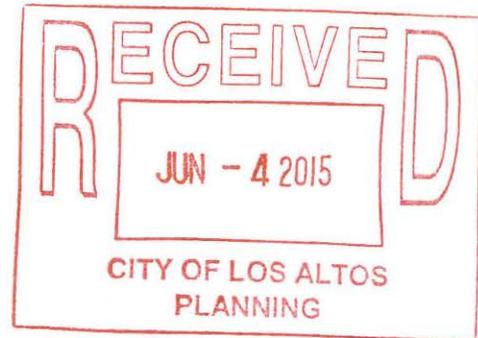
David Kornfield

From: James Walgren
Sent: Thursday, June 04, 2015 5:18 PM
To: David Kornfield; James Walgren; Jerry E. Moison; Jon Baer; Kenneth Lorell; Malika Z. Junaid; Michael McTighe; Phoebe Bressack; Ronit A. Bodner; Yvonne Dupont
Cc:
Subject: FW: Opposition to 999 Fremont Avenue

Please see attached, James

James Walgren, AICP
Community Development Director
650.947.2635

City of Los Altos
One North San Antonio Road
Los Altos, California 94022



NEW! Sign-up to receive City of Los Altos news delivered right to your inbox! www.losaltosca.gov/enotify

-----Original Message-----

From: Cynthia Gaertner-Bridges
Sent: Thursday, June 04, 2015 5:05 PM
To: James Walgren
Subject: Opposition to 999 Fremont Avenue

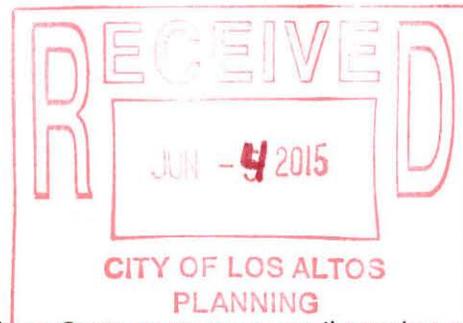
Dear Planning and Transportation Commissioners,

I am writing to express my opposition to the proposed mixed-use project at 999 Fremont Avenue.

As a 17-year resident of this Los Altos neighborhood, I am dismayed that such a project -- which is so out of character with our neighborhood -- would even be up for consideration. Not only are the size and scale entirely inappropriate for the area, but the related traffic and parking problems that would arise make it harmful for the neighborhood. As members of the planning commission, please consider not just the financial or economic factors, but also the health, safety and quality of life of residents. The 999 Fremont Avenue project does not add value to our neighborhood or town, but rather detracts from it.

Los Altos has a special character found nowhere else in the Bay Area, and once our land is developed inappropriately, there is no going back. In your role as stewards of this town's character and history, I respectfully request that you deny the application for this project.

Best regards,
Cynthia Gaertner-Bridges



June 4, 2015

Dear PTC Commissioners:

On March 13, 2015 Greg Bunker's Silicon Valley Business Center sent me an email newsletter containing the following information:



Real Estate Development

I have a few real estate development projects currently in the works. One, a mixed-use development in Los Altos, is getting closer to coming to fruition. We hope to break ground on it in the next year or so.

Mr. Bunker is presenting his third proposal for 999 Fremont to the Planning & Transportation Commission on June 4th. The first two were denied.

His architect claims that "the proposed project is wholly consistent with the General Plan and the Loyola Corners Specific Plan (LCSP)."

I respectfully disagree. The LCSP calls for an "attractive and functional shopping and commercial use facilities, maintain existing small pedestrian scale, preserve and protect residential neighborhoods from traffic, noise & visual impacts, provide safe & effective circulation and parking, provide range of neighborhood-serving commercial uses."

From the LCSP:

- *Continue & rigorously enforce existing City policy requiring retail uses on the first floor of commercial buildings.* The staff report says, "The project will provide approximately 1,800 square feet of better oriented ground floor commercial space." In fact, only 1300 square feet is on the ground floor, with the rest as basement storage. That's less than 10% of the building. Most of the first floor is a parking lot.
- *Assure adequate parking to support Loyola Corners at locations which make shopping easier and which do not disrupt nearby residential areas.* The plan includes 8 spaces for tenants (but none for guests , e.g., book clubs, play-dates, lunches, housecleaners, etc.); 5 for retail (not sufficient for employees and customers) and 1 ADA. There's little available on-street parking in the area and the lot behind Tom's is usually full, particularly at peak hours.
- *There is a general perception that significant traffic and circulation problems exist in terms of efficient traffic flow, traffic safety, parking and pedestrian access.* Garage access to this building will be through a roll-up door on Miramonte, where loading and unloading will also take place. This will add to traffic confusion and delays near the A Street/Fremont/Miramonte intersections, already congested during commute and school hours.

- *New buildings should be compatible in terms of scale, color and materials with surrounding structures.* Although the third floor has been set back, the building still stands out as a massive structure on a small island. The building's bulk and mass do not respect the surrounding area.

In a November 24, 2014 letter, Mr. Bunker says his building will "be a community/neighborhood benefit by both creating a first class introduction to Los Altos for anyone driving into the City from Silicon Valley and establishing new easily accessible retail and commercial space."

Many community members do NOT consider this project a benefit for the reasons stated above. The LCSP says: *As with all aesthetic issues inherent in community design, recognition of how residents feel about the area in terms of the relationship between people and the constructed environment is paramount.*

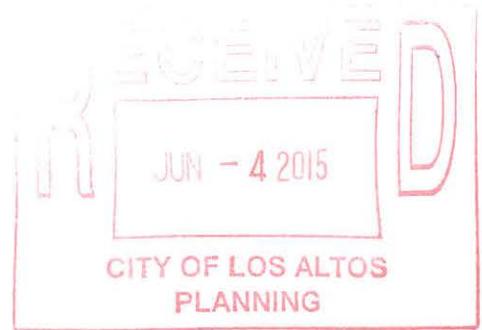
I have circulated this email to neighbors and friends, who agreed to co-sign this letter with me. Their names and addresses are below.

Thank you for considering our comments.

Pat Marriott

Leonard Yool
Teresa Moore & Tom Ferry
Katherine & Steve Wurzburg
Richard & Linda Newton
Joanne Schott
Susan Pierce
Teresa & Ron Ullman
Emily Ullman
Carolyn C. Synerholm
Katherine Poonen
Shobana & Yogesh Gubbi
Wendy Furuichi
Wilson T. Chang
Kitty O. Lee
Amy L. Wright
Sunil Upender
Jessie Jacob
Candace Behlendorf
Jeff and Christina Cox
Ron Meserve
Mardell (Dell) Larcen
Loren Chow
Catherine & David Greenberg
Martha Glew
Rich & Susan Redelfs
Sendur & Silaja Sellakumar
Richard and Phyllis Godfrey
Kathleen Chu
Catherine Karol
Andrew Pejack

Patrick Pejack
Jeff and Christy Richardson
Christine Hoberg
Paula & Vaughn Stanek
Katherina and Jeff Russakow
Chinchung John Won
Abby King & Steve Lovett
Kris Potter
Neil and Marilyn Hornor
Jan and Maria Tavenier
Paul & Shirley Tavenier
Joan Takenaka
Sherri & Phil Shemanski
Lawrence Aronson MD
Linda and Rick White
Wendy & Rick Walleigh
Andrea Hawkins
Camille Casale
Terry & Heather Larkin
Travis Vu
Stephen More
Henry & Adelle More
Debbie Skelton
Jeff Wildfogel
James Lempke
Barbara & Kevin O'Reilly
Gail Ostendorf
Maria Gonzales
Mariah Lopez
Tammy Le



January 9, 2015

To Whom It May Concern:

I would like the opportunity to voice my position and opinion relative to the proposed mixed use project on Loyola Corners.

On balance I feel it is an positive endeavor and for the following reasons:

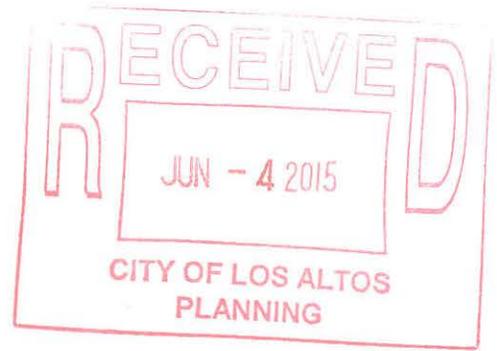
1. It should add much needed vibrancy to the local area.
2. The mixed use concept will add housing to the area.
3. It will provide an expanded and updated retail environment.
4. It will replace a 30 year old structure with a rather large and infrequently used parking area. Parking will be within the structure.
5. It will provide pedestrian access completely around the site.
6. The mass and scale of the building appears to compliment the surrounding area.

I make these comments based on a cursory review of the developer's and Architect's plans.

Cordially,

Diane Thomas
Manzoni & Company

Los Altos.



January 9, 2015

To Whom It May Concern:

I would like the opportunity to voice my position and opinion relative to the proposed mixed use project on Loyola Corners.

On balance I feel it is an positive endeavor and for the following reasons:

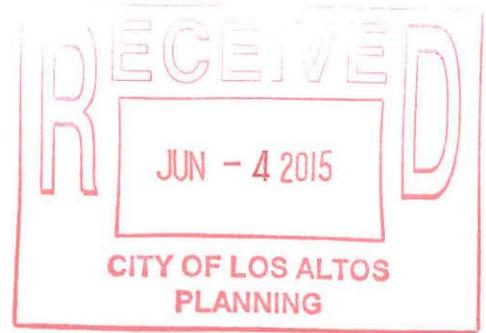
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3. It will provide an expanded and updated retail environment.
4. It will replace a 30 year old structure with a rather large and infrequently used parking area. Parking will be within the structure.
5. It will provide pedestrian access completely around the site.
6. The mass and scale of the building appears to compliment the surrounding area.

I make these comments based on a cursory review of the developer's and Architect's plans.

Cordially,

Gas Ann Spence
Los Altos Resident

ORIGINAL



June 1, 2015

To The Planning Commission:

Re: 999 Fremont Avenue

I would like the opportunity to voice my favorable position and opinion relative to the proposed mixed use project on Loyola Corners.

On balance I feel it is an POSITIVE endeavor and for the following reasons:

1. It should add much needed vibrancy to the local area.
2. The mixed use concept will add housing to the area.
3. It will provide an expanded and updated retail environment.
4. It will replace a 30 year old structure with a rather large and infrequently used parking area. Parking will be within the structure.
5. It will provide pedestrian access completely around the site.
6. The mass and scale of the building appears to compliment the surrounding area. The building at 30' height is lower than the US bank bldg. and the tower as well as the building to which the tower is attached.

I make these comments based on a cursory review of the developer's and Architect's plans.

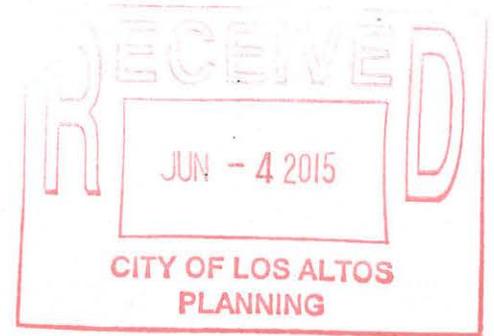
Cordially,

A handwritten signature in cursive script that reads "Judy Sims".

Los Altos, CA. 94022

Property of - 999 Fremont

January 9, 2015



Dear Madam and Sir:

The Architect for the proposed new building at Loyola Corners showed me the plans for the new structure. Actually he presented more than one set of perspectives illustrating the progress from the initial concept to what now is apparently a final proposal.

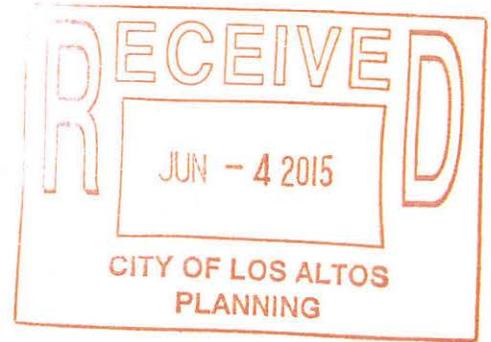
I feel that although the original concept provided the amenities required for such a project, the scale and density were a bit overwhelming.

The elevations for the most recent proposal appear to have resolved these issues. The building now is much less dense and more fluid.

In summary, I would vote yes for this most recent proposal.

Cordially,

Cher Paul - Beauty, Gifts + More
Property of 995 A Street



January 9, 2015

To Whom It May Concern:

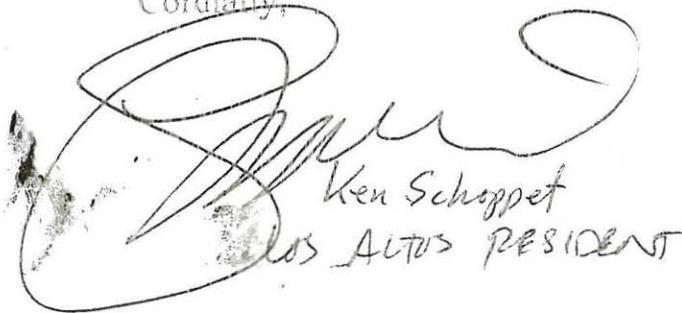
I would like the opportunity to voice my position and opinion relative to the proposed mixed use project on Loyola Corners.

On balance I feel it is an positive endeavor and for the following reasons:

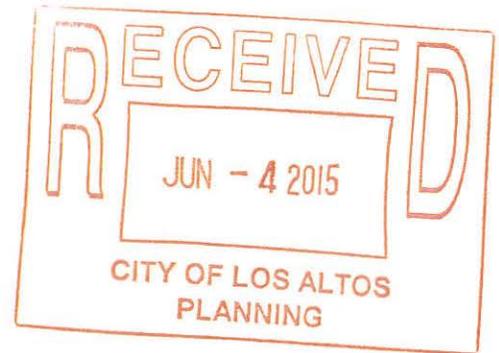
1. It should add much needed vibrancy to the local area.
2. The mixed use concept will add housing to the area.
3. It will provide an expanded and updated retail environment.
4. It will replace a 30 year old structure with a rather large and infrequently used parking area. Parking will be within the structure.
5. It will provide pedestrian access completely around the site.
6. The mass and scale of the building appears to compliment the surrounding area.

I make these comments based on a cursory review of the developer's and Architect's plans.

Cordially,



Ken Schoppet
LOS ALTOS RESIDENT



January 11, 2015

To whom it may concern:

I understand there are some questions involving the height of the proposed plan to replace the beauty shop at Loyola Corners with a new three story building containing retail space, housing and off street parking.

One only needs to look at the "Clock Tower" building located at the corner of Miramonte and Fremont to see that should not be an issue.

I'm told that building's overall height is between thirty three and thirty five feet. From looking at the elevations of the proposed new structure the overall height is only thirty feet.

I would think this would be a minor issue as the proposed building, in my opinion is less bulky that the clock tower building.

When one considers all that this new building will provide: Additional quality retail space, nice housing, off street parking and a very good looking structure easily accessible from all three sides, the prospect for going forward should be a no brainer.

You've got my affirmative vote.

Sincerely,

Sue Roche

Los Altos



January 8, 2015

Loyola Corners Project

Dear Sirs and Madams:

Last week I met with the developer and Architect of the subject project.

I was asked after a brief review to give my opinion on the project's value in a number of areas.

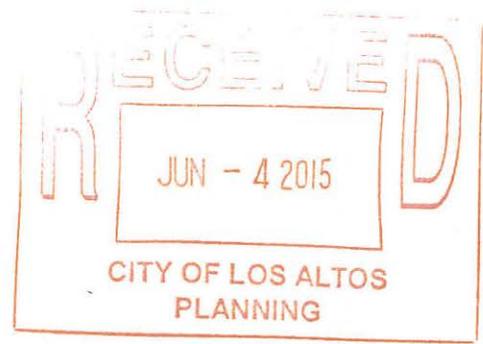
My answers were all positive as they related to the configuration of the building, the new and increased retail space in the area, the elegant housing and improved access to this part of Loyola Corners.

Hopefully this project will move forward and that others will follow in the future.

Thank you,

Mary F Park
Barr Park

Los Altos, CA 94022



June 2, 2015

To the PTC Commissioners and staff:

RE: Loyola Corners

I POSITIVELY ENDORSE THE PROJECT at 999 FREMONT AVE

Following a brief meeting with the developer and Architect relative to the Plan for a new building at Loyola Corners, I would like to offer a few Comments.

First, I think the size and composition of the proposed building is very attractive and would compliment the local area.

Second, from what I was able to gain from the plans shown to me, the addition of retail space and housing, particularly nice I might add, is a bonus for the area as well.

The building this plan would replace is a marked improvement.

I hope my comments are helpful.

Sincerely,

DICK KENARNEY

A handwritten signature in black ink, appearing to read "Dick Kenarney", written over the printed name.

LOS ALTOS, CA 94022

MICHAEL PREVOT

5/31/15

Sean:

I support the project @
Loyala Commons. Please help the
project & give your full support &
make it look nice on the "other
side of the city"! Best regards
Mike Prevot



From: David Kornfield
Sent: Wednesday, June 03, 2015 5:39 PM
To: Planning Transportation Commission
Subject: FW: Support of Loyola Corners Development
More correspondence regarding the 999 Fremont project.

From: Tony Carrasco
Sent: Wednesday, June 03, 2015 11:57 AM
To: David Kornfield
Subject: Fwd: Support of Loyola Corners Development



Hi David,
I am in support of Gregg Bunkers project at 999 Fremont Street.
I am an architect primarily on the Peninsula and we are designing a project across A Street from 999 Fremont.
After studying the Specific Plan for the area, we have found that three story buildings at any of the sites in the Specific Plan are the only economically viable alternative we have.
I support the building as designed as well as the landscaping, wider sidewalks and street trees.

On Tue, Jun 2, 2015 at 5:55 PM, _____ wrote:
James Walgren, David Kornfield and Los Altos Planning Commissioners,

I would like to voice my support for Greg Bunker's proposed development at 999 Fremont Ave, Los Altos. I've been a resident, builder and developer in Los Altos for the past 25 years. I am familiar with his project and the numerous changes and concessions he has made to improve his project. I believe this project will be a great asset to Loyola Corners and the Los Altos community.

Gary Ross

--
Tony Carrasco
CARRASCO & ASSOCIATES
<http://www.carrasco.com/>
1885 El Camino Real, Palo Alto CA 94306
[650-322-2288](tel:650-322-2288)

--
Tony Carrasco
CARRASCO & ASSOCIATES
<http://www.carrasco.com/>
1885 El Camino Real, Palo Alto CA 94306
[650-322-2288](tel:650-322-2288)

[Print](#) | [Close Window](#)

Subject: 999 Fremont Los Altos, California
From: Jeannice Fairrer Samani
Date: Mon, Jan 12, 2015 11:24 pm
To:
Attach: 999 Fremont Ave.pdf

Hi Judy,

It was a pleasure having coffee with you and Dick. Here is the letter.
The best on your project.

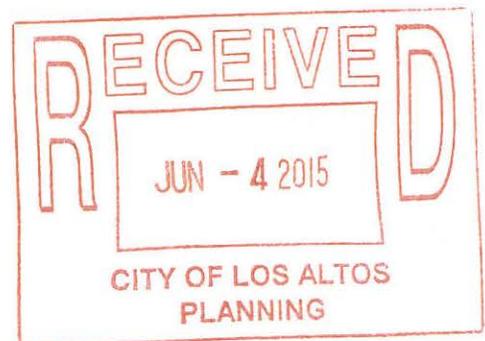
Warm regards,
Jeannice

--

Jeannice Fairrer Samani, PhD, MBA
Building Sustainable Environments
408.915.7122 (O) | 650.776.6022 (M)
Twitter@jeannice | www.fairrermgt.com | Jeannice@fairrermgt.com
Skype: jeannicefairrersamani



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12 January 2015

To whom it may concern:

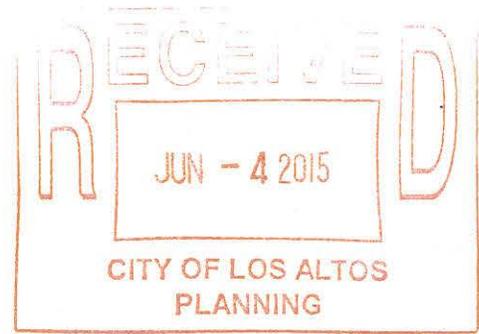
I am writing this letter in support of development of the 999 Fremont Avenue. Judy Simes and Dick Kenarney of Design Associates West who is working with Gregg Bunker approached me regarding the mixed-use project. As a resident of Los Altos for nearly twenty years, I am very interested in sustainable development and the economic vitality of our city.

With the update proposed renderings for the Loyola Corners new development project, I feel confident that this project would be added value to the economy and esthetics appeal of the Loyola Corner site.

Best regards,

Jeannice Samani, PhD, MBA, MCP

January 9, 2015



Dear Madam and Sir:

The Architect for the proposed new building at Loyola Corners showed me the plans for the new structure. Actually he presented more than one set of perspectives illustrating the progress from the initial concept to what now is apparently a final proposal.

I feel that although the original concept provided the amenities required for such a project, the scale and density were a bit overwhelming.

The elevations for the most recent proposal appear to have resolved these issues. The building now is much less dense and more fluid.

In summary, I would vote yes for this most recent proposal.

Cordially,

Wayne Rocañas
The Bicycle Outfitter
@ Loyola Corners

David Kornfield

From: Elie Alcheck
Sent: Thursday, June 04, 2015 1:49 PM
To: David Kornfield; Planning Transportation Commission
Subject: Letter in SUPPORT of 999 Fremont Avenue



Dear Planning Commissioners,

I am unable to attend the public hearing tonight regarding the proposed development at 999 Fremont Avenue but want to clearly communicate my support for the proposed project.

In 1975, after living in Los Altos Hills for 3 years, my wife and I bought our current home in Los Altos. Over the last 40 years, I have lived, worked and invested in Los Altos. In 1981, I purchased the building at 1000 Fremont Avenue where I have had my office ever since.

Nearly twenty five years ago, I served on the Loyola Corners Study Advisory Committee that drafted the Loyola Corners Specific Plan. Of the original members, I believe I am the only member that has both remained a homeowner of Los Altos and a commercial property owner in Loyola Corners. At the time of the Loyola Corners Specific Plan, the commercial center contained approximately 91,000 sq. feet of floor area. (See page 1 of the Specific Plan.) According to the Staff Report recently prepared by Sean Gallegos and David Kornfield for tonight's hearing, since then only 500 square feet of ground floor commercial space has been added to Loyola Corners within its core. In twenty-five years, there has been an increase of approximately **0.5%**. That is a far cry from the Committee's 1990 **one to three year goal** of 23,000 square feet of new commercial space. To have reached that goal of **25%** growth within 3 years, the Committee recommended numerous specific actions and recommendations to the City, including the establishment of a parking assessment district as well as a loosening of parking standards to accomplish such growth (p. 30). Despite how well received the Committee's recommendations were at the time, the City took no action on any of the recommendations and the 0.5% increase in commercial space reflects an unparalleled *lack of development* as a result of the abandonment of Loyola Corners by the city. Some neighbors would like to see the status quo of stagnation continue and have made alarmist claims that one new building will begin the transformation of the area into Santana Row and destroy the "village" character of Loyola Corners.

Fortunately, many residents of Los Altos, all of the commercial property owners in the Loyola Corners district, as well as Planning Staff can imagine a more vibrant, thriving Loyola Corners and support Mr. Bunker's project at 999 Fremont Avenue. Twenty-five years ago, the Loyola Corners Study Advisory Committee acknowledged that "[i]n order to compete effectively, Loyola Corners must expand." (p. 31). That statement holds true today. For opponents to suggest that the current building is inconsistent with the Loyola Corners Specific Plan or that the Committee did not intend to increase density in Loyola Corners are simply rewriting history.

At the time of the report, we acknowledged that "there are no vacant parcels as each property is fully developed with either buildings or paved off-street parking facilities." (p. 4). We understood that increased vitality would require increased density and we recognized that the *most recent* development (then) had commercial use on the first level and housing units above. (p. 4). Specifically, we expected private land owners to "shape the magnitude and specific design for future improvements such as new buildings, building additions and site design improvements." (p. 8). Moreover, at the time, we did not find the 30 foot height limit as being inconsistent with pedestrian-scale development and amenities. We were very specific that expansion of existing buildings could be either horizontal or vertical as long as it was within then existing 30' height development. Twenty-five years ago, a 30 foot building was deemed consistent with pedestrian scale development, and it remains so today.

I believe it would be a terrible mistake to force a new developer in 2015 to design his structure to “match” the current buildings. In our Specific Plan, we recognized that most of the buildings in Loyola Corners were small one-story structures constructed in the late 1940’s to the early 1960’s. We described them as “an architecturally undistinguished visual blur.” (p. 5). It is ironic that these buildings are now being described as a charming village by opponents who truly want nothing to change. Twenty five years ago, the Committee criticized the architectural style of the buildings with the following statement:

Visually, the area appears to be a mixture of unrelated structures, signs and street furniture with little or no landscaping extant. There is no established design focus...The parking area is frequently characterized as a “sea of asphalt,” with no landscaping, exposed trash receptacles – and inhospitable rear entrances to the commercial buildings. Similarly, there is little or no edge definition along the major streets and pedestrian walkways (as a result it gives the impression that everything just runs together).” (p. 4)

While it was debated whether there should be a “Design theme,” this notion was rejected. The Committee soundly determined that the adoption of rigid themes, modes or materials was considered “too limiting and restrictive in terms of creative design solutions.” (p. 51). “The principal goal is to achieve an attractive unified whole without requiring sameness.” (p. 51).

Mr. Bunker’s development accomplishes the goals of the Loyola Corners Specific Plan. It is an attractive and functional building that will spur other development in the area and will encourage and facilitate the long term viability of commercial activity at Loyola Corners. His design maintains the pedestrian scale of the area consistent with neighborhood commercial activity. And equally important, it provides more housing in our town. State law requires that Los Altos increase their housing supply between 2014 to 2022 by 477 units. This is not an aspirational goal of the city but a mandated requirement to accommodate the growing California population.

For these reasons, as well as all the reasons set forth in the Staff Report which recommends approval of this application, I hope the Planning Commission will recommend approval of this project to the City Council.

Elie Alcheck

Los Altos Resident, 40 years

Loyola Corners Property Owner, 33 years

1000 Fremont Ave., Suite 120

Los Altos, CA 94024

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Subject: 999 Fremont Ave 1 .docx
From: Angie Galatolo
Date: Thu, Jan 15, 2015 12:52 pm
To: '
Attach: 999 Fremont Ave 1 .docx
ATT00001.txt

Hi Judy,

Greg and I reviewed the previous and new renderings and we support and in favor of your new concept.

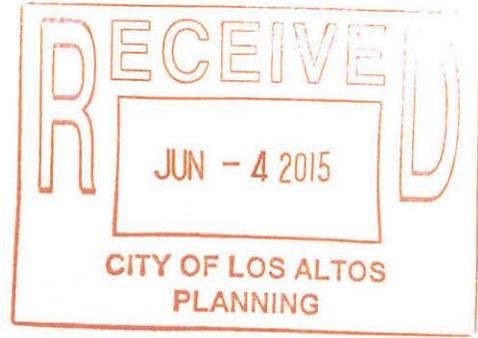
Best,

Angie

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From: James Walgren
Sent: Thursday, June 04, 2015 9:09 AM
To: Planning Transportation Commission
Cc:
Subject: FW: Support for Loyola Corners/ 999 Fremont Ave
Commissioners, please see below.



James

James Walgren, AICP
Community Development Director
650.947.2635

City of Los Altos
One North San Antonio Road
Los Altos, California 94022

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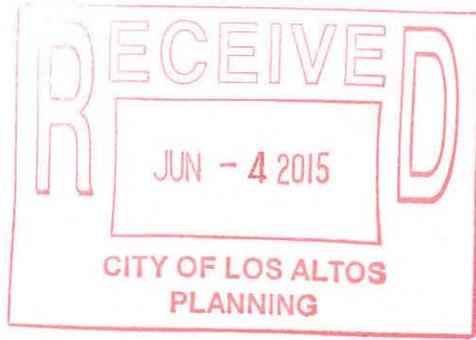
From: Anand Ganesan
Sent: Wednesday, June 03, 2015 10:15 PM
To: Sean Gallegos; James Walgren
Subject: Support for Loyola Corners/ 999 Fremont Ave

Dear James, Sean, Planning staff and PTC members:

We would like to express support of the mixed use project at Loyola Corners/ 999 Fremont Ave. Currently the Loyola Corner is not a nice site for the eye. It seems an outdated corner that hasn't seen much improvements and upgrades in a long time. We welcome that someone is willing to upgrade what looks like a rather run-down building and invest into the neighborhood. We believe that the changes that were done by the applicants since their initial submittal positively revitalizes the Loyola Corner in a respective way to the flair of the neighborhood.

The upgrade are long past due and we hope that with this initial investments it will encourage others to maintain and improve their properties as well. Many times the city hears primarily of strong oppositions and the people that support and are in favor of change don't necessary actively speak out, or think it is necessary. We wanted to let you know that as a Los Altos resident, we have spoken to many other fellow Los Altos residents, which are in support of the new proposed Loyola Corner, and think it is a welcome addition.

Sincerely,
Stefi and Anand Ganesan
, Los Altos, Residents



LOS ALTOS RESIDENT
NEAR LOYOLA
CORNERS.
SHE PREFERRED NOT
TO GIVE HER ADDRESS.

January 11, 2015

To whom it may concern:

I understand there are some questions involving the height of the proposed plan to replace the hair salon at Loyola Corners with a new three story building containing retail space, housing and off street parking.

One only needs to look at the "Clock Tower" building located at the corner of Miramonte and Fremont to see that should not be an issue.

I'm told that building's overall height is between thirty three and thirty five feet. From looking at the elevations of the proposed new structure the overall height is only thirty feet.

I would think this would be a minor issue as the proposed building, in my opinion is less bulky that the clock tower building.

When one considers all that this new building will provide: Additional quality retail space, nice housing, off street parking and a very good looking structure easily accessible from all three sides, the prospect for going forward should be a no brainer.

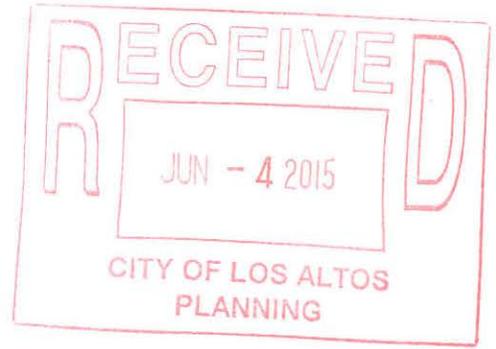
You've got my affirmative vote.

Sincerely,

Erin Chenna
Manager Ruffi Vitale

If the first design does not go through I like to see the second design go through. Los Altos is changing and becoming modern to fit today's generation, thus the new building will add more value to the city.

... it because it adds more elegant to



January 14, 2015

Dear Madam and Sir:

Design Associates West, the designers working with M Designs Architect for the proposed new mixed use building at 999 Fremont Avenue (Loyola Corners), showed me the plans for the new structure. They also presented multiple perspectives illustrating the progress from the initial concept to what now is apparently a final proposal.

I feel that although the original concept provided the amenities required for such a project, the scale and density were a bit overwhelming.

The elevations for the most recent proposal appear to have resolved these issues. The building now is now in better perspective to the surroundings and it will, in my opinion, substantially add retail space and attractive housing.

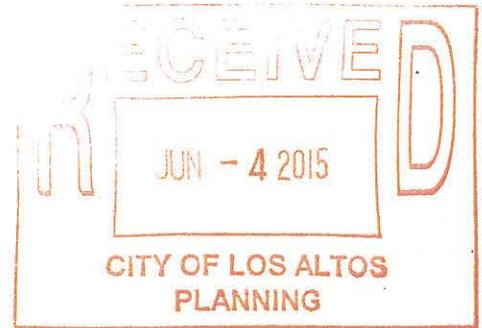
In summary, I vote yes for this most recent proposal.

Cordially,

A handwritten signature in dark ink, consisting of a large, sweeping loop followed by a wavy line.

Andrew Murray

Los Altos, CA 94024



January 9, 2015

To Whom It May Concern:

I would like the opportunity to voice my position and opinion relative to the proposed mixed use project on Loyola Corners. *999 Fremont*

On balance I feel it is an positive endeavor and for the following reasons:

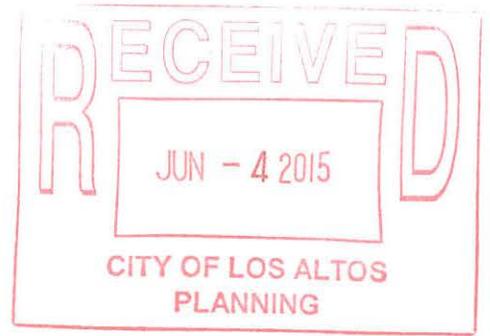
1. It should add much needed vibrancy to the local area.
2. The mixed use concept will add housing to the area.
3. It will provide an expanded and updated retail environment.
4. It will replace a 30 year old structure with a rather large and infrequently used parking area. Parking will be within the structure.
5. It will provide pedestrian access completely around the site.
6. The mass and scale of the building appears to compliment the surrounding area.

I make these comments based on a cursory review of the developer's and Architect's plans.

Cordially,

I frequent Loyola Corners

Jane Hardmiller - Alona Ct



January 11, 2015

To whom it may concern:

I understand there are some questions involving the height of the proposed plan to replace the hair salon at Loyola Corners with a new three story building containing retail space, housing and off street parking.

RE: 999 Fremont Avenue Los Altos, CA. 94024
One only needs to look at the "Clock Tower" building located at the corner of Miramonte and Fremont to see that should not be an issue.

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When one considers all that this new building will provide: Additional quality retail space, nice housing, off street parking and a very good looking structure easily accessible from all three sides, the prospect for going forward should be a no brainer.

You've got my affirmative vote.

Sincerely,

A handwritten signature in cursive script that reads "Jessica Nuel".

Residence:

. Los Altos, CA. 94024